AGENDA VILLAGE OF PLEASANT PRAIRIE PLEASANT PRAIRIE VILLAGE BOARD PLEASANT PRAIRIE WATER UTILITY PLEASANT PRAIRIE SEWER UTILITY Village Hall Auditorium 9915 – 39<sup>th</sup> Avenue Pleasant Prairie, WI May 20, 2013 6:00 p.m.

- 1. Call to Order
- 2. Pledge of Allegiance
- 3. Roll Call
- 4. Minutes of Meetings May 6, 2013
- 5. Public Hearing
  - A. Consider 2013-2014 Liquor License Renewal Applications.
- 6. Citizen Comments (Please be advised per State Statute Section 19.84(2), information will be received from the public and there may be limited discussion on the information received. However, no action will be taken under public comments.)
- 7. Administrator's Report
- 8. New Business
  - A. Consider an award of contract for the STH 165 Water Tower painting project.
  - B. Receive Plan Commission recommendation and consider Ordinance #13-15 to amend The Courtyard Junction Condominium Planned Unit Development to remove the requirement "at no time shall more than 20% of the completed units be rented as individual apartments."
  - C. Receive Plan Commission recommendation and consider Ordinance #13-16 adopting the Village of Pleasant Prairie Park and Open Space Plan 2013-2018 as a component of the 2035 Comprehensive Plan.
  - D. Receive Plan Commission recommendation and consider Ordinance #13-17 to amend Section 420-41 A and C of the Village Zoning Ordinance to clarify requirements of a home occupation permit.

Village Board Agenda May 20, 2013

- E. Receive Plan Commission recommendation and consider Resolution #13-11 for a Floodplain Boundary Adjustment on a 91 acre vacant property located on the east side of 88th Avenue south of Bain Station Road.
- F. Receive Plan Commission recommendation and a Certified Survey Map of the vacant 91 acre property generally located approximately ¼ mile south of Bain Station Road on the east side of 88th Avenue for the proposed 1.2 million square foot warehouse and distribution facility to be known as Majestic Badger Logistic Center.
- G. Receive Plan Commission recommendation and consider Ordinances #13-18 and #13-19 amend the zoning map to add a PUD Planned Unit Development Overlay District and a zoning text amendment to create the specific Planned Unit Development (PUD) District on the vacant 91 acre property generally located approximately ¼ mile south of Bain Station Road on the east side of 88th Avenue for the proposed 1.2 million square foot warehouse and distribution facility to be known as Majestic Badger Logistic Center.
- H. Consider the request for a one (1) year extension of the Conceptual Plan for the proposed Paradise Lake Subdivision on the properties generally located south of 104th Street and 22nd Avenue.
- I. Consider a professional engineering services agreement to design the construction, utilities and grading for the 116th Avenue extension.
- J. Consider professional services agreement to perform a traffic impact analysis for the Lakeview Corporate Park
- K. Consider renewal of an Agreement with the Kenosha County Humane Society, d/b/a Safe Harbor Humane Society, for impoundment services.
- L. Consider Board of Review and Police Commission appointments.
- M. Consider Operator License Applications on file.
- 9. Village Board Comments
- 10. Adjournment

The Village Hall is handicapped accessible. If you have other special needs, please contact the Village Clerk, 9915 –  $39^{th}$  Avenue, Pleasant Prairie, WI (262) 694-1400

# VILLAGE OF PLEASANT PRAIRIE PLEASANT PRAIRIE VILLAGE BOARD PLEASANT PRAIRIE WATER UTILITY PLEASANT PRAIRIE SEWER UTILITY 9915 - 39th Avenue Pleasant Prairie, WI May 6, 2013 6:00 p.m.

A regular meeting of the Pleasant Prairie Village Board was held on Monday, May 6, 2013. Meeting called to order at 6:00 p.m. Present were Village Board members John Steinbrink, Monica Yuhas, Steve Kumorkiewicz, Clyde Allen and Mike Serpe. Also present were Michael Pollocoff, Village Administrator; Tom Shircel, Assistant Administrator; Jean Werbie-Harris, Community Development Director;; Dave Smetana, Police Chief; Rocco Vita, Village Assessor; Mike Spence, Village Engineer; John Steinbrink Jr., Public Works Director; Carol Willke, HR and Recreation Director and Jane M. Romanowski, Village Clerk. Four citizens attended the meeting.

#### 1. CALL TO ORDER

# 2. PLEDGE OF ALLEGIANCE

# 3. ROLL CALL

#### 4. ELECT PRESIDENT PRO-TEM

Steve Kumorkiewicz:

I make a motion to return Mrs. Yuhas as President Pro-Tem.

Clyde Allen:

Second.

John Steinbrink:

Motion by Steve, second by Clyde. Any discussion? Monica has done a very good job, and she continues to do a good job. I think she amazes us all with what she accomplishes sometimes.

#### Monica Yuhas:

Thank you, Mr. President. It's been five years now, and it's a privilege to work with all of you and the department heads. And I do appreciate the continue confidence and support.

#### John Steinbrink:

Five years, that isn't that much. Good job, Monica.

# KUMORKIEWICZ MOVED TO ELECT MONICA YUHAS AS PRESIDENT PRO-TEM; SECONDED BY ALLEN; MOTION CARRIED 5-0.

#### John Steinbrink:

Congratulations.

# Monica Yuhas:

Thank you.

# 5. PRESENTATION BY THE PLEASANT PRAIRIE FIRE & RESCUE AUXILIARY OF A MONETARY DONATION TO THE RACINE FIRE BELLS.

#### Ann Barnes:

Hello, my name is Ann Barnes, and I'm representing or the spokesperson for the Ladies Auxiliary. And I have with us Mr. Dave Hadden. He is the Vice President of the Racine Fire Bells. Mr. Hadden, the Pleasant Prairie Fire & Rescue Ladies Auxiliary would like to thank the Racine Fire Bells for a valuable service that you provide our men and women in the fire department. And in order to continue the support we would like to present a check in the amount of \$1,000 to the Racine Fire Bells.

# Dave Hadden:

Thank you very much. I guess I'd say thank you to everybody that was involved with their fundraising and everything that supports them. This will go to good use. Thank you.

#### Mike Pollocoff:

I just might add that the recent Fire Bells, it's been quite a while now when we have a major incident and the guys are out there for a prolonged period of time. They come down with the truck for water, refreshments, helping keep them warm. And they're doing that throughout the Kenosha/Racine area. And it's something that the Village or some of the other communities don't have to take on. So it's really an appreciated service. And it's nice that the Ladies Auxiliary helped to finance that because I don't think they get -- Racine is not paying your way.

#### Dave Hadden:

Well, we live off the donations. So whatever we get by donations is how we live. But, yeah, we do three counties, Racine, Kenosha and Walworth. We do have a unit out in Lake Geneva that does that area out there for us. But, like you say, we supply all the rehab for any major fire, police incident. Anything that we're called for we'll supply the coffee, soda, water, Gatorade and some snacks for the firemen and that, get them sugared back up to get them back in there. So, again, thank you.

John Steinbrink:

Congratulations. I might add you may live off the donations but we live off the salad luncheons here. Thank you.

#### 6. MINUTES OF MEETINGS - APRIL 1, 2013.

Monica Yuhas:

Motion to approve.

Steve Kumorkiewicz:

Second.

John Steinbrink:

Motion by Monica, second by Steve. Any additions, corrections?

# YUHAS MOVED TO APPROVED THE MINUTES OF THE APRIL 1, 2013 VILLAGE BOARD MEETING AS PRESENTED IN THEIR WRITTEN FORM; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

#### 7. CITIZEN COMMENTS

Jane Romanowski:

Richard Ginkowski.

John Steinbrink:

Do we call you Judge now?

Richard Ginkowski:

I guess since I'm appearing officially, yes. No, you can do whatever you want. Dick Ginkowski appearing in my capacity as Municipal Judge. Just to give you a brief update on a few things. They're not action items, but just so that you have information our court handles over 5,000 cases a year. And while it's an independent branch of the Village government I promised that it would not be isolated.

A few quick thinks to note. Fiscal responsibility, at 12:01 a.m. on May 1st I issued an order raising the court costs from \$23 to \$28 which is the maximum. There are bills in the legislature right now, AB 22 and Senate Bill 15 to increase the cost to \$38, the maximum amount. I wrote to our legislators seeking their support. If the Board would like to put a resolution at some point that would be nice. I will also order as I am required to do reimbursement for the police

> department's cost for withdrawing blood specimens in drunk driving cases. That's not been done in the past. Once we get the dollar amounts we will be recovering those. The municipal bond schedule actually remains the same in that the base amount for each violation is unchanged. Any revisions will be presented to the Board after I've had the change to discuss them with our stakeholders and get their input.

> I reappointed Brian Sheffler as the court's clerk. That's a no brainer given the good work that he's done. Our part-time deputy clerk, Stephanie Koser has taken a job with the Racine Municipal Court as its clerk. That's a major promotion for Stephanie. Tammy Miller whose position in the engineering department was being phased out is taking over for Stephanie. The court will remain in session every Wednesday. After getting feedback from stakeholders the hearing times are going to be moved from the afternoon to the mornings except for the fourth Wednesday of the month when we'll have a late afternoon or early evening session to make the court more convenient for users, promote public safety by allowing our third shift officers to get home and get some sleep rather than not go without sleep in the morning, and eliminate police overtime expense for second shift officers to appear in court during the afternoon. They will appear in the evening session if they need to appear.

> Juvenile cases will be on the 5:30 p.m. calendar. It makes no sense to have a kid with a truancy ticket miss school to come to court. And all juvenile and drunk driving cases will now require a court appearance. We implemented a special email address so that citizens can enter their not guilty pleas by email, reduce the congestion a little bit in the hallway in the building on court days. I communicated with Village department heads seeking input on ways we could work together. And while we still have some work to do to bring the court up to contemporary standards and to match the level of service provided by the Village in other areas, I wanted at least to bring that information to your attention. I'm sure we'll be communicating more in the future. But at least you should know a little bit about what we've done initially to take care of some things that needed immediate attention. Thank you.

John Steinbrink:

Thank you.

Jane Romanowski:

There are no other signups tonight, Mr. President.

John Steinbrink:

Anyone else wishing to speak under citizens' comments?

# 8. ADMINISTRATOR'S REPORT

#### Mike Pollocoff:

Mr. President, two weeks ago we had some significant rains in the Village. And although we had some areas where we had some road closures I think on 116th Street or 104th by the bike path, nothing really significant at Cooper Road by the Zirk Pond. We were able to evaluate performance of some of the stormwater projects that we've undertaken and we've put in place. And significantly Carol Beach Unit 2 we didn't have any problems there. It was handling a lot of water, but I think the key thing was it was handling the water. And we did have some flooding backup not in houses at Chateau but in that sewer area there where we're working on a problem to address that at this time.

As the public works director notified the DNR that we made the decision in some locations where it was necessary to bypass the sanitary sewer rather than -- the choice that he was faced with was bypass the sanitary sewer or store it in basements. So we did that and a couple of the areas where the Kenosha system was backed up and our system drains into Kenosha's. So once they're in a state of bypass it just reflects automatically on ours and we have to change it. There's probably going to be some kind of remedy or fix that's going to be required to that once we put them on notice. And I think just about everybody in Southeast Wisconsin is going to be dealing with that, but I don't think it's going to be unmanageable. I think the key thing as far as flooding is we really seemed to in some of the areas we were doing some work we alleviated those problems and we didn't have an issue. It was really hard to evaluate that last year since it was so dry, but this year it's giving us a good change to take a look at it.

John Steinbrink:

Thank you, Mike.

# 9. **NEW BUSINESS**

A. Receive Plan Commission recommendation and consider Ordinance 13-11 to amend a portion of the Village of Pleasant Prairie 2035 Comprehensive Land Use Plan Map 9.9 to remove the Urban Reserve Designation from the property located at the northwest corner of STH 50 and 91st Avenue and to update Appendix 10-3 related to the Goddard School development.

Jean Werbie-Harris:

Mr. President, I would ask that Items A, B and C be brought up at this time. I will discuss them as part of one presentation, and separate actions will be needed with a roll call vote on the one from the Village Board.

B. Receive Plan Commission recommendation and consider Ordinances 13-12 and 13-13 to rezone the vacant property generally located at northwest corner of STH 50 and 91st Avenue from B-2 (UHO) Community Business District with an Urban Landholding Overlay District to B-2 (PUD), Community Business District with a Planned Unit Development Overlay District and approve a zoning text amendment to create the specific PUD District zoning regulations related to the Goddard School development.

C. Receive Plan Commission recommendation and consider a Development Agreement for the installation of public sidewalks and associated landscaping within the rightof-way of 91st Avenue, to modify the Final Plat for Westfield Heights Subdivision and to modify the vision triangle easements, removing an existing access easement and dedicating a new access easement for property generally located at the northwest corner of STH 50 and 91st Avenue related to the Goddard School development.

#### Jean Werbie-Harris:

As shown on the slide, the Goddard School is proposing to build a facility of 8,251 square feet. It's proposed to be a daycare preschool. It will be on a 1.3 acre site at the northwest corner of Highway 50 and 91st Avenue. The Goddard School is proposed to provide care and schooling for infants, toddlers and preschool children ranging in ages from six weeks to six years of age. At full capacity the school will have 132 children. The facility will include nine classrooms with restroom facilities and direct access to an outside fenced in play area, two office areas on either side and a secured entry vestibule, a kitchen area, staff area, a men's and women's restroom within the center of the building and classrooms, again, to accommodate the older children as well as the younger children will have a kitchenette, that's the 12 months to 18 months. There's no general purpose or general assembly space within the building. Outside there will be two separate fenced in playground areas.

The hours for the facility will be 7 a.m. to 6 p.m. There will be 30 full-time and part-time employees, 132 children. There will be adequate parking space, both regular parking spaces and handicapped accessible parking space. There will be additional trees and plants that will be planted along 91st Avenue to help screen and provide buffer to the adjacent pumper pad parking area location as well as the parking in the dumpster area that's located on the site.

The first part of their request is the 2035 Comprehensive Plan amendment. Again, this is Ordinance 13-11. The Village's Comprehensive Plan Map 9.9 is proposed to be amended, and this is to remove the urban reserve designation from the property. The underlying commercial land use designation will remain, and we'll also be updating Appendix 10-3 of the Comprehensive Plan to include the amendment. Again, all of the other items, the site and operational plan and those related items have been previously approved by the Village Plan Commission, but they all are subject or conditioned upon the approval of these items before the Village Board this evening.

As you can see, this is an area that we have designated as a community commercial land use designated area, again, on either side of 91st Avenue and then 74th Street extending all the way to 88th Avenue. There is one existing family home that is between 73rd and 74th Street. We did not modify that to a commercial designation. It still is in a residential designation. The lands to the south of this are part of the Prairie Ridge commercial development.

With respect to this project, again, they are looking for the removal of the UHO which is the Urban Landholding Overlay District, and the placement of a PUD or a Planned Unit Development Overlay District on the property. This is based upon various modifications that have been recommended by the Plan Commission and the staff such as some modifications with respect to the driveway setback, the parking lot setbacks, and the side yard setback. We've made it so that the site is a little bit closer to the adjacent roadways, but the situation is this is a high profile area of Highway 50, commercial land on Highway 50, and we wanted to mirror the setbacks to the adjacent land uses that are already in the Village of Pleasant Prairie.

They still meet the minimum requirements for open space and such on the site. And they are still providing an access across from 91st-74th Street over the top of this site basically to access the vacant land that lies to the west adjacent to Highway 50. So, again, the zoning text amendment is specifically to create that PUD and to remove that UHO overlay. And the zoning map ordinance amendment would be 13-12, and the text amendment would be 13-13. Again, all of these items were a matter of public hearing before the Village Plan Commission, and there were no objections but rather support by the audience as well as the petitioner and the landowner.

Specifically just to outline a couple of the PUD modifications in the B-2 District, the 1.3 acre parcel is what is being proposed. Under the current ordinance a 2 acre parcel is required. A building setback of 20 feet from the west property line is being requested. And typically under the ordinance a 30 foot setback would be required. A 10 foot setback to the west property line and a 1 foot setback to the east property line, again, this is to accommodate the pumper pad for the fire department onsite. There was some initial discussion to put it on 91st Avenue, but in working with the fire department and the turning radiuses that have been proposed it does accommodate the opportunity to swing that fire engine right in and to stage adjacent to the east side of the building.

The other modification was that a detached garbage enclosure is being allowed for this development subject to adequate screening. And they intent to do screening along 91st Avenue, and in that bulb area which is right at that entrance they'll do some additional screening there. And then there's screening on either side of the enclosure as well.

This development as it is going to have a planned unit development does require a DSIS agreement. And, again, that's the digital security imaging system. And, in fact, we've written the agreement and the property owners and the new tenant will have that particular agreement. And what it does is it identifies that the camera system for the exterior of this site and exterior of the building will have a live feed connection back to the police department for monitoring of the site. Again, there won't be minute-by-minute monitoring. This is in the event that there's an incident or something that occurs out there that we will have that access.

Again, this is a system that's going to be owned and operated by the Goddard School. The Village is not going to be owning or operating the system. We have the right but not the obligation with an access easement to enter upon that site with permission and to obtain the camera images and the information. They do need to keep two weeks worth of video at this location. They also have a pretty sophisticated system inside for security and thumb print

entrance. And so they have a number of things that they will be incorporating as well to make this a very safe and secure facility for their students, parents and teachers at this location.

The other item is the development agreement. Because there are going to be public sidewalks adjacent to this development to the east in 91st Avenue and 74th, they will be responsible for installing the public sidewalks and maintaining those sidewalks. And we have a development agreement to secure that those will be installed. And then they will be relocating and planting street trees in the 91st Avenue. There are a couple of them there. I think they have to do some relocations, and a couple of them I think were removed. So they need to replant those. And they'll be doing additional landscaping in the right of way to screen that pumper pad and, again, the garbage dumpster to the northwest.

The next item is an affidavit of correction of the final plat. The developer and his engineer talked to the State which respect to how large vision triangles that they needed to actually have adjacent to Highway 50 at 91st Avenue. The State agreed that the vision triangles were somewhat excessive and agreed to reduce them from the 50 by 50 down to the typical 15 by 50. And so you'll see that affidavit of correction to the plat that needs to be recorded as well for this particular project.

And then finally the last item is a correction instrument. Because this is a daycare/preschool type facility where children are coming out with their parents, the original conceptual plan had identified that there would be some cross-access through the parking lot to the land to the west. All of us agreed that it probably didn't make a whole lot of sense to have a lot of through traffic going through a parking lot where children are being dropped off and picked up by their parents. So the discussion was not to have that cross-access, and then to slightly modify the access through Lot 19 and then over across the top of Lot 20 in order to access that lot.

Again, the current developer does not own the vacant lot to the west. That's owned by a different individual in town. And for commercial purposes if that lot does develop as a commercial property they would not have direct access off of Highway 50. Their access would come through this easement area. It's an easement that would accommodate access, a private access road to get to this site wide enough to accommodate the fire department or any other vehicles that need to access.

So that is all of the items related to the Goddard School. If there are any questions I'd be happy to answer them. Again, we would ask for three separate motions on each of the items, the Comprehensive Plan amendments, the zoning map and text amendments, and then finally the development agreement, final plat amendment and new access easements. Those can all be grouped as one. The DSIS was already approved by the Plan Commission, but again those are all referenced in the development agreement documents as well.

# Steve Kumorkiewicz:

I have a question for Jean. Jean there's a piece of property adjacent to White Caps on 75th Street. Right now as it is it is [inaudible] because you cannot exit to Highway 50, correct?

#### Jean Werbie-Harris:

There is I believe one residential AP, but there's not a commercial access, and that's this property right here that's owned by Dr. Duranti. So the access to this property would be through this access easement. They would have to build this private driveway, private roadway connection in order to access this property.

#### Steve Kumorkiewicz:

Okay, thank you.

#### Michael Serpe:

Hearing this at the Plan Commission I like the presentation that Mr. Mills gave with reference to security. Jean, do we have in place with the DSIS, the security system, to review that and if needed can we force an upgrade or an improvement on that as years go on?

#### Jean Werbie-Harris:

Well, I'm not sure if I'm going to answer your question. So let me just start. The Village just recently adopted a new ordinance, the Village Board did a couple weeks ago. And what the ordinance did was it refocused the provisions and the specifications for a DSIS. It no longer specified a specific camera type or specific information like that. But rather it gave information clarity, resolution and different things that needed to be provided throughout the lifetime of the cameras. It will be up to the property owners, My Sunshine Real Estate, to make sure that the cameras are always bringing back the type of clarity of photos and images that we need at the police department. If there's any questions with respect to the visibility of images coming back, at that point the Village has the right to contact the owner to ask them to do something to upgrade their system.

One of the things we incorporated now is the infrared technology. We specifically made a reference to that. Because a number of the newer retailer operations out in Prairie Ridge, for example, they want to start powering down some of their lights, and that's only possible if they reinstall new cameras. And so even though some of those cameras were just put in in 2008 which was five years ago, they're changing out some of those cameras to make sure that they have the latest technology. I'm sure that Goddard School will want to stay current with the technology and the images as the Village would.

#### Michael Serpe:

Good, thank you. I think this is going to be a great success for that area with the development that's taking place and the amount of people that are working in that area. So I'd move approval of Ordinance 13-11.

Clyde Allen:

Second.

John Steinbrink:

Motion by Mike, second by Clyde for adoption of 13-11. Any further discussion? Hearing none, this is a roll call?

Jane Romanowski:

Yes, it is.

SERPE MOVED TO CONCUR WITH THE PLAN COMMISSION RECOMMENDATION AND ADOPT ORDINANCE #13-11 TO AMEND A PORTION OF THE VILLAGE OF PLEASANT PRAIRIE 2035 COMPREHENSIVE LAND USE PLAN MAP 9.9 TO REMOVE THE URBAN RESERVE DESIGNATION FROM THE PROPERTY LOCATED AT THE NORTHWEST CORNER OF STH 50 AND 91ST AVENUE AND TO UPDATE APPENDIX 10-3 RELATED TO THE GODDARD SCHOOL DEVELOPMENT; SECONDED BY ALLEN; ROLL CALL VOTE: SERPE – AYE; KUMORKIEWICZ AYE; YUHAS – AYE; ALLEN – AYE; STEINBRINK – AYE; MOTION CARRIED 5-0.

Monica Yuhas:

Motion to approve Ordinance 13-12.

Steve Kumorkiewicz:

Second.

John Steinbrink:

Motion by Monica, second by Steve for adoption of Ordinance 13-12. Any further discussion?

Monica Yuhas:

Id' like to amend that to Ordinance 13-12 and 13-13.

Steve Kumorkiewicz:

Including both.

Monica Yuhas:

Yes.

Steve Kumorkiewicz:

Okay.

John Steinbrink:

Second agrees?

Steve Kumorkiewicz:

Second.

John Steinbrink:

We have a motion and a second. Any discussion on this item?

YUHAS MOVED TO CONCUR WITH THE RECEIVE PLAN COMMISSION RECOMMENDATION AND ADOPT ORDINANCES 13-12 AND 13-13 TO REZONE THE VACANT PROPERTY GENERALLY LOCATED AT NORTHWEST CORNER OF STH 50 AND 91ST AVENUE FROM B-2 (UHO) COMMUNITY BUSINESS DISTRICT WITH AN URBAN LANDHOLDING OVERLAY DISTRICT TO B-2 (PUD), COMMUNITY BUSINESS DISTRICT WITH A PLANNED UNIT DEVELOPMENT OVERLAY DISTRICT AND APPROVE A ZONING TEXT AMENDMENT TO CREATE THE SPECIFIC PUD DISTRICT ZONING REGULATIONS RELATED TO THE GODDARD SCHOOL DEVELOPMENT; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

Michael Serpe:

Move approval of the development agreement.

Steve Kumorkiewicz:

Second.

John Steinbrink:

Motion by Mike, second by Steve in concurrence with the Plan Commission recommendation on the development agreement. Any further discussion?

SERPE MOVED TO CONCUR WITH THE PLAN COMMISSION RECOMMENDATION AND APPROVE A DEVELOPMENT AGREEMENT FOR THE INSTALLATION OF PUBLIC SIDEWALKS AND ASSOCIATED LANDSCAPING WITHIN THE RIGHT-OF-WAY OF 91ST AVENUE, TO MODIFY THE FINAL PLAT FOR WESTFIELD HEIGHTS SUBDIVISION AND TO MODIFY THE VISION TRIANGLE EASEMENTS, REMOVING AN EXISTING ACCESS EASEMENT AND DEDICATING A NEW ACCESS

# EASEMENT FOR PROPERTY GENERALLY LOCATED AT THE NORTHWEST CORNER OF STH 50 AND 91ST AVENUE RELATED TO THE GODDARD SCHOOL DEVELOPMENT; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

## John Steinbrink:

Thank you. And I think everybody is looking forward to another daycare.

D. Consider partial termination and release from four Development Agreements between V.K. Development Corporation and the Village of Pleasant Prairie for public-related infrastructure improvements in the Prairie Ridge Development installed in the land areas located between 88th and 104th Avenues south of STH 50 in the Prairie Ridge Development which property is now owned by Water Street Land LLC.

#### Jean Werbie-Harris:

Mr. President and members of the Village Board, Attorney Lynn Ludke of Godfrey & Kahn she is the agent for Water Street Land, LLC. On behalf of the property owner she is requesting a partial termination and release from four different development agreements that were entered into by and between the Village of Pleasant Prairie and V.K. Development. These development agreements were for the infrastructure improvements for Prairie Ridge. And this is for all that land between 88th Avenue and 104th Avenue from Highway 50 almost all the way down to County Trunk Highway C.

As you know that a number of these agreements were entered into between 1996 I believe and about 2002, various agreements for different types of public improvements. All of the public improvements for Prairie Ridge as referenced in these agreements have been completed, inspected and accepted by the Village of Pleasant Prairie. All of the obligations have been completed, inspected and accepted by the Village of Pleasant Prairie.

Oftentimes the attorneys would like to see more clear title on properties and lands start to exchange hands, and that's the purpose of this request for the release or of these terminations of these partial release. Again, specifically the land that Water Street owns is all of that land at that northwest corner between basically 99th Avenue and 104th Avenue, 77th Street up to 75th Street in that particular area. So when I talk about a partial release I'm just talking about the lands up there.

So there are four different development agreements that specifically they're asking a release from. The first is the development agreement and memo of development agreement entered into on September 13, 1996 between V.K. Development Corporation and the Village. The agreements were related to the initial mass rough grading, drainage improvements and erosion control work for the development based upon their preliminary plat and preliminary engineering for the Prairie Ridge development.

The second release relates to the development agreement and memorandum of development agreement entered into on November 4, 1997 between V.K. Development Corporation and the Village. The agreements are related to the completion of the final required public and private infrastructure improvements for the development based on the final plat and final engineering. The third is the development agreement and memorandum of development agreement entered into on March 2, 1998 between V.K. Development Corporation and the Village. The agreements were related to the completion of the final required public and private infrastructure improvements for the development based on the final required public and private infrastructure improvements for the development based on the final required public and private infrastructure improvements for the development based on the final plat and final engineering plans. And this is for Prairie Ridge Subdivision phase 1 of the development also known as addition number 1.

And, finally, number four the development agreement and memorandum of development agreement entered into on October 8, 2001 between V.K. Development Corporation and the Village. These agreements were related to the completion of mass grading of 76th and 77th Streets based on the preliminary engineering plans at Prairie Ridge. There was then a follow-up amendment to the agreement, and this related to the completion of the full infrastructure for 76th, 77th Streets. And, again, this is this segment of roadway and all the related public infrastructure.

The staff actually worked with the attorney to prepare all these documents, and we recommend approval of the partial termination of development agreements for Water Street Land.

Steve Kumorkiewicz:

One question here. Jean, with this development right here, V.K. is going to be completely out of the picture, right?

#### Jean Werbie-Harris:

Well, yes, and technically V.K. has been out of the picture for quite some time since of the bankruptcies he's actually been out of the picture because he doesn't technically have any financial obligations left. The situation is all of the public improvements have been completed, and the Village is not at risk and there's no liability.

#### Steve Kumorkiewicz:

Okay, that was my only concern. Thank you.

#### John Steinbrink:

Any other questions?

#### Jean Werbie-Harris:

Just to clarify they would like to get these removed from title.

Steve Kumorkiewicz:

Okay, I make a motion to approve.

Monica Yuhas:

Second.

John Steinbrink:

Motion by Steve, second by Monica for approval. Any further discussion?

# KUMORKIEWICZ MOVED TO APPROVE A PARTIAL TERMINATION AND RELEASE FROM FOUR DEVELOPMENT AGREEMENTS BETWEEN V.K. DEVELOPMENT CORPORATION AND THE VILLAGE OF PLEASANT PRAIRIE FOR PUBLIC-RELATED INFRASTRUCTURE IMPROVEMENTS IN THE PRAIRIE RIDGE DEVELOPMENT INSTALLED IN THE LAND AREAS LOCATED BETWEEN 88TH AND 104TH AVENUES SOUTH OF STH 50 IN THE PRAIRIE RIDGE DEVELOPMENT WHICH PROPERTY IS NOW OWNED BY WATER STREET LAND LLC; SECONDED BY YUHAS; MOTION CARRIED 5-0.

E. Consider Resolution #13-09 to support the transfer of Chiwaukee Prairie land owned by the Village to The Nature Conservancy for permanent open space protection.

Jean Werbie-Harris:

Mr. President and members of the Board, I am really, really happy to announce that the purpose of this Resolution 13-09 is to approve the transfer of land from the Village of Pleasant Prairie to The Nature Conservancy of Wisconsin. Back in 2001 the Village had done a joint press release with The Nature Conservancy of Wisconsin and the Village of Pleasant Prairie, and the purpose of that press release was for the Village to announce that we were accepting a 443 acre land donation from The Nature Conservancy of Wisconsin to the Village for all of that land west of the Des Plaines basically from 165 north to almost the River Road area which has now been a part of the Village's Prairie Springs Park as you know for a number of years.

In exchange for that we had taken possession of some lots down in Carol Beach. Specifically there were three lots which have since been combined as to one lot that is just to the west of the Prairie Harbor Yacht Club Marina. This is a very significant site for them because of the different types of plant life that is found there. And the situation is when we obtained it from the developer from Michigan it didn't come with a clean title. Unfortunately we got it right before they went bankrupt, and there were 16 or 18 mortgages that attached to the property.

So after some significant research, our Village attorney one by one had to get release from each of these banks in order to release it from this particular parcel. So it took a long time, almost 12 years to get the releases and then to get a legal description put together and to go through the entire process that we needed to. Honestly there were times when probably it was sitting in a

cold file someplace before we could get focus back on the particular file. But I'm very pleased to say that we're there, and The Nature Conservancy is very anxious to get this particular property.

So the resolution you have before you, again, references the 1985 Chiwaukee Prairie Carol Beach Land Use Management Plan as the plan that we used for acquisition areas down in Chiwaukee Prairie. As early as 1990 the Prairie Harbor Yacht Club Marina had donated to the Village in these particular lots in the Chiwaukee Prairie Subdivision. Since that time these lots have been combined, that's reference in the resolution. The Village has had a longstanding cooperative relationship with The Nature Conservancy and supports their continued efforts in the acquisition, preservation and protection of the prairie.

Again, they transferred over 400 acres to the Village which we have owned for a number of years. And they hung in there while we went through this entire process to get this land free and clear to transfer to them. And that's referenced in the resolution. The attorney worked for several years with me, and eventually we established clean title. Working with the Village Assessor we put together a legal description. And now we are recommending that this land be transferred to The Nature Conservancy of Wisconsin.

So by Village Board Resolution 13-09 we are recommending based on the Village's commitment agreement and our longstanding positive relationship to promote the environmental preservation of conservancy areas of our community, the Village hereby approves the transfer of this referenced property. The Tax Parcel Number is 93-4-123-323-1451. The staff recommends approval as presented.

Steve Kumorkiewicz:

I make a motion to approve Resolution 13-09.

Monica Yuhas:

Second.

# John Steinbrink:

Motion by Steve, second by Monica. Any further discussion? Clyde?

#### Clyde Allen:

Thank you, Mr. President. I'm not questioning what we're doing. I guess my question is if we ended up making a deal to get this land to give to The Nature Conservancy, how is that benefitting us as a Village? Or, is it just The Nature Conservancy being benefitted by us entering into the agreement for releasing the land?

#### Jean Werbie-Harris:

Actually, in 2001 we had a lengthy discussion on this matter. And we felt that for The Nature Conservancy to give us 425 acres of land in an area that would benefit the Village's park which is Prairie Springs Park, and our relationship to help manage that particular area, that seemed to be reasonable, fair, accommodating. And we felt that their strengths were down in Chiwaukee Prairie. We owned very scattered pieces of land down in Chiwaukee Prairie, and it's very difficult for us to manage those pieces of land. And we typically would cooperate and enter into agreements with either The Nature Conservancy or the DNR for the management of those areas.

So, again, they have been very patient and cooperative since we received their part of the donation in 2001, and we've been working since that time to get this land for them. So it seemed like a real logical transition and cooperative exchange to make with The Nature Conservancy.

# Clyde Allen:

Okay, so basically Prairie Springs Park benefitted in 2001?

Jean Werbie-Harris:

I think not only Prairie Springs Park but the Village has a whole benefitted greatly.

Clyde Allen:

Correct. That's where our benefit came in. Thank you.

John Steinbrink:

Any further comment or question?

# KUMORKIEWICZ MOVED TO ADOPT RESOLUTION #13-09 TO SUPPORT THE TRANSFER OF CHIWAUKEE PRAIRIE LAND OWNED BY THE VILLAGE TO THE NATURE CONSERVANCY FOR PERMANENT OPEN SPACE PROTECTION; SECONDED BY YUHAS; MOTION CARRIED 5-0.

#### F. Consider award of contract for the 2013 Paving Program.

#### Mike Spence:

Mr. President and members of the Board, sealed bids were received for the 2013 Village paving program on April 25th. We received two bids for this year's program. There's basically seven items that are included in this year's program. Section 1 is the completion of 116th Street. That's to pulverize and relay from where we left off last year. So when that is completed this year then 116th will have totally been resurfaced almost all the way to the lake there.

Section 2 is the pulverizing and relay of 89th Street which is just off of Cooper Road there. Section 3 is pulverize and relay the pavement on 98th Avenue which is just south of C there. Section 4 is a portion of the Prairie Ridge Subdivision. There we're going to be doing an ultra thin overlay with some adjustments. And that's area in there. Section 5 is our 57th Avenue, the water tower site. That road for public service works vehicles is very deteriorated. So we're going to pulverize and relay that. Section 6 is the entrance road to Ingram Park is going to be paved from 93rd Street north to where the sledding hill is. Finally, Section 7 includes -- a couple years ago when we did the realignment of the west frontage road the existing road was left in place because of budget considerations. And so as part of this project the millings from the old west frontage road are going to be removed as part of this project.

As I said there were two bids. The low bid was by Black Diamond in the amount of \$649,265.10. Payne & Dolan was second at \$665,276.34. As you can see with that amount of money the bids were pretty tight. So we're pretty happy with that. The budget amount for this year is actually \$650,000. So out of this total some of the money is actually not coming out of the paving program. So we're well within the budget for this year's program. So I recommend that the contract for, should say 2013, I'm sorry, 2013 paving be awarded to Black Diamond Group. I'd be glad to answer any questions.

#### Michael Serpe:

I have one. Go ahead, Mike.

#### Mike Pollocoff:

I was just going to mention that this is not as meager as last year, but it's trending meager. At one point we were putting about \$1.3 million into asphalt, resurfacing and rehabilitation and surface coating and things like that. That's gone away. We lost 10 percent of our road aids in the last budget. It looks like we're on track to lose another 10 percent. And it's the confluence of reduction in aid, the flat mill rate, and then on top of it, and I don't care what level of government you have, the price of asphalt is going up. So what you can get done that product is going up. And the amount of money we have to do it is going down. I guess at least for the next couple years I don't see that changing much.

One of the things that we're going to be looking at is doing -- I think with this resurfacing here John's pretty much got the arterials caught up with where we need to be. The main things that we're going to be focusing in on now is not letting our roads slip into a maintenance status where it has to be pulverized and rebuilt. So the roads we have for a small amount of money we can put a surface coat or treatment on it and buy 15 more years or 10 more years depending on what kind of road it is to extend that. And hopefully we can get through a cycle where we can start building up the paving fund again.

You see some of that work happening on Prairie Ridge. The amount of money that we're going to be spending on that when you compare it to 89th Street which is basically a mill it up and completely redo it because it's failed, we're able to extend the life of the roads, and that's really in this tight time that's what we're going to do. And I think one of the things we're going to be

getting in the newsletter is making sure people understand when we do a surface treatment on it that's not a pavement. The purpose of that is to fill the cracks and keep it held together so water doesn't damage the road over the next ten years. And then we'll have to put one on there, but we can do that for substantially less than just waiting for it to fail completely and resurface it.

But I think next year without having an arterial to do we'll be able to get into more neighborhoods and get more neighborhood work done once you get 116th Street done, because 116th was by far the worst arterial we had at this time.

#### Michael Serpe:

The ultra thin coat is that what was put on 85th Street west of 39th Avenue?

#### Mike Spence:

No, it's not. It's a different treatment.

#### Mike Pollocoff:

It's actually asphalt.

#### Michael Serpe:

So it's actually a thin layer. Alright. Is 85th Street are you satisfied with the job that was done on that?

#### Mike Spence:

The way we left it last year is that we were going to go back and meet with the contractor and see how it fared over the winter to see if the plows and if that did anything. We haven't done that yet.

#### Monica Yuhas:

Mike, I know on 116th when you did the paving going east there was some drainage issues. You're not running into any drainage going west to finish 116th up to 39th?

#### Mike Spence:

No, no. I mean generally when we do this public works a lot of times will go and replace culverts before the paving. But there's no major storm water issues that we're going to be tackling as part of this.

#### Monica Yuhas:

Thank you.

Michael Serpe:

Move approval of the contract for 2013.

Clyde Allen:

Second.

John Steinbrink:

Motion by Mike, second by Clyde for adoption of the contract to Black Diamond Group in the amount of \$649,265.10. Any further discussion?

SERPE MOVED TO AWARD A CONTRACT FOR THE 2013 PAVING PROGRAM TO BLACK DIAMOND GROUP IN THE AMOUNT OF \$649,265.10; SECONDED BY ALLEN; MOTION CARRIED 5-0.

# G. Consider a professional construction engineering inspection services agreement for the 2013 Paving Program.

Mike Spence:

Mr. President and members of the Board, this is related to the last item. Typically we do hire a consultant to assist with the inspection and staking on our paving program. We solicited proposals from two firms to do these services. Houle Enterprises gave us a proposal that had a lower hourly rate than Crispell-Snyder. They did the work for us last year. We were satisfied with their inspection. I should point out that that wasn't part of their job on 85th Street so that was something different.

But this indicates the services, basically they'd be involved in the preconstruction, some of the administration of the contract with Black Diamond. They do the staking and documentation and also provide full-time inspection and record the quantities of asphalt and do a final punch list. Based on this year's project the estimated total fee is \$21,196. That's based on an hourly basis what they think how long the contractor is going to take. If the paving contractor takes less that number will go down. It could conceivably go up as well, but generally this is estimated as conservative. So I recommend that this contract for Houle Enterprises for construction related services be approved. And I'd be glad to answer any questions.

Mike Pollocoff:

With the proposal by Crispell-Snyder do you have that?

#### Mike Spence:

The basic hourly rate was, let's see here, I can't find the cost here. It was basically their hourly rate, the Houle rate was \$64 an hour, and Crispell-Snyder was \$71. Crispell-Snyder actually estimated more hours, too. But I didn't necessarily take that into consideration because it will be whatever it is. So it's basically a difference in the hourly charge out.

#### Monica Yuhas:

I'll make a motion to award the professional construction engineering inspection services to Houle not to exceed \$21,196.

#### Steve Kumorkiewicz:

Second.

# John Steinbrink:

Motion by Monica, second by Steve. Any questions? Clyde?

#### Clyde Allen:

Thank you. Mike, have we ever used Houle Enterprises before?

# Mike Spence:

Yes, we used them last year. They did the inspection.

# Clyde Allen:

Okay, thank you.

# John Steinbrink:

Did they do the inspection on 94th?

#### Mike Pollocoff:

No.

#### John Steinbrink:

That was the developer?

#### Mike Pollocoff:

That was Nielsen, Madsen & Barber. That was the developer.

#### Mike Spence:

The developer's engineer.

John Steinbrink:

Okay, we have a motion and a second. Any further discussion?

# YUHAS MOVED TO APPROVE A PROFESSIONAL CONSTRUCTION ENGINEERING INSPECTION SERVICES AGREEMENT FOR THE 2013 PAVING PROGRAM TO HOULE ENTERPRISES IN THE AMOUNT OF \$21,196; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

H. Consider a professional services agreement for the site development of the Prairie Springs Pointe South property owned by the Community Development Authority consisting of 30 acres generally located west of 120th Avenue and south of STH 165.

Mike Spence:

Mr. President and members of the Board, this item is related to the land that's owned by the Community Development Authority. It's called Prairie Springs Pointe South. On the map on the screen there north is actually to the right. I-94 is just east of the site, and the site is located south of 165, Q, and then off the west frontage road. There's been a lot of interest in this property from developers since we've put it for sale. So in order to prepare this site for development we're pursuing a TEA Grant which is Transportation Economic Assistant Program Grant from the Wisconsin Department of Transportation. This is similar to what we did years ago for the Uline development. Basically the State has a grant program for developments that bring jobs to the area.

So as a result of all this we want to use that grant money, which we would have to match, to basically finish off the site for development for design and construction of the infrastructure. I requested a proposal from R.A. Smith National for the planning and design of the services. The scope that's included is we believe there may be some wetlands on site. There's going to be some surveying and will have to do some geotechnical investigation for both building development and roadway. There's floodplain in the area. They would also do the preliminary design and permitting, management of stormwater on site, and then the final design.

The infrastructure that we're talking about here is a public road. It would be 113th Street extended west, and then that would allow development of the property to the west here. So there would be a roadway coming in here which would open up access to this site. There is water and sewer adjacent to this site, but we would also put in -- we would extend that water and sewer to

the west, again, to allow development to the parcel to the west. And then also we would develop stormwater just south of the roadway here to handle the development of this site.

R.A. Smith is already on their own talked to the DNR and the Corps regarding some of the environmental issues. So they're very capable of this type of development. They put together a very good proposal. So it's for these reasons I'm asking that this contract with R.A. Smith to get going on the infrastructure for Prairie Springs Pointe South be executed. I'd be willing to answer any questions.

#### Monica Yuhas:

Mike, what is the time frame on this?

#### Mike Spence:

They would be getting going tomorrow once I give them the contract. We'd be looking at having construction plans by we're hoping late summer. And we actually could maybe possibly get construction started yet this year.

# Monica Yuhas:

Thank you.

#### Michael Serpe:

We're looking for it was in the past high end office and stuff in that area or whatever?

# Mike Pollocoff:

Yeah, we are talking with a potential user that would fit a good part of that proposal.

#### Michael Serpe:

Good. I don't now that we should talk about it tonight.

#### Monica Yuhas:

I'll make a motion to award the contract to R.A. Smith National in the amount of \$40,800.

#### Clyde Allen:

Second.

#### John Steinbrink:

Motion by Monica, second by Clyde. Any further discussion?

# YUHAS MOVED TO APPROVE A PROFESSIONAL SERVICES AGREEMENT TO R.A. SMITH NATIONAL IN THE AMOUNT OF \$40,000 FOR THE SITE DEVELOPMENT OF THE PRAIRIE SPRINGS POINTE SOUTH PROPERTY OWNED BY THE COMMUNITY DEVELOPMENT AUTHORITY CONSISTING OF 30 ACRES GENERALLY LOCATED WEST OF 120TH AVENUE AND SOUTH OF STH 165; SECONDED BY ALLEN; MOTION CARRIED 5-0.

# I. Consider approval of a professional services agreement for the Village's Hydraulic Model Calibration.

Mike Spence:

Mr. President and members of the Board, I've got a few slides here just to kind of explain the process and what we're going through with the Village's hydraulic model. Where we're at right now, well we've previously identified our hydraulic modeling needs for our water system. Then last year we completed the step in tying our model to our GIS system which is really critical. So now when our IT department, our GIS puts new water mains in this system it will automatically be updated in the model.

The next step that I want to talk about today is the model calibration. There's a number of benefits for our model. I mean we've been using our model, but we had to coordinate it with our GIS. It hadn't been done. The other thing that we're talking about tonight is the actual calibration, and that's the verify that the results that we get from the modeling match the reality in the system. This is important when we're doing fire flow calculations. I won't read them all, but there's a number of applications why this modeling is important. It's important for planning, I mentioned fire flow. For our public works director in terms of maximizing the efficiency and operation of the system it's very important.

And we'll be tying this with our SCADA system which is the system that indicates when pumps are pumping and what's in operation. So there's many applications for the model. And, again, I had mentioned our model is called Infowater which is now called Inovise. And this is what we completed last year in terms of getting our model where it is at this point. The next step, again, is the calibration so we can match the actual conditions with the existing. And that includes looking at flow rates, pressure in the system, looking at storage facility levels and pumping.

The way we're going to go about calibration with the model we'll do field testing including fire flow and pressure testing. We'll also do looking at the system with the SCADA system data as well as pressure recorders. And then we also will be looking at importing water usage data so that the actual water usage fits to where the pipes are in the system. And then finally we can do what's called an extended period calibration where it's not just the snapshot. You can run it for a period of time, and you can see how the system is operating.

Again, these are different costs with modeling. I just wanted to point out, and a lot of these we've already done. We've got our SCADA system, and we're going to be doing the field testing. Modeling system costs were pretty much in line with other communities our size. We spent

about \$40,000 so far getting the model to where it is now. And the consultant that's going to assist us with the calibration is at \$37,000. So that's where we're at.

Okay, I requested proposals from a number of firms. I asked for recommendations from the company that has our software who has the knowledge. We got a proposal from Advanced Engineering and Environmental Services AE2S. They've got over 200 engineers including many hydraulic modeling engineers. Their primary service is drinking water. They're very experienced in distribution system modeling and calibration. Again, they were recommended by our software provider as an expert.

The fee of \$45,500 includes onsite training. That's why that number is higher than the one I just showed you. I can also tell you that I've been working with the software company to get that number reduced. I found out today that they're going to knock another \$1,000 off it. It has to do with the licenses for training. They get you, and we need a certain amount of -- because we want to make sure public works gets trained in using the models because they're going to use it quite extensively.

This is just an example of an output from the model. And I apologize it's hard to read, but what we're looking at right here is this is Springbrook, this is 165, this is the Village Green Heights neighborhood. This is Cooper Road. And if you could see it better you have the water main and the nodes for the fire hydrants and that. And then on the sides it has the data and the information that you get when you model. This just kind of an example of the output. Schedule-wise for this project we're looking at looking at the water demand and hourly demands on the model, doing that yet this month. Then we'd be doing field testing in June and basically completing the calibration in July. That's it.

So what you have tonight before you is the contract from AE2S to assist the Village with calibration. They'll be actually assisting us. They have all the equipment. They'll be doing the pressure testing and the field testing in the field. And then also providing us training on our model with the software.

Michael Serpe:

Can I ask Mike a question? Is this a one shot, one time experience? Do we have to do this every year?

Mike Spence:

No, this is a one shot deal. Once we allocate the flows in the system and we tweak everything we'll have a very good idea that the model is giving us the results that you have in the field. At that point between John and I we'll understand the model and the system. We really shouldn't have to calibrate it again unless -- I can't think of a reason. I mean once we keep up with everything it's a one time deal.

#### Mike Pollocoff:

What this does is we've gone through the initial hearings for the water rate increase. And Kenosha said that your system wasn't operating right and they would like to model it. They have modeled it and they know that's why it doesn't work right. We asked them for the model of theirs and they said we don't have a model. And right now, this really must be a boring meeting, if you look at \$40,000 or \$45,000 it sounds like a lot of money, but when you can take and operationally decrease your electricity costs, make sure that you size the mains correctly when you're putting in new mains, optimize how your pumps are going to be running, match this up with the SCADA system, it's an expense that really helps you keep your costs down and drive your costs down even further.

And John has been able to accomplish that with the SCADA system. And with having this as part of the other tool we'll be complete, and that should be a system that we have that Kenosha doesn't have, or at least they say they don't have. They might have one at their desk or in a waste basket, but this will enable us to do the things we need to do to make the utility as effective as we need to have it be. And, as Mike said, it's going to be public works and engineering they're all going to be up to speed on this to get the biggest bang for the buck out of it.

# Mike Spence:

If I could add one more thing, the other thing that is important is that now that development is starting to increase a little bit this really becomes a great tool. Because when and if we start getting another subdivision plat we can really have a good idea what that impact on the system is. And if we need to require them to beef up a line coming to the site or whatever it's a valuable tool.

# Michael Serpe:

What are you looking for, Mike, the \$45,000 approval or \$44,000?

#### Mike Spence:

I can tell you right now that it's going to be \$1,000 less.

#### Michael Serpe:

You want the \$45,000.

#### Mike Spence:

That's what it actually says in the contract.

#### Michael Serpe:

So moved.

#### Steve Kumorkiewicz:

Second.

#### John Steinbrink:

Motion by Mike, second by Steve. Any further discussion? You compared this to the company we get our software with. Were there other bids?

#### Mike Spence:

They're the only ones -- the other firm I talked with was out of State as well. These people are actually from North Dakota. The other firm was from North Carolina, and they didn't want to come here. But I did some reviewing around other prices and this seems competitive.

#### John Steinbrink:

Is that's the case. We have a motion and a second.

# SERPE MOVED TO APPROVE A PROFESSIONAL SERVICES AGREEMENT WITH ADVANCED ENGINEERING AND ENVIRONMENTAL SERVICES IN THE AMOUNT OF \$45,000 FOR THE VILLAGE'S HYDRAULIC MODEL CALIBRATION; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

# J. Consider Ordinance 13-14 to amend Chapter 292 of the Village Municipal Code relating to hazardous waste collection.

John Steinbrink, Jr.:

Mr. President and members of the Board, hazardous waste has always been funded through a grant from State of Wisconsin, and it was awarded to Veolia. And we worked in conjunction with Kenosha County. We always had two days a year, one in the spring and one in the fall where we would collect -- one of the hazardous wastes would be at Kenosha County in May, and then the first Saturday in September we would have it at the Prange, and it was like that for a number of years.

Beginning last year the State took the grant away and only funded the amount for Kenosha County's hazardous waste collection. And they're kind of leaving us in the dark after we had published it. So we had gone ahead and paid for the hazardous waste collection in the fall just out of the solid waste fund. But then we put together a new program to fund it, and that's what we will be talking about this evening.

So Kenosha County is still going to have one day each year, the first Saturday in May which we just had this past Saturday, to collect household hazardous waste collection. Everything is listed above on one of the previous screens and the next screens. And then the Village had gone

through and we purchased the hazardous storage collection hut, all the necessary training our crews have been through in conjunction with the fire department. So we're actually looking at collecting it on the first Saturday of every month from April through December at the Prange Center. So it's much more convenient for all the residents.

So we're just looking to change the ordinance a little bit, just some minor wording. So we'll be collecting one day through that grant of the State to Kenosha County, they'll be at Highway 50 and 45. And then the first Saturday of every month from April until December we'll be collecting at the Prange with our own staff. We've entered into a contract with Veolia services to haul the stuff away at a per pound basis depending on what comes in. Whether it's light bulbs, oil based paint. We really don't have any nuclear waste or we really don't collect anything like that. It's just your standard household hazardous waste. And it's just in an environmentally safe way to dispose of. And we felt it was important or the Board felt it was important over the last budget cycle to fund it once a month. So this is just increasing our collection dates to once a month, and then just updating the ordinance to reflect that.

#### Monica Yuhas:

John, on the ordinance you crossed out tires.

John Steinbrink, Jr.:

Yeah, tires are collected at our compost site for a fee. I believe it's \$5 a tire. So it's the same that it's been in the past. So we thought while we were updating the ordinance now would be a good time just to update that. So we still recommend that people -- when you have your tired changed a lot of times they'll take it for \$2 or \$3 at the tire place. We still recommend doing that. But if someone has some tires in a garage or if they choose to hang onto them and dispose of them it's still available for them. So we're just updating the ordinance on that with the tire disposal.

Monica Yuhas:

Thank you.

#### Clyde Allen:

I make a motion to approve.

# Michael Serpe:

Second.

#### John Steinbrink:

Motion by Clyde, second by Mike. Any further discussion?

# ALLEN MOVED TO ADOPT ORDINANCE 13-14 TO AMEND CHAPTER 292 OF THE VILLAGE MUNICIPAL CODE RELATING TO HAZARDOUS WASTE COLLECTION; SECONDED BY SERPE; MOTION CARRIED 5-0.

Michael Serpe:

Good idea, John.

John Steinbrink, Jr.:

Thank you.

# K. Consider Resolution #13-10 designating May 19-25, 2013 National Public Works Week.

#### Mike Pollocoff:

I think this previous item was a good description of what a progressive, innovative public works can do. John took what became an unfunded mandate and found a way to make it work and work it within the existing budget we have. Once the State quit paying for it, it didn't mean that stuff was going away, and so he found a way to get it collected and managed. And our public works department has been able to do that the last couple years as things have been cutting back and finding ways to make things happen, whether it's water, sewer, streets, highway, public buildings and solid waste collection.

And I can't think of hardly any complaints we get about our solid waste collection, street snow plowing and taking care of things in the winter. I was at a meeting last night where people were amazed that -- they weren't amazed, they were really glad that some of the stormwater improvements we put in in Unit 2 were working. So we have a really good public works department that we can all be proud. I think that makes this resolution more meaningful to recognize national public works week.

Michael Serpe:

Mike, I have to agree with you. I guess the great thing about this Village is the employees that service the people. And so far, John, since you've taken over that position I mean the service to the residents has been outstanding. We're constantly getting compliments from garbage pickup to snow plowing to after snow plowing when some gravel was pushed up to the side of a dead end road. Public works goes out there and brings it all smoothed out again. Just a fantastic amount of service being done by John's department. I can't say enough good about it. Congratulations to all you guys. With that move approval of 13-10.

Steve Kumorkiewicz:

Second.

#### John Steinbrink:

Motion by Mike, second by Steve. Any further comments? And I have to say, too, when I'm out and about in the community or in any of the stores or businesses people come up and make comments for the job we do with our streets, with our snow plowing, with our garbage collection and with your leaf pickup. And those are things people really appreciate and take notice to. Clyde, you did ask will there be cake for this week, will there be a party?

John Steinbrink, Jr.:

We are going to do a lunch in celebrating it. We'll make sure we invite all the department heads and the Board and anyone else that would like to attend also.

John Steinbrink:

Alright, I believe we had a motion and a second. Was it voted already? Alright, we're done with that one.

# SERPE MOVED TO ADOPT RESOLUTION #13-10 DESIGNATING MAY 19-25, 2013 NATIONAL PUBLIC WORKS WEEK; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

# L. Consider disallowance of a claim for excessive assessment filed by Target Corporation.

Rocco Vita:

Good evening, Mr. President and Board. I'm Rocco Vita, Village Assessor. On the agenda tonight and before you is a packet concerning a claim for excessive assessment filed by the Target Corporation against the Village of Pleasant Prairie. I'm going to read my memo to you for the record.

Target Corporation has filed a Claim of Excessive Assessment against the Village of Pleasant Prairie related to the 2012 assessed value of their retail property located at 9777 76th Street in the Village of Pleasant Prairie. The 2012 assessed value as determined by the Village Assessor is \$12,181,300. Target Corporation requested and received a hearing before the 2012 Pleasant Prairie Board of Review. The board of Review sustained the value determined by the assessor's office. Applying the Village's 2012 net tax rate of \$21.20, less the first dollar credit, results in a 2012 net property tax of \$258,185. Target Corporation claims the 2012 fair market value of their property is \$8,000,000 and claims a refund of no less than \$87,760 plus any interest as provided by law.

And before you decide upon this, I'm going to give you a short inclination on what the appeal process is like under Wisconsin assessment law. All appeals start with contacting the assessor's office and talking to us about the assessed value. And I think in your packets I provided you our

Board of Review testimony. And in that Board of Review testimony on page three that looks something like this, is this loaded into the -- it is not, okay.

When contacted by the property owner's tax representative they go over with our appraiser all the details regarding the valuation of the property. In our conversations with Target this year we agreed with everything about the valuation process except for a potential lease rate. We use a potential lease rate if Target didn't own this what the property owner might lease that property for. And we provided them a list of the rent comparables from that project and other large facilities like Gander Mountain and like Best Buy nearby to give us an indication of what leasing these size properties are.

And the representative says, well you know, that's good information, I think it's a little too high but I'm going to get back to you with what we believe that you can consider in the valuation process. And we said fine, that's great. They never got back to us. I had contacted them again, they never got back to us until to apply for the Village's Board of Review hearing which is everybody's right. So we've gone through the process. We shared with them everything we have. They said, well, it looks really good except for this one item, we'll get back to you and they never do.

They come to the Board of Review hearing, and Ed goes through his testimony which you see in your presentation there is a number of pages, color pictures, a lot of information, three approaches to value, an income approach, a cost approach and a sales comparison approach. He provides them a great deal of information. The Board, he provides them the Board a great deal of information for the Board to make the kind of decision they're empowered to make. Target comes to the hearing and presents one page of very tiny text, difficult to read. Now, when Ed makes his presentation it encompasses about five or six pages, and I included this in there for you to see, five or six pages of testimony for the Board. Target's representative provides this one page of micro text and one paragraph of testimony.

Well, based on this information the Board of Review has to make a determination on which value is correct based on the information that they received. Ed's information, lots of pages, lots of information for them to use, lots of easy to understand information, or Target's information, something maybe not so easy to understand. So the Board finds in favor of the assessor. I didn't think this was all that boring, but it's my game.

Now, in Wisconsin you can appeal two ways from the Board of Review. You can go directly to circuit court, I think you file like within 60 days or 30 days after the hearing, and circuit court at that point listens to the testimony provided at the Board of Review hearing and determines whether the Board made a proper decision based on what it heard, okay? Well, clearly if they would have done this Target would have -- the assessor's office would have been supported.

Or, they can do what's called filing a claim for excessive assessment in Wisconsin. They wait until they pay a portion of their tax bill, and then they file a claim with you, the Village Board, saying we've been unfairly assessed, and we want you to pay this claim, this amount. And if you don't we'll sue you in circuit court. And it's not based on any evidence provided, it's called de novo all over again. So they get their team of lawyers and appraisers and tax consultants. The

Village has to hire its team of lawyers and appraisers and tax consultants, and we prepare to go to war in circuit court, something that costs the community about \$125,000 for each one of these cases, okay?

This is something that's occurring with greater frequency each and every year in Wisconsin. I'm kind of on by soapbox here but this gives me a good opportunity. Target Corporation doesn't really care about the Village of Pleasant Prairie. Target Corporation really only cares about themselves. They provided minimal information at the local Board of Review hearing just to say that they've been there. They can then circumvent that whole process, file a claim for excessive assessment with us. You're going to deny them because you have no choice . You're in the same position the local Board of Review was provided this amount of information. And so Target will say, well fine, we're going to start sucking money from you through this claim process hoping that you negotiate or your attorneys eventually negotiate with them. It has nothing to do with the value of the property was right, it has everything to do with forcing you into a financial and economic decision to negotiate a lower value.

And while we were successful in our most recent claim with the assisted living facility, most communities opt to negotiate. And this method is clearly a law that needs to be reworked in the books. And sooner or later communities like you and people like you and boards like you need to begin talking to legislators about this occurrence. It's not just happening in this instance here in Pleasant Prairie, it's happening over and over again in a lot of communities that care about their property values, and in other areas not so much.

The whole point of this is that if Target really cared they would have worked with us up front but they didn't care. So it's the assessor's recommendation that the Board disallow the claim for excessive assessment and allow us the opportunity to deal with it in Seventh Circuit Court.

# Michael Serpe:

You know, Rocco, I have to agree. If we get into a negotiated settlement that word is going to go through the commercial and industrial community like wildfire, and we'll be making negotiation settlements constantly. I think in something like this I think you have to be prepared to spend the money, to take the time, and if you feel that strongly and that good about your appraisal of Target then I think we stand behind you and take this all the way. I wouldn't negotiate this because you're just opening up the door for future --

#### Rocco Vita:

I don't think it has as much to do with our value, it has totally to do with them and their process. Whether the value is correct -- our initial value was \$13.1 million. Then the market crashed. We reduced it 14 percent to about \$11.8 million. Now, because things got better and this shopping center right there sold for \$25 million excluding Target and excluding Penneys, we increased the value 3 percent to reflect a better commercial market here. And now they're saying we want \$8 million, it's only worth \$8 million. Their point is not valuation or good valuation or being a good commercial customer or participant in the Village's processes or in the Village's way of life.

They're just circumventing this and holding the Village at an economic disadvantage in trying to obtain a lower tax rate.

#### Michael Serpe:

I don't know how many people or lawyers out there that have more knowledge about this subject matter than you do.

# Rocco Vita:

There's a lot of them.

#### Michael Serpe:

Well, I think you handle yourself quite well. You could probably handle this case by yourself, too.

#### Mike Pollocoff:

This is really fairly insidious. Because what happens is this is just another way where corporations can use this process, in this case it's \$74,000, and that \$74,000 is going to come out of somebody's pocket. The fair allocation of it is in this case Target just like it was in some of the other ones. But they don't care if it comes out of the residential taxpayers' pockets because that's where it happens. The pot gets smaller and the residential people pay more. They don't care. This is just another way to make money. And there really hasn't been a lot of desire or need in the legislature up to now to even address this. We're open for business. This is one of the side effects.

# Clyde Allen:

I'm going to make a motion to disallow the claim for excessive assessment filed by Target.

Michael Serpe:

Second.

John Steinbrink:

Motion by Clyde, second by Mike. Any further discussion?

#### Steve Kumorkiewicz:

I know where I'm not going to shop any longer.

#### John Steinbrink:

Will there be an editorial in our newsletter?

#### Rocco Vita:

If Chris can get this down to the second or third grade level maybe perhaps.

# John Steinbrink:

I thought that's what you gave to the Board.

#### Steve Kumorkiewicz:

To me it's very simple. If the assessor has set it lower in one area he's going to [inaudible] someplace else. That means the residential will have to pay the difference.

#### Rocco Vita:

Every property owner will pay the difference.

Steve Kumorkiewicz:

Exactly.

John Steinbrink:

We have a motion and a second.

# ALLEN MOVED TO APPROVE THE DISALLOWANCE OF A CLAIM FOR EXCESSIVE ASSESSMENT FILED BY TARGET CORPORATION; SECONDED BY SERPE; MOTION CARRIED 5-0.

#### M. Consider Commission Appointments.

Mike Pollocoff:

Mr. Chairman, I recommend for the Plan Commission that Thomas Terwall, Wayne Koessl and Larry Zarletti be appointed with terms to end May 1, 2016. Judy Juliana and Andrea Rode with terms to end in 2014. Park Commission I recommend that Michealene Day, Glenn Christiansen and Bill Mills and Clyde Allen -- I'm sorry, the first three, that they be reappointed with terms beginning May 1, 2015. I'm recommending Clyde Allen be appointed to the Park Commission with his term beginning June 1, 2015. And Cindy Schwab and Jim Bandura their terms would be May 1, 2014.

On the Rec Commission I'm recommending that Greg Scheppler, Kate Field and John Skalbeck be reappointed to terms to May 1, 2015, and Dino Laurenzi as their sole alternate at 2014. The Board of Appeals I'm recommend that Christine Genthner be reappointed for a term ending in 2016, and a new appointee Deb Skarda whose term would also be to May 1, 2016. On the Board of Review we'd reappointing Lena Schlater for a term that would end May 1, 2018, that's a long term. And then James Kennedy who's an alternate would fill the unexpired term of John Burke who resigned. And that term would end May 1, 2017. And then to the KACVB myself, Larry Nelson and Carol Willke with terms that would expire May 1, 2016.

#### Michael Serpe:

Move approval on all.

Monica Yuhas:

Second.

#### John Steinbrink:

Motion by Mike, second by Monica. Any discussion on any of the appointments?

# SERPE MOVED TO APPROVE THE COMMISSION APPOINTMENTS AS FOLLOWS:

#### **Plan Commission**

Thomas W. Terwall	Term – May 1, 2016
Wayne E. Koessl	Term – May 1, 2016
Larry Zarletti	Term – May 1, 2016
Judith A. Juliana (Alternate #1)	Term – May 1, 2014
Andrea Rode (Alternate #2)	Term – May 1, 2014

#### Park Commission

Michealene Day	Term – May 1, 2015
Glen Christiansen	Term – May 1, 2015
William Mills	Term – May 1, 2015
Clyde R. Allen	Term – May 1, 2015
Cindy Schwab (Alternate #1)	Term – May 1, 2014
Jim Bandura (Alternate #2)	Term – May 1, 2014

#### **Recreation Commission**

Gregory J. Scheppler	Term – May 1, 2015
Kate Field	Term – May 1, 2015
John D. Skalbeck	Term – May 1, 2015
Dino Laurenzi (Alternate #1)	Term – May 1, 2014

Village Board Meeting May 6, 2013

### **Board of Appeals**

Christine Genthner	Term – May 1, 2016
Deb Skarda	Term – May 1, 2016

### **Board of Review**

Lena Schlater	Term – May 1, 2018
**James Kennedy	Term – May 1, 2017

\*\* James Kennedy will fill unexpired term for John Burke who resigned. Alternate position is currently open.

### Kenosha Area Convention and Visitor's Bureau

Michael Pollocoff	Term – May 1, 2016
Larry Nelson	Term – May 1, 2016
Carol Willke	Term – May 1, 2016

### **SECONDED BY YUHAS; MOTION CARRIED 5-0.**

### N. Consider Operator License Applications on file.

Jane Romanowski:

Two applications tonight for Tamara A. Nickel and Jenny Wilson. And they both meet the requirements as outlined in the ordinance.

Clyde Allen:

Move approval.

Steve Kumorkiewicz:

Second.

John Steinbrink:

Motion by Clyde, second by Steve. Any discussion?

# ALLEN MOVED TO APPROVE OPERATOR LICENSES FOR TAMARA NICKEL AND JENNY WILSON; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

Village Board Meeting May 6, 2013

#### 10. VILLAGE BOARD COMMENTS

John Steinbrink:

Just one. Chief, you had sent down a notice that Wednesday is the law enforcement memorial down at the Civil War Museum, Wednesday the 8th noon followed by a lunch.

### Chief Smetana:

The National Peace Officer's week is celebrated coming up next week. And in honor of that as an annual ceremony the Kenosha Police Department puts together, along with other law enforcement agencies in the County, a memorial service just north of the library I believe at the law enforcement memorial area near the Civil War Museum.

### John Steinbrink:

Harbor Park by the Civil War Museum. Between the museums I guess we could say.

### Chief Smetana:

And the program that was sent to you didn't have a time. It does start at noon, and you are all welcome. So it would be an honor to have you there.

### Michael Serpe:

Chief, one comment. I'm sorry I won't be able to come, I'll be in Atlanta. But on the horse farm you guys did a hell of a job.

### Chief Smetana:

Thank you.

### Michael Serpe:

I mean it wasn't easy to come across something like that and to handle it the way you did. That's been the talk of the community for quite a while and it still is.

### Chief Smetana:

Right, we're in the midst of it. We are dealing with it on a daily basis. It still isn't wrapped up and it probably won't be for quite some time. I can't speak highly enough for the officers and investigators who were involved in it. A great deal of -- I made the comments to many people, and I may have made it to some of you, I've been in police work for 30 years and that was the most complex involved search warrant I've ever been on. And the most complex involved operation. I can't remember ever being on a three day search warrant before. And we just Village Board Meeting May 6, 2013

wrapped that up on Thursday. So we look forward to the cooperation of the community and the cooperation from the rest of the justice system on that as well.

### John Steinbrink:

And if we didn't say it before, welcome to Pleasant Prairie.

### Chief Smetana:

And it's still a pleasure to be here.

### John Steinbrink:

The good thing is I turn on the news and I know you're here, but now everybody around the State knows you're here.

### Chief Smetana:

And all of my relatives now know I'm here. Thank you all very much.

### John Steinbrink:

Good job on everything, Chief.

### Chief Smetana:

Thanks for your support.

### John Steinbrink:

Any other Village Board comments?

### 11. ADJOURNMENT

# YUHAS MOVED TO ADJOURN THE MEETING; SECONDED BY ALLEN; MOTION CARRIED AND MEETING ADJOURNED AT 7:35 P.M.



# **MEMORANDUM**

TO: Village Board Trustees

FROM: Jane M. Romanowski Village Clerk

DATE: May 15, 2013

RE: Liquor License Renewal Applications July 1, 2013 - June 30, 2014

Listed below are the liquor license applications to be considered for the July 1, 2013-June 30, 2014 licensing year.

### CLASS "A" FERMENTED MALT BEVERAGE

### NAME & ADDRESS

TRADE NAME

Roadside Petroleum, Inc. Surendra Singh, Agent 7511 118<sup>th</sup> Avenue Pleasant Prairie, WI 53158 Pantry 41 Citgo 7511 - 118<sup>th</sup> Avenue Pleasant Prairie, WI 53158

Premise Description: One story building only located at 7511 118<sup>th</sup> Avenue

PDQ Food Stores, Inc. Kathy Loberger - Agent P.O. Box 620997 Middleton, WI 53562 PDQ Store #352 8800 - 75<sup>th</sup> Street Kenosha, WI 53142

Premise Description: One story building only located at 8800 75<sup>th</sup> Street

Graham Enterprise, Inc. Anthony Buches - Agent 12720 Sheridan Road Pleasant Prairie, WI 53158 Stateline Citgo 12720 Sheridan Road Pleasant Prairie, WI 53158

Premise Description: One story building only located at 12720 Sheridan Road

Truesdell Mini-Mart, Inc. Steve Schuler - Agent 7831 45<sup>th</sup> Avenue Kenosha, WI 53142 Truesdell Mini-Mart 8531 75th Street Kenosha WI 53142

Premise Description: One story building only located at 8531 75<sup>th</sup> Street

Walgreens Co. Patricia Briggs - Agent P.O. Box 901 MS 3301 Deerfield, IL 60015

Walgreens #07935 7520 118<sup>th</sup> Avenue Pleasant Prairie, WI 53158

Premise Description: One story building only located at 7520 118<sup>th</sup> Avenue.

### CLASS "A" FERMENTED MALT BEVERAGE AND "CLASS A" INTOXICATING

### NAME & ADDRESS

Racine, WI 53402

### Ayman LLC Akil Ajmeri, Agent 2714 4 ½ Mile Road

Ayra's Liquor Store 4417 75<sup>th</sup> Street Kenosha, WI 53142

TRADE NAME

Premise Description: One story building only located at 4417 75<sup>th</sup> Street

### **NOTE:** Community Development Dept. – Landscaping and parking lot patching

Target Corporation Shane Richard Chesick – Agent 1000 Nicollet Mall TPN-0910 Minneapolis, MN 55403 Target Store T2251 9777 76<sup>th</sup> Street Pleasant Prairie, WI 53158

Premise Description: Indoor first floor located at 9777 76<sup>th</sup> Street

### **CLASS "B" FERMENTED MALT BEVERAGE**

### NAME & ADDRESS

Jose N. Reyes - Agent Timber Ridge Ventures LLC 6117 123<sup>rd</sup> Place Pleasant Prairie, WI 53158 Big Oaks Golf Club 6117 123rd Place Pleasant Prairie, WI 53158

TRADE NAME

Premise Description: One story clubhouse and golf course located at 6117 123<sup>rd</sup> Place

### CLASS "C" WINE AND CLASS "B" FERMENTED MALT BEVERAGE

### NAME & ADDRESS

### Honada Sushi Corp. Lizhu Cao, Agent 8501 75<sup>th</sup> Street, Suite G Kenosha, WI 53142

Honada Sushi & Hibachi 8501 75<sup>th</sup> Street, Suite G Kenosha, WI 53142

TRADE NAME

Premise Description: One story building only located at 8501 75th Street, Suite G

### CLASS "B" FERMENTED MALT BEVERAGE & "CLASS B" REGULAR INTOXICATING LIQUOR

### NAME & ADDRESS

### TRADE NAME

Restaurant of Pleasant Prairie, Inc. Adam Modrow, Agent 7613 W. State Street Wauwatosa, WI 53213 Chancery Pub & Restaurant 11900 - 108<sup>th</sup> Street Pleasant Prairie, WI 53158

Premise Description: Restaurant at 11900 108<sup>th</sup> Street and hotel rooms and banquet facilities located at 11800 108<sup>th</sup> Street

ERJ Dining III, LLC Paul Thompson – Agent 1903 Stanley Gault Parkway Louisville, KY 40223 Chili's Grill & Bar 6903 – 75<sup>th</sup> Street Kenosha, WI 53142

Premise Description: One story building, excluding parking lot, located at 6903 75<sup>th</sup> Street

Earl's Club, Inc. John C. Willkomm - Agent 3490 196<sup>th</sup> Avenue Kenosha, WI 53144 Earl's Club 7529 88th Avenue Pleasant Prairie, WI 53158

Premise Description: One story building, basement, deck and horseshoe/volleyball/picnic area contiguous to building, excluding parking lot, located at 7529 88<sup>th</sup> Avenue

\*Team R' n B Wisconsin LLC Peter Benedict - Agent 6600 N. Ballard Road Appleton, WI 54913 Famous Dave's 9900 77<sup>th</sup> Street Pleasant Prairie, WI 53158

Premise Description: One story building including outdoor covered fenced-in patio located at 9900 77<sup>th</sup> Street

\*Reserve "Class B" Intoxicating Liquor License

Prairie Pub LLC Linda DeBartolo - Agent 8217 60<sup>th</sup> Avenue Kenosha, WI 53142 Gordy's Prairie Pub 3812 Springbrook Road Pleasant Prairie, WI 53158

Premise Description: First floor and basement of building and picnic area south/east contiguous to building located at 3812 Springbrook Road.

Halter Wildlife, Inc. John F. Burke - Agent 9626 - 113th Street Pleasant Prairie, WI 53158 Halter Wildlife 9626 113<sup>th</sup> Street Pleasant Prairie, WI 53158

Premise Description: Two story lodge and storage shed located at 9626 113<sup>th</sup> Street

\*GMRI, Inc. Jeffery Zimmerman – Agent 1000 Darden Center Drive Orlando, FL 32837 The Olive Garden Italian Restaurant #1845 10110 77<sup>th</sup> Street Pleasant Prairie, WI 53158

Premise Description: One story building located at 10110 77<sup>th</sup> Street \*Reserve "Class B" Intoxicating Liquor License

Ray Radigan's Inc. R. Michael Radigan - Agent 10510 Lakeshore Drive Pleasant Prairie, WI 53158 Ray Radigan's 11712 Sheridan Road Pleasant Prairie, WI 53158

Premise Description: Building only located at 11712 Sheridan Road

NOTE:	Delinquent Utility Bill	\$2,976.33
	Delinquent 2012 Real Estate Taxes	\$4,276.44
	Delinquent 2012 Personal Property Taxes	\$ 968.06
	Delinquent Sanitary Sewer Surcharge	\$ 223.96

Frank J. Ruffolo 6218 - 31st Street Kenosha, WI 53144 Ruffolo Special Pizza III 11820 Sheridan Road Pleasant Prairie, WI 53143

Premise Description: One story building and basement, excluding parking lot, located at 11820 Sheridan Road

JAAD, LLC Angela Daniels - agent 8934 33<sup>rd</sup> Avenue Kenosha, WI 53142 Starlite Club 8936 24th Avenue Kenosha, WI 53143

Premise Description: One story building and basement excluding parking lot, located at 8936 24<sup>th</sup> Avenue

### NAME & ADDRESS

### TRADE NAME

Uncle Mike's Top Shelf Pub LLC David Schulte - Agent 8834 42<sup>nd</sup> Avenue Kenosha, WI 53142 Mikey's 10936 Sheridan Road Pleasant Prairie, WI 53158

Premise Description: Building, basement and outdoor patio/picnic area within fencing contiguous to building, excluding parking lot, located at 10936 Sheridan Road

### NOTE: Delinquent 2012 Personal Property Tax \$34.33

PAS Village Inn LLC Susan Neahous – Agent 10909 Sheridan Road Pleasant Prairie, WI 53158 The Village Supper Club 10909 Sheridan Road Pleasant Prairie, WI 53158

Premise Description: First and lower level of building, excluding parking lot, located at 10909 Sheridan Road

Joseph A. Nickel 5813 43<sup>rd</sup> Avenue Kenosha, WI 53144 The Wooden Nickel 11606 Sheridan Road Pleasant Prairie, WI 53158

Premise Description: One story building and picnic area contiguous to building, excluding parking lot, located at 11606 Sheridan Road

NOTE:	Delinquent Utility Bill	\$ 771.24
	Delinquent 2012 Real Estate Taxes	\$1,948.37
	Delinquent 2012 Personal Property Taxes	<b>\$ 159.56</b>

The premise descriptions for all establishments have not changed from the previous licensing year. I recommend approval of the renewal of the licenses presented subject to the correction of any outstanding building, fire and zoning violations and the payment of license fees, publication fees and any delinquencies of property taxes, personal property taxes, utilities and invoices prior to the issuance of any license.

The renewal application for the Holiday Inn Express, 7887 94<sup>th</sup> Avenue, was submitted late and will be on the June 3, 2013 Village Board meeting.

The license renewal for BP AM/PM, 10477 120<sup>th</sup> Avenue, will not be considered by the Village Board until after a determination is rendered by the Plan Commission on May 24<sup>th</sup> with respect to its conditional use violations and tentatively will be on the June 17<sup>th</sup> Village Board agenda.

\*\*\*\*



# DIXON

ENGINEERING AND INSPECTION SERVICES FOR THE COATING INDUSTRY 1104 Third Avenue Lake Odessa, MI 48849 Telephone 1-616-374/3221 Fax 1-616-374/7116

May 10, 2013

Mr. John Steinbrink, Jr. Village of Pleasant Prairie 8600 Green Bay Rd. Pleasant Prairie, WI 53158-2709

Re: Bids Taken for 750,000 Gallon Spheroid Elevated Tank Repainting - Recommendation for Award

Dear Mr. Steinbrink:

Dixon Engineering has reviewed the bids submitted for repainting and repairs to the Village's 750,000 gallon elevated tank, and recommends award to the low bidder, L & T Painting of Clinton Township, MI, for the bid amount of \$304,800. This includes all line items, and this bid price is very competitive. Of the four (4) bids received, the L & T bid is \$18,100 below the second bidder. The third and fourth bidders consistently bid higher hoping for a sleeper until the project they want comes out and then they become competitive. L & T Painting was prequalified with the Village and also with Dixon for this scope of work.

Currently Sprint is changing out their equipment and will only need three cables, BUT each of the cables will have a  $5 \times 12$ -inch cylinder integral at the top. If we eliminated the pipe, the access tube would be modified to allow 6 inch conduits out the side. That would save them about \$5,000 but limit future development. They are considering purchasing conduit for future use. Cricket will be given the same opportunity. There is no future development for either carrier without the conduit.

Enclosed for your review and action are three copies of the Notice of Award. After City approval, please sign and forward all three copies to the contractor for signature. Instruct the contractor to return one copy to you, forward a copy to our office, and retain a copy for his files.

Also, please notify the contractor that he is to forward to our office the required bonds and certificate(s) of insurance for inclusion in the Contract Documents.

Additional changes from the carriers will be addressed by Bulletin/Change Order.

A letter concerning the extra bid items for antenna modifications will follow shortly.

If you have any questions regarding our recommendation, please contact me at (616) 374-3221, Ext. 305.

FOR DIXON ENGINEERING, INC.,

William J.I inon/14

William J. Dixon, P.E.

Members: Steel Structures Painting Council American Water Works Association Consulting Engineers Council

## **Bid Tabulation**

Project Name: 2013 LakeView Booster Painting

Owner: Village of Pleasant Prairie

County: Kenosha

	CONTRACTOR	L and T Painting	LC United	TMI Coatings	Classic Protective Coating
	BID SECURITY	10%	10%	10%	10%
	BID RANK (1= lowest)	1	2	3	4
No.	Item	Total	Total	Total	Total
A1.	Replace Access Tube Hatch	\$5,000	\$5,000	\$7,600	\$4,900
2	Mud Valve	\$4,500	\$4,000	\$5,200	\$5,900
3	Fall Prevention Devices	\$7,000	\$6,000	\$7,000	\$9,900
4	Painter's Rail	\$3,000	\$4,000	\$9,400	\$11,500
5	Aviation Lights	\$1,800	\$6,500	\$1,700	\$3,200
6	Antenna Conduit	\$16,000	\$4,500	\$20,500	\$125,000
7	Antenna Cable Brackets	\$3,400	\$24,000	\$4,600	\$15,000
9	Subtotal- Part A- Items 1 thru 7	\$40,700	\$54,000	\$56,000	\$175,400
B1.	Exterior Overcoat	\$80,000	\$119,000	\$174,400	\$244,328
2	Wet Interior RePaint	\$130,100	\$109,000	\$212,300	\$125,000
3	Dry Interior Partial Repaint	\$24,000	\$18,000	\$24,800	\$75,000
9	Subtotal- Part B- Items 1 thru 3	\$234,100	\$246,000	\$411,500	\$444,328
C1.	Electircal upgrades	\$10,000	\$7,800	\$20,000	\$2,800
D1.	Cathodic Protection System	\$20,000	\$15,200	\$18,500	\$19,000
	Subtotal Parts C & D	\$30,000	\$23,000	\$38,500	\$21,800
	Total Price	\$304,800	\$323,000	\$506,000	\$641,528

# **NOTICE of AWARD**

To:	L & T Painting, Inc.		
	43347 Woodbridge Dr.		
	Clinton Twp. MI 48038		
Contract:	Pleasant Prairie, Wisconsin		
	750,000 Gallon Spheroid		
	Exterior Overcoat		
	Wet Interior Repaint		
	Dry Interior Partial Repaint		
	and Miscellaneous Repairs		
Project Scor	be (selected line items):		
5 1	<b>Replace Access Tube Hatch (\$5,000)</b>		
	Mud Valve (\$4,500)		
	Fall Prevention Devices (\$7,000)		
	Painter's Rail (\$3,000)		
	Aviation Lights (\$1,800)		
	Antenna Conduit (\$16,000)		
	Antenna Cable Brackets (\$3,400)		
	Exterior Overcoat (\$80,000)		
	Wet Interior Repaint (\$130,100)		
	Dry Interior Partial Repaint (\$24,000)		
	Electrical Upgrade (\$10,000)		

Cathodic Protection System (\$20,000)

The Owner has considered the Bid submitted by you for the above described work in response to its Advertisement for Bids and Information for Bidders dated May 7, 2013.

You are hereby notified that your Bid has been accepted for the line items listed in the Project Scope in the amount of \$304,800.

You are required by the Information for Bidders to execute the Agreement and furnish the required Contractor's Performance Bond, Payment Bond, and Certificate(s) of Insurance within ten (10) calendar days from the date of this Notice to you. If you fail to execute said Agreement and to furnish said Bonds within ten days from the date of this Notice, said Owner will be entitled to consider all your rights arising out of the Owner's acceptance of your Bid as abandoned and as a forfeiture of your Bid Bond. The Owner will be entitled to such other rights as may be granted by law.

You are required to return an acknowledged copy of this NOTICE of AWARD to the Owner.

Dated this the	day of	, 2013.
		<u>VILLAGE OF PLEASANT PRAIRIE,</u> <u>WISCONSIN</u>
		<u>By</u>

Title\_

### **ACCEPTANCE of NOTICE**

Receipt of the above NOTICE of AWARD is hereby acknowledged by L & T Painting, Inc. this the \_\_\_\_\_\_ day of \_\_\_\_\_\_, 2013.

## L & T PAINTING, INC.

By

<u>Title</u>\_\_\_\_\_

Consider **Zoning Text Amendments (Ord. #13-15)** for the request of Mike Dilworth agent for Courtyard Junction Ltd., to amend The Courtyard Junction Condominium Planned Unit Development to remove the requirement that requires "at no time shall more than 20% of the completed units be rented as individual apartments" for the properties located at 5732, 5744, 5766, 5788, 5812, 5822, 5846, 5906, 5920 80<sup>th</sup> Street.

**Recommendation**: On May 13, 2013 the Plan Commission held a public hearing and determined that due to the facts and circumstances presented this is not considered precedent setting, that the development must remain a condominium development and further that the Village Board approve the Zoning Text Amendment (Ord. #13-015).

### VILLAGE STAFF REPORT OF MAY 20, 2013

Consider **Zoning Text Amendments (Ord. #13-15)** for the request of Mike Dilworth agent for Courtyard Junction Ltd., to amend The Courtyard Junction Condominium Planned Unit Development to remove the requirement that requires "at no time shall more than 20% of the completed units be rented as individual apartments" for the properties located at 5732, 5744, 5766, 5788, 5812, 5822, 5846, 5906, 5920 80<sup>th</sup> Street.

In February 2008 Courtyard Junction Ltd., converted Courtyard Junction Apartments to Condominiums. Courtyard Junction Apartments and associated infrastructure was constructed in 1996 and includes 7-12 unit buildings and 2-6 unit buildings for a total of 96 units that range in size from 1,135 square feet to 1,193 square feet on the 11.02 acre property. All units have individual entrances and two bedrooms and a one car attached garage which allowed them to be easily converted from apartments to condominiums.

As a part of that conversion the Village Board adopted Ord. #08-08 to create the specific PUD requirements for this development. Section c (vi) of the Courtyard Junction Condominium Planned Unit Development (PUD) Ordinance states that:

"Each unit within the CONDOMINIUM DEVELOPMENT shall be occupied and used only as an "owner occupied" residence. The term "Owner occupied" shall mean that each Unit shall be occupied by one of the following: (a) a Unit Owner who is a natural person; (b) the equitable beneficiary of a Unit Owner that is a trust; (c) the shareholder, member or partner of a Unit Owner that is an entity provided that such occupancy is without charge and is not in the nature of a transient tenancy; or (d) a member of the immediate family or a Unit Owner who is a natural person and is residing with the Unit Owner. The term "immediate family" is limited to parents, grandparents, children, grandchildren, siblings, or in-laws. Notwithstanding the foregoing, any Unit which is owned by Declarant may be rented as an individual apartment for a period of no less than one year; provided however, that at no time shall more than twenty percent (20%) of the completed Units be rented as individual apartments. The term "completed Unit: shall be a Unit for which the Village of Pleasant Prairie has issued an occupancy permit. Prior to leasing a Unit, the Declarant shall deliver to the Association written notice of the name of any Person leasing such Unit and the commencement and termination dates of the lease. No Unit Owner, other than Declarant, shall lease any Unit as an individual apartment. Furthermore, no Unit shall be subject to any time share or similar arrangement under Wis, Stats, Chapter 707 as amended."

At the time of the conversion, their goal was to sell all 96 units within two to three years. However, shortly after the conversion, the economy and the real estate market began a downward turn. By November 2010 sales were at a standstill with only 24 of the 96 units conveyed to condo owners. To date 72 units are still unsold and are owned by Courtyard Junction Ltd.

The petitioner is requesting that the Courtyard Junction Condominium Planned Unit Development be amended to remove the requirement that requires "at no time shall more than 20% of the completed units be rented as individual apartments" (see underlined above). Currently there are 53 legal, non-conforming rental units in the Condominium Development (A total of 19 units or 20% of the 96 are allowed to be rented.) On December 3, 2012 a special condo owners meeting was held by Courtyard Junction Ltd. to obtain feedback and vote to amend the Declaration from a 20% rental cap to allow an unlimited number of rentals. Six (6) or 25% of owners were present and voted favorably to amend the Declaration based on the following information:

- 1. Courtyard Junction remains as condominiums and not convert back to apartments.
- 2. Association enforces rules. The Bylaws can be enforced by the Association as a third party to lease. The Association has the right to evict a tenant or terminate a lease should a violation continue ten days after a written notice. Courtyard Junction has continued to have a peaceful coexistence between tenants and owners.
- 3. Avoid becoming a fractured development. By allowing the Declarant or potential investor to rent the 72 unsold units to meet financial obligation to the Association (monthly condo fees for 72 unsold units total \$10,000). Many condo associations have seen increasing condo fees or special assessments. Courtyard Junction continues at the same monthly fee with adequate funds in the reserve. Amending the Declaration will also allow existing condo owners the opportunity to rent their unit if unable to sell and, thus reducing potential foreclosures.

Pursuant to the application, within the last five years, banks have foreclosed on many developers and homeowners. As Declarant, Courtyard Junction, Ltd., is experiencing similar pressures by the bank to now consider selling the remaining 72 units to an investor in order to pay-off the mortgage balance as it is uncertain how long it will take for the market to recover. A potential investor will have to pay \$3,000/unit towards the exterior building improvement fund, \$216,000, to the Association (\$72,000 was paid on the 24 units previous conveyed). In Summary, there would then be a total of approximately \$288,000 in the exterior building improvement fund plus reserves.

A copy of a staff report from the Village Assessor is **attached** to this memorandum.

On May 13, 2013 the Plan Commission held a public hearing and determined that due to the facts and circumstances presented this is not considered precedent setting, that the development must remain a condominium development and further that the Village Board approve the Zoning Text Amendment (Ord. #13-015).

Office of the Village Assessor **Rocco Vita** 



# Memorandum

Date: May 2, 2013

To: Jean Werbie-Harris, Community Development Director

From: Rocco Vita Pleasant Prairie Assessor

**Re:** Courtyard Junction Declaration amendment allowing a greater rental percentage

Jean:

Courtyard Junction was conceived and constructed during 1997 as a 96-unit apartment complex with one legal description and one parcel number for the entire development. During 2008 the apartment complex was legally declared a condominium development and each apartment was converted to a separate legal entity with its own legal description and parcel number.

The initial value of the development as apartments as of January, 1998, was \$56,700 per unit. This value had grown to \$82,000 per unit as of Jan, 2008, just prior to their conversion to condominiums. Upon conversion and as a result of consequent unit sales, the value increased by one third to \$109,300 per unit as of Jan. 2009. A declining market in subsequent years has reduced the value to \$87,700 per unit as of Jan, 2012. The 2012 assessed value per unit, based on comparable sales, is about 7% greater than the 2008 assessment, the last year the development was valued as an apartment complex.

Asmt Year	Units	Total Value	Value/ Unit	Value Change	Unit Type
1998	96	5,444,700	56,716	N/A	Apartment
2008	96	7,869,100	81,970	45%	Apartment
2009	96	10,490,700	109,278	33%	Condominium
2010	96	9,616,700	100,174	-8%	Condominium
2012	96	8,419,900	87,707	-12%	Condominium

Thus far a total of 24 units have been sold by the original developer and because of market conditions, 72 units continue to be rented by the original developer. It is important to note that each individual unit has its own unique tax parcel number providing the owner of each unit the right to sell the unit to another party. Creation of a condominium plat with individual parcel numbers lends itself to identifying a highest and best of the units as a condominium unit and using sales of comparable property in the valuation process rather than as an apartment unit and using an income approach to valuation.

Secondly, as of today, the value per unit using the sales comparison approach is greater than using an income approach if determining the value as an apartment complex. With this being the case, the highest and best use of these units continues to be as condominium ownership with valuation as a condominium development using the sales comparison approach. This is true in light of the fact that two thirds of the units continue to be rented like an apartment.

Short of 'undoing' the condominium plat whereby removing all of the rights afforded to each of the units afforded via the condominium plat and combining all of the parcel numbers into one tax parcel number encompassing the entire development under one ownership, the highest and best use will remain as individual condominium units and valued using comparable sales.

Amending the original condominium declaration to allow for a greater rental percentage, by itself, would not change the highest and best use or the method of valuation. Since two thirds of the units remain to be rented today and the unit values continue to be greater as a condominium unit rather than an apartment unit, it would be difficult to quantify a negative consequence, if any, by increasing the allowable rental percentage.

### **ORDINANCE # 13-15**

### ORDINANCE TO AMEND THE COURTYARD JUNCTION CONDOMINIUM PLANNED UNIT DEVELOPMENT (PUD #25 IN APPENDIX C OF CHAPTER 420, THE VILLAGE ZONING ORDINANCE) IN THE VILLAGE OF PLEASANT PRAIRIE, KENOSHA COUNTY, WISCONSIN

**BE IT ORDAINED** by the Village Board of Trustees of the Village of Pleasant Prairie, Kenosha County, Wisconsin, that the Courtyard Junction Condominium Planned Unit Development Ordinance 25 c (iv) in Appendix C of Chapter 420 is hereby amended pursuant to Chapter 420-137 of the Village Zoning Ordinance as follows:

Each unit within the CONDOMINIUM DEVELOPMENT shall be occupied and (iv) used only as an "owner occupied" residence. The term "Owner occupied" shall mean that each Unit shall be occupied by one of the following: (a) a Unit Owner who is a natural person; (b) the equitable beneficiary of a Unit Owner that is a trust; (c) the shareholder, member or partner of a Unit Owner that is an entity provided that such occupancy is without charge and is not in the nature of a transient tenancy; or (d) a member of the immediate family or a Unit Owner who is a natural person and is residing with the nit Owner. The term "immediate family" is limited to parents, grandparents, children, grandchildren, siblings, or in-laws. Notwithstanding the foregoing, any Unit which is owned by Declarant may be rented as an individual apartment for a period of no less than one year. ; provided however, that at no time shall more than twenty percent (20%) of the completed Units be rented as individual apartments. The term "completed Unit: shall be a Unit for which the Village of Pleasant Prairie has issued an occupancy permit. Prior to leasing a Unit, the Declarant shall deliver to the Association written notice of the name of any Person leasing such Unit and the commencement and termination dates of the lease. No Unit Owner, other than Declarant, shall lease any Unit as an individual apartment. Furthermore, no Unit shall be subject to any time share or similar arrangement under Wis, Stats, Chapter 707 as amended.

### Adopted this 20<sup>th</sup> day of May, 2013.

VILLAGE OF PLEASANT PRAIRIE

ATTEST:

John P. Steinbrink Village President

Jane M. Romanowski, CMC Village Clerk

Posted:

15-Courtyard Junction PUD Amend.doc



129/13 20 Filed Published 516 20 Public Hearing 130 Fee Paid 20 Approved Notices Mailed 20/ Denied 20

### VILLAGE OF PLEASANT PRAIRIE, WISCONSIN ZONING MAP AND TEXT AMENDMENT APPLICATION

ECEIVE

Village of Pleasant Prairie

To: Village Plan Commission & Village Board of Trustees of the Village of Pleasant Prairie:

I, (We), the undersigned owner(s)/agent do hereby petition the Village Board to amend the Village of Pleasant Prairie Zoning Map as hereinafter requested.

It is petitioned that the following described property be rezoned from the present PUD

District(s) to Amend PUD District(s). The property petitioned
to be rezoned is located at: $5846$ , $5906$ , $5930$ , $8014$ , $5812$ , $5822$ , and is legally described
(address)
as follows: see attached legal description for Courtyard
Junction Condominium Development
Tax Parcel Number(s): <u>see attached list of 72 parcel numbers</u>
The proposed use for this property is: to remove the following section in 20 ning text
"requires that at no time shall more than 20% of the completed Units be rented
Petitioner's interest in the requested rezoning: original developer and declarant who owns the mused
Compatibility with adjacent land uses: apartments immediately to the West (See Letter)
I (We) are also requesting a Zoning Text Amendment to amend Section $25 \cdot c \cdot (v_1)$ of
the Village Zoning Ordinance.

I (We), have contacted the Community Development Department to arrange a pre-application meeting to discuss the proposed request to determine additional information that may be needed for this request.

I, (We), hereby certify that all the above statements and attachments submitted herewith are true and correct to the best of my knowledge.

PROPERTY OWNE		
Print Name: <u>Mike</u>	Dilworth	۱
Signature:	$\underline{\bigcirc}$	)
Address: \$575 W	). Forest H	ome Ak #160
Dreenfield	WI	53228
(City)	(State)	(Zip)
Phone: 414-42	5-4939	
Fax: 414-42	15-4946	
Email: mdilwor	the ener.	Con. com
Date 4-8-13		

### **OWNER'S AGENT:**

Print Name:		
Signature:		
Address:		2
(City)	(State)	(Zip)
Phone:		
Fax:		
Email:		
Date:		

### Legal Description – Courtyard Junction

The properties included are collectively known as buildings 1-9 and the associated common areas within the Courtyard Juncton Condominium as recorded at the Kenosha County Register of Deeds Office on February 27, 2008, as Document #1548846 a re-division a Parcel 1 of CSM 2615 and located in U.S. Public Land Survey Section 10, Township 1 North, Range 22 East in the Village of Pleasant Prairie, Kenosha County, Wisconsin, described as follows: Commence on the South line of said ¼ section at a point 465.03 feet North 89 degrees 11'57" West from the Southeast corner of said ¼ section; thence North 02 degrees 10'59" West 40.05 feet parallel to the East line of said ¼ section to the Northerly right of way line of 80<sup>th</sup> Street and point of beginning; thence North 89 degrees 11'57" West 629.24 feet along said right of way: thence North 02 degrees 25'36" West 543.86 feet to the Southeasterly line of the right of way of Chicago and Northwestern Railroad and point on a curve of Southeasterly convexity whose radius is 5779.15 feed and whose chord bears North 55 degrees 25'48" East 399.96 feet: thence Northeasterly along the arc of said curve 400.04 feet and said railroad right of way; thence North 53 degrees 26'48" East 354.92 feet along said railroad right of way; thence South 02 degrees 10'59" East 991.21 feet parallel with the East line of said ¼ section to the point of beginning.

## **Courtyard Junction**

72 Unsold Units

Address	Unit	Parcel Number
5732 80th Street	101	91-4-122-101-0231
5732 80th Street	102	91-4-122-101-0232
5732 80th Street	104	91-4-122-101-0234
5732 80th Street	106	91-4-122-101-0236
5822 80th Street	201	91-4-122-101-0237
5822 80th Street	202	91-4-122-101-0238
5822 80th Street	203	91-4-122-101-0239
5822 80th Street	204	91-4-122-101-0240
5822 80th Street	205	91-4-122-101-0241
5822 80th Street	207	91-4-122-101-0243
5822 80th Street	208	91-4-122-101-0244
5822 80th Street	209	91-4-122-101-0245
5822 80th Street	211	91-4-122-101-0247
5822 80th Street	212	91-4-122-101-0248
5920 80th Street	301	91-4-122-101-0249
5920 80th Street	302	91-4-122-101-0250
5920 80th Street	303	91-4-122-101-0251
5920 80th Street	304	91-4-122-101-0252
5920 80th Street	305	91-4-122-101-0253
5920 80th Street	306	91-4-122-101-0254
5906 80th Street	401	91-4-122-101-0255
5906 80th Street	402	91-4-122-101-0256
5906 80th Street	405	91-4-122-101-0259
5906 80th Street	406	91-4-122-101-0260
5906 80th Street	409	91-4-122-101-0263
5906 80th Street	411	91-4-122-101-0265
5906 80th Street	412	91-4-122-101-0266
5846 80th Street	502	91-4-122-101-0268
5846 80th Street	504	91-4-122-101-0270
5846 80th Street	506	91-4-122-101-0272
5846 80th Street	507	91-4-122-101-0273
5846 80th Street	508	91-4-122-101-0274
5846 80th Street	509	91-4-122-101-0275
5846 80th Street	511	91-4-122-101-0277
5846 80th Street	512	91-4-122-101-0278
5812 80th Street	601	91-4-122-101-0279
5812 80th Street	603	91-4-122-101-0281
5812 80th Street	604	91-4-122-101-0282
5812 80th Street	605	91-4-122-101-0283
5812 80th Street	606	91-4-122-101-0284
5812 80th Street	607	91-4-122-101-0285
5812 80th Street	608	91-4-122-101-0286

5812 80th Street	609	91-4-122-101-0287
5812 80th Street	610	91-4-122-101-0288
5812 80th Street	611	91-4-122-101-0289
5812 80th Street	612	91-4-122-101-0290
5788 80th Street	701	91-4-122-101-0291
5788 80th Street	702	91-4-122-101-0292
5788 80th Street	703	91-4-122-101-0293
5788 80th Street	704	91-4-122-101-0294
5788 80th Street	705	91-4-122-101-0295
5788 80th Street	706	91-4-122-101-0296
5788 80th Street	707	91-4-122-101-0297
5788 80th Street	708	91-4-122-101-0298
5788 80th Street	711	91-4-122-101-0301
5788 80th Street	712	91-4-122-101-0302
5766 80th Street	801	91-4-122-101-0303
5766 80th Street	802	91-4-122-101-0304
5766 80th Street	803	91-4-122-101-0305
5766 80th Street	806	91-4-122-101-0308
5766 80th Street	809	91-4-122-101-0311
5766 80th Street	811	91-4-122-101-0313
5744 80th Street	901	91-4-122-101-0315
5744 80th Street	902	91-4-122-101-0316
5744 80th Street	903	91-4-122-101-0317
5744 80th Street	904	91-4-122-101-0318
5744 80th Street	905	91-4-122-101-0319
5744 80th Street	906	91-4-122-101-0320
5744 80th Street	907	91-4-122-101-0321
5744 80th Street	910	91-4-122-101-0324
5744 80th Street	911	91-4-122-101-0325
5744 80th Street	912	91-4-122-101-0326



**REAL ESTATE: DEVELOPERS & BUILDERS & PROPERTY MANAGEMENT & SALES** 8575 W. Forest Home Ave., Suite 160 Greenfield, WI 53228 Office (414) 425-4939 Fax: (414) 425-4946

April 1, 2013

Mrs. Jean Werbie-Harris Community Development Director Village of Pleasant Prairie 9915 39<sup>th</sup> Avenue Pleasant Prairie, WI 53158

Village of Pleasant Prairie

RE: Courtyard Junction - Amending Declaration on Rentals

Dear Jean:

As you know, Lexington Village was our first condo conversion in August, 2006. Based on the success in selling 120 units in less than two years, I decided to convert Courtyard Junction which was approved in February, 2008.

### <u>History</u>

Our goal was to sell units within two-three years. We did not expect the economy and real estate market to take a downward spiral shortly after sales began in March, 2008.

Initially, there was no concern of the 20% rental cap as stated in the Declaration.

Unfortunately, by November 2010, sales were at a standstill with only 24 of the 96 units conveyed to condo owners.

### Association Turnover

As Declarant, I turned over the Association to the unit owners on April 5, 2012, approximately three years from date of first sales as required per the Declaration.

There are five Board members: 2 condo owners (two year terms), myself and two of my employees (3 year terms). Condo owners on the board serves two purposes; reassures owners representation and funds appropriately handled per the requirements of the Bylaws and State Statutes.

### Special Condo Owners Meeting

A meeting was held on site on December 3, 2012 to obtain feedback and vote to amend the Declaration from a 20% rental cap to allow an unlimited number of rentals. Six or 25% of owners were present and voted favorably to amend the Declaration based on the following information:

1. Courtyard Junction remains as condominiums and not convert back to apartments.

### 2. Association enforces rules.

The Bylaws can be enforced by the Association as a third party to lease. The Association has the right to evict a tenant or terminate a lease should a violation continue ten days after a written notice. Courtyard Junction has continued to have a peaceful coexistence between tenants and owners.

3. Avoid becoming a fractured development.

By allowing the Declarant or potential investor to rent the 72 unsold units to meet financial obligation to the Association (monthly condo fees for 72 unsold units total \$10,000). Many condo associations have seen increasing condo fees or special assessments. Courtyard Junction continues at the same monthly fee with adequate funds in the reserve.

Amending the Declaration will also allow existing condo owners the opportunity to rent their unit if unable to sell and, thus reducing potential foreclosures.

### Summary

Within the last five years, banks have foreclosed on many developers and homeowners. As Declarant, I am experiencing similar pressures by the bank to now consider selling the remaining 72 units to an investor in order to payoff the mortgage balance as it is uncertain how long it will take for the market to recover. A potential investor will have to pay \$3000/unit towards the exterior building improvement fund, \$216,000, to the Association (\$72,000 was paid on the 24 units previous conveyed). In Summary, there would then be a total of approximately \$288,000 in the exterior building improvement fund plus reserves.

I have attached the most current appraisal report dated November 10, 2011. Please refer to page 1, which gives value as apartments or condominiums.

We discussed your concern, as well as comments from the Tax Assessor, of property values declining or tax assessments being affected by amending the Declaration to allow rentals. The current assessed value of the 72 remaining unsold rented units is \$6,350,500, averaging \$85,000 to \$92,000 per unit (based on 2012 tax bills). The enclosed appraisal gives value of the 72 unsold condominiums at \$6,165,000 or \$85,625 per unit.

The value as apartments is \$5,800,000 or \$80,555 per unit.

Courtyard Junction could be avoided becoming a fractured development, if I am able to consider selling to a potential investor. I may have a solid buyer, interested in writing an offer around \$5,600,000 - \$5,800,000 or \$77,777-\$80,555 per unit. This individual is experienced in multi-family developments and familiar with the Pleasant Prairie area. Even though the value would be lower, it would be similar if I had kept Courtyard Junction as apartments.

I would appreciate your consideration and approval to amend the Declaration to allow rentals.

Please feel free to contact me at 414-425-4939, should you have any further questions.

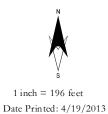
Thank you.

Sincerely,

Mike Dilworth



stablished 185



198-001 OURTYARD JUNCTION Ş CONDOMINIUM VILLAGE OF PLEASANT PRAIRIE CITY OF KENOSHA RD

DISCLAIMER This map is neither a legally recorded map nor a survey and is not intended to be used as one. This drawing is a compilation of records, data and information located in various state, county and municipal offices and other sources affecting the area shown and is to be used for reference purposes only. Kenosha County is not responsible for any inaccuracies herein contained. If discrepancies are found, please contact Kenosha County.

Consider **Village 2035 Comprehensive Plan Amendments (Ord. #13-16)** to consider adopting the Village of Pleasant Prairie Park and Open Space Plan 2013-2018, as a component of the Village of Pleasant Prairie, Wisconsin 2035 Comprehensive Plan and amending Section 395-6 B of the Village Municipal Code to reference this new plan as a component of the 2035 Comprehensive Plan.

**Recommendation**: On May 13, 2013 the Plan Commission held a public hearing and adopted Plan Commission Resolution #13-06 that recommended the Village Board to approve the **Village of Pleasant Prairie Park and Open Space Plans 2013-2018 as a component of the 2035 Comprehensive Plan (Ord. #13-16) as presented.** 

### VILLAGE STAFF REPORT OF MAY 20, 2013

Consider **Village 2035 Comprehensive Plan Amendments (Ord. #13-16)** to consider adopting the Village of Pleasant Prairie Park and Open Space Plan 2013-2018, as a component of the Village of Pleasant Prairie, Wisconsin 2035 Comprehensive Plan and amending Section 395-6 B of the Village Municipal Code to reference this new plan as a component of the 2035 Comprehensive Plan.

On September 12, 2011 the Village Plan Commission adopted Resolution #12-06 to initiate an amendment to the Village Comprehensive Plan to update the Village of Pleasant Prairie Park and Open Space Plan: 2006-2011 (Park and Open Space Plan) that was approved by the Village Park Commission on February 7, 2006, the Village Plan Commission on March 13, 2006 and the Village Board of Trustees on March 20, 2006. On December 19, 2009 the Village Board also adopted the Park and Open Space Plan as a component of the Village of Pleasant Prairie, Wisconsin 2035 Comprehensive Plan (Comprehensive Plan).

The Wisconsin Department of Administration requires that the Village update its Comprehensive Outdoor Recreation Plan, a/k/a as the Village Park and Open Space Plan, every five (5) years to maintain eligibility for grant funding for the acquisition and development of park and recreational facilities through the State Stewardship and Federal LAWCON Programs and to assure that park planning goals, objectives, and policies are current.

Park land, recreation trails and natural areas are key components of high quality living environments. Such open spaces provide a community with many benefits. These include helping to meet human needs for outdoor recreation, promoting and accommodating a healthy lifestyle for residents, enhancing the aesthetic quality of a community, increasing property values, attracting visitors and tourists, shaping development patterns, and protecting the natural environment.

Over the years, the Village has planned for and developed a park and recreation system that is designed to meet the needs of the people who live in the community. Along the way, the Village has not only accounted for its own park and open space needs, but has also maintained a regional perspective, taking measures to preserve critical ecosystems and watersheds, and providing parks and recreational facilities that serve the surrounding communities in Wisconsin and Illinois. This Park and Open Space Plan update was prepared to further this tradition of park and recreation planning in Pleasant Prairie.

The primary purpose of the Park and Open Space Plan is to proactively plan for the Village's future park and recreation needs. Furthermore, the policy recommendations and programs presented in the Plan will guide the acquisition, preservation, and development of land for parks, recreation trails, and other open spaces in the Village to meet the needs of a growing and changing population, including those with special needs and handicapped. Such recommendations will also serve to protect and enhance the community's natural resource base into the future. Although, the Park and Open Space Plan addresses the long-range park and open space needs of the community (through the year 2035), it also focuses on policy recommendations and programs to be implemented over the next five-year period.

The Park and Open Space Plan is also intended to incorporate and refine the previous findings and recommendations presented in the Village Park and Open Space Plan (2006-2011) and the Village of Pleasant Prairie, Wisconsin 2035 Comprehensive Plan.

This Plan was prepared in accordance with guidelines that will make it certifiable by the Wisconsin Department of Natural Resources (DNR) and qualify the Village for matching grant funds through the Federal Land and Water Conservation Fund (LAWCON) and the State of Wisconsin Stewardship Fund. The Plan must be updated every five years to ensure that it reflects the current needs of the community and retains its DNR certification. This Plan was also prepared as a component of the Village's 2035 Comprehensive Plan.

The Park and Open Space Plan 2013-2018 has been updated and includes the following:

**Chapter 1: Introduction Chapter 2: Background Information Chapter 3: Goals, Objectives and Standards Chapter 4: Existing Park and Recreational Facilities Regional Parks** Community Parks Neighborhood Parks Special Open Space Passive Areas Special Open Space Active Areas **Recreational Trails** Bicycle Lanes Other Village Owned Recreational Facilities Public School Parks Accessibility-Universal Design and ADA Compliance Map 4.1 Existing Park and Open Space Land Map 4.2 Environmental Corridors and Isolated Natural Areas Map 4.3 Existing Off-Street Bicycle and Recreational Trails **Chapter 5: Public Participation and Planning Process** Community-wide Survey Visioning Sessions **Public Informational Meetings** Chapter 6: Analysis of Existing Park and Recreational Facilities **Ouantitative Analysis** Qualitative Analysis Geographic Analysis Public Input Review of the Wisconsin Statewide Comprehensive Outdoor Recreational Plan (SCORP) Map 6.1 Service Area of Existing Parks **Chapter 7: Policy Recommendations and Programs Implementation** Introduction **Basic Definitions** Policy Recommendations and Programs Implementation of Program Recommendations Chapter 8: Recommended Park, Open Space and Trail Conceptual Plans, Proposed **Improvements and Future Park and Open Space Areas** Proposed Park Conceptual Plan Developments and Recommendations Recommendations for Special Open Space Areas Implementation Map 8.1 Existing and Future Park and Open Space Land Map 8.2 Service Areas for Existing and Proposed Parks **Appendix A:** Pleasant Prairie Parks and Recreation Survey **Appendix B:** Open Ended Responses to Community Survey Questions 21 and 25 **Appendix C:** Potential Funding Sources Appendix D: Conceptual Plans (Figures 8.1 to 8.15) as discussed in Chapter 8 Appendix E: Village of Pleasant Prairie 2010 Bicycle and Pedestrian Trails Plan

On May 13, 2013 the Plan Commission held a public hearing and adopted Plan Commission Resolution #13-06 that recommended the Village Board to approve the **Village of Pleasant Prairie Park and Open Space Plans 2013-2018 as a component of the 2035 Comprehensive Plan (Ord. #13-16)** as presented.

### ORD. # 13-16

### ORDINANCE TO AMEND SECTION 390-6 OF THE VILLAGE MUNICIPAL CODE ENTITLED COMPREHENSIVE PLAN IN THE VILLAGE OF PLEASANT PRAIRIE, KENOSHA COUNTY, WISCONSIN

**THE VILLAGE BOARD OF TRUSTEES OF THE VILLAGE OF PLEASANT PRAIRIE, KENOSHA COUNTY, WISCONSIN,** do hereby ordain that the Village of Pleasant Prairie, Wisconsin 2035 Comprehensive Plan be amended to adopt the Village of Pleasant Prairie Park and Open Space Plan: 2013-2018 (*Exhibit 1*) as a component of the Village's Comprehensive Plan and that Section 390-6 B of the Village Municipal Code is hereby amended to read as follows:

B. Village of Pleasant Prairie Park and Open Space Plan: 2013-2018.

The Village Community Development Director is hereby directed to record this Amendment to the Comprehensive Plan on the appropriate pages of said Plan and to update Appendix A in Chapter 390 of the Village Municipal Code to include said amendment.

### Adopted this 20<sup>th</sup> day of May, 2013.

VILLAGE OF PLEASANT PRAIRIE

ATTEST:

John P. Steinbrink, Village President

Jane M. Romanowski Village Clerk

Ayes: \_\_\_\_ Nayes: \_\_\_\_ Absent: \_\_\_\_

Posted: \_\_\_\_\_

Ord #13-16 Park and Open Space Plan.doc

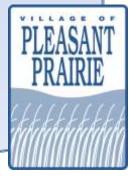
# VILLAGE OF PLEASANT PRAIRIE PARK AND OPEN SPACE PLAN

# 2013-2018

Adopted by the Village Park Commission on May 7, 2013 by Resolution #13-01 Adopted by the Village Plan Commission on May 13, 2013 by Resolution #13-06 Adopted by the Village Board on May 20, 2013 by Ordinance #13-16



Lake Andrea Plaza in Prairie Springs Park



### **Village Board of Trustees**

John P. Steinbrink, Village President Monica Yuhas, Trustee #1 Steve Kumorkiewicz, Trustee #2 Clyde Allen, Trustee #3 Michael Serpe, Trustee #4

### Village Administration

Michael Pollocoff, Village Administrator Thomas Shircel, Assistant Village Administrator

### **Plan Commission**

Thomas Terwall, Chairperson Michael Serpe, Vice-Chairperson Donald Hackbarth, Secretary James Bandura John Braig Wayne Koessl Larry Zarletti Judith Juliana, Alternate Andrea Rode, Alternate

#### **Park Commission**

Michealeane Day, Chairperson Rita Christiansen, Vice Chairperson Troy Holm Glen Christiansen William Mills Steven Kundert Monica Yuhas Cindy Schwab, Alternate James Bandura, Alternate

### Others involved in the preparation of this Plan

John Steinbrink Jr., Director of Public Works Jean Werbie-Harris, Community Development Director Carol Willke, Director of Recreation and Director of Human Resources Peggy Herrick, Assistant Planner/Assistant Zoning Administrator Ruth Mack-Stoner, Executive Secretary Jan Petrovic, Executive Secretary

### VILLAGE OF PLEASANT PRAIRIE PARK AND OPEN SPACE PLAN-2013-2018 TABLE OF CONTENTS

Chapter	1: Introduction1
Chapter	2: Background Information2
	General Regional Context and Village History Natural and Cultural Resources Population and Demographics Household Characteristics Review of Existing Plans
Chapter	3: Goals, Objectives and Standards21
	Introduction Basic Definitions Goals Objectives Standards
Chapter	4: Existing Park and Recreational Facilities29
	Regional Parks Community Parks Neighborhood Parks Special Open Space Passive Areas Special Open Space Active Areas Recreational Trails Bicycle Lanes Other Village Owned Recreational Facilities Public School Parks Accessibility-Universal Design and ADA Compliance
	Maps:47
	Map 4.1 Existing Park and Open Space Land Map 4.2 Environmental Corridors and Isolated Natural Areas Map 4.3 Existing Off-Street Bicycle and Recreational Trails
Chapter	5: Public Participation and Planning Process
	Community-wide Survey Visioning Sessions Public Informational Meetings
Chapter	6: Analysis of Existing Park and Recreational Facilities74
	Quantitative Analysis Qualitative Analysis Geographic Analysis Public Input Review of the Wisconsin Statewide Comprehensive Outdoor Recreational Plan (SCORP)
	Maps:
	Map 6.1 Service Area of Existing Parks

Introduction Basic Definitions Policy Recommendations and Programs Implementation of Program Recommendations Chapter 8: Recommended Park, Open Space and Trail Conceptual Plans, Proposed Proposed Park Conceptual Plan Developments and Recommendations Recommendations for Special Open Space Areas Implementation Map 8.1 Existing and Future Park and Open Space Land Map 8.2 Service Areas for Existing and Proposed Parks **Appendix A:** Pleasant Prairie Parks and Recreation Survey **Appendix B:** Open Ended Responses to Community Survey Questions 21 and 25 **Appendix C:** Potential Funding Sources

**Appendix D:** Conceptual Plans (Figures 8.1 to 8.15) as discussed in Chapter 8

**Appendix E:** Village of Pleasant Prairie 2010 Bicycle and Pedestrian Trails Plan

# CHAPTER 1 INTRODUCTION

Park land, recreation trails and natural areas are key components of high quality living environments. Such open spaces provide a community with many benefits. These include helping to meet human needs for outdoor recreation, promoting and accommodating a healthy lifestyle for residents, enhancing the aesthetic quality of a community, increasing property values, attracting visitors and tourists, shaping development patterns, and protecting the natural environment.

Over the years, the Village of Pleasant Prairie has planned for and developed a park and recreation system that is designed to meet the needs of the people who live in the community. Along the way, the Village has not only accounted for its own park and open space needs, but has also maintained a regional perspective, taking measures to preserve critical ecosystems and watersheds, and providing parks and recreational facilities that serve the surrounding communities in Wisconsin and Illinois. This Park and Open Space Plan update was prepared to further this tradition of park and recreation planning in Pleasant Prairie.

The primary purpose of this Plan is to proactively plan for the Village's future park and recreation needs. Furthermore, the policy recommendations and programs presented in the Plan will guide the acquisition, preservation, and development of land for parks, recreation trails, and other open spaces in the Village to meet the needs of a growing and changing population, including those with special needs and handicapped. Such recommendations will also serve to protect and enhance the community's natural needs of the community (through the year 2035), it also focuses on policy recommendations and programs to be implemented over the next five-year period.

This Plan is also intended to incorporate and refine the previous findings and recommendations presented in the Village Park and Open Space Plan (2006-2011) and the Village of Pleasant Prairie, Wisconsin 2035 Comprehensive Plan.



Park path connection to Kenosha County Bike Trail from Rolling Meadows Park

This Plan was prepared in accordance with guidelines that will make it certifiable by the Wisconsin Department of Natural Resources (DNR) and qualify the Village for matching grant funds through the Federal Land and Water Conservation Fund (LAWCON) and the State of Wisconsin Stewardship Fund. The Plan must be updated every five years to ensure that it reflects the current needs of the community and retains its DNR certification. This Plan was also prepared as a component of the Village's 2035 Comprehensive Plan.

## **CHAPTER 2**

## **BACKGROUND INFORMATION**

### GENERAL REGIONAL CONTEXT AND VILLAGE HISTORY

Pleasant Prairie is a growing community located in Southeastern Wisconsin's Kenosha County. Situated on the southwest coast of Lake Michigan, the Village is bordered by the City of Kenosha on the north and Lake County, Illinois on the south. Interstate Highway 94 is in proximity to the western jurisdictional boundary of the Village. The Village is approximately 40 miles south of Milwaukee and 70 miles north of downtown Chicago. Positioned between two Midwestern metropolises and located directly off of a major interstate, Pleasant Prairie has an opportunity to provide a unique park and recreation system that may draw visitors from all over the region.



The Village was incorporated in 1989 from the former Town of Pleasant Prairie. Prior to the Village's incorporation, development originated from nine separate settlement areas, many of which still exist today. Therefore, unlike more traditional urban development patterns that radiate outward from a central location, Pleasant Prairie does not have a distinguishable core, and development is dispersed throughout the Village. This characteristic has important implications for park and trail systems planning. As of January 1, 2012, the Village encompassed approximately 34 square miles of land area and had a Village-estimated population of 19,850 residents.

Following the Village's incorporation and the creation of the Tax Increment Finance District #1, WisPark Corporation began development of the 2,391-acre LakeView Corporate Park that is now situated in the central western portion of the Village. In addition to the development of LakeView Corporate Park east of I-94, in 2008 construction began on the first development within PrairieWood Corporate Park west of I-94. PrairieWood Corporate Park is 270 а acres business/office park designed to accommodate office, industrial and When fully built out, warehousing uses. PrairieWood Corporate Park could support approximately 600,000-square-feet of office development and 2 million squarefeet of warehouse and/or manufacturing space with employment at full build-out estimated at 2,500 to 3,500 jobs.



LakeView Corporate Park

In 1996, approximately 221 acres was developed for commercial development within the Prairie Ridge development area generally located south of STH 50 (75<sup>th</sup> Street) between 88<sup>th</sup> and 104<sup>th</sup> Avenues. Today the Prairie Ridge Commercial Development contains a number of multi-tenant buildings including, Prairie Ridge Market Place, which includes three (3)

buildings; Prairie Ridge Commons which includes three (3) buildings; and the Shoppes at Prairie Ridge which includes eight (8) buildings. In addition, a number of standalone buildings and developments exist within the development including St Catherine's Hospital and Medical Office building, a hospice facility, a nursing home, a hotel, a bank, a child care facility, a church and several restaurants.



Prairie Ridge Commercial Area

Due in large part to the over 8,000 jobs created within the LakeView and PrairieWood Corporate Parks and the Prairie Ridge Commercial Development, the Village's daytime population is estimated to be between 40 and 50% higher than its Census population. Moreover, industrial and residential land uses are separated from one another, suggesting a need for additional connections between job centers, residential neighborhoods, and Village park and recreational facilities.

#### NATURAL AND CULTURAL RESOURCES

A survey of Pleasant Prairie's natural and cultural resources provides an important framework for guiding the park and open space planning process. The term cultural resource encompasses historic buildings, structures and sites; archaeological sites; and museums. Cultural resources in Village have important recreational and educational value and help to provide the Village with a sense of heritage, identity, and civic pride. Resources such as historical and archaeological sites and historic districts can also provide economic opportunities through tourism. There are several unique natural and cultural characteristics of the Village that will direct the development of future park, open space and recreational facilities. These include:



Jambeau Trail glacial boulder in Mompers Woods

- 4-season recreational opportunities;
- Significant water resources, such as the Des Plaines River and Lake Michigan; and
- Concentrations of high quality wildlife and vegetative habitat in the Chiwaukee Prairie Carol Beach area and Des Plaines River watershed.
- Prehistoric and historic archaeological sites such as the Barnes Creek, Chesrow, and Lucas, Scott, Wispark XXX1, Cabbage Patch, the Hasting Sites and the Jambeau Trail.

Detail description and discussion of the Natural and Cultural Resources in the Village can be found in Chapter 6 of the Village 2035 Comprehensive Plan.

**Climate:** Southeastern Wisconsin's climate is characterized by four distinct seasons. Warm summers generally span the months of June through August. The winter months of December, January, and February are cold, with average temperatures below freezing. The region is characterized by temperate conditions in spring and autumn. Because of its proximity to Lake Michigan, the climate in Pleasant Prairie is slightly modified by winds coming off of the Lake. These lake effects are most pronounced in the spring and early summer when the prevailing winds are easterly.

**Soils:** The Village is within the glacial drift region of Wisconsin. Blanketed with over 100 feet of silts, sand, and clay, a large proportion of Village land (42%) is classified as National Prime Farmland. That is, the soil is well suited for the production of food and has the capacity to sustain high yields of crops when properly managed. Furthermore, 9% of land in the Village has been identified as Farmland of Statewide Importance. Currently, 24% of Village land is being farmed.

**Surface Water and Watersheds**: Situated on the coast of Lake Michigan, Pleasant Prairie is located within the Des Plaines River watershed and the Lake Michigan direct drainage watershed. Bisecting the Village from north to south is the subcontinental divide, which separates the region into two major drainage areas. The Des Plaines watershed, encompassing the western two-thirds of the Village, drains to the south and southwest and is part of the Mississippi River drainage system. The Lake Michigan direct drainage watershed encompasses the eastern one-third of the Village.

The Des Plaines River flows from the northwestern portion of the Village south into Illinois, eventually contributing to the Illinois River. There are several small streams in the eastern portion of the Village that flow perpendicular to Lake Michigan. Barnes Creek and Tobin Creek are short perennial streams that have their origins in the Village and empty into Lake Michigan. There are three lakes in Pleasant Prairie. Lake Andrea is a 110-acre spring-fed



Des Plaines River

Lake Michigan

lake that was created from an old sand and gravel pit operation. The lake is located within Prairie Springs Park and is used primarily for recreation. Lake Russo is a privately-owned 21-acre lake located in the northwest corner of the Village in the River Oaks subdivision, and Paradise Lake is an approximately 39-acre lake located in the southeastern portion of the Village.



Lake Andrea

**Vegetation**: At the time of European settlement, much of southeastern Wisconsin was covered with prairie and oak savannah, oak woods, and lowlands. Since that time, most of the land has been converted to agricultural and urban uses. Currently, woodland areas mostly exist in isolated patches less than 40 acres in size. Most of the remaining prairie lands in the region are located in the Chiwaukee Prairie – Carol Beach area along the eastern-most portion of the Village and along Lake Michigan. Some native prairie habitat also still exists along the Des Plaines River.

**Wildlife Habitat**: Species of wildlife that are common to the southeastern Wisconsin region are rabbits, squirrels, woodchucks, raccoons, muskrats, and beavers. Larger mammals such as white-tailed deer, coyotes, and foxes also inhabit the region. Common bird species include: pheasants, cardinals, robins, woodthrushes, great blue herons, and killdeer. The most significant concentrations of high quality wildlife habitat are located in the Chiwaukee Prairie – Carol Beach area and surrounding the Des Plaines River.

**Wetlands:** Wetlands perform an important set of natural functions which include support of a wide variety of desirable, and sometimes unique, forms of plant and animal life; water quality protection; stabilization of lake levels and streamflows; reduction in stormwater runoff by providing areas for floodwater impoundment and storage; and protection of shorelines from erosion. Wetlands and their boundaries are continuously changing in response to changes in drainage patterns and climatic conditions. While wetland inventory maps provide a basis for area wide planning, detailed field investigations are necessary to precisely identify wetland boundaries on individual parcels. Field investigations are required at the time a parcel

is proposed to be developed or subdivided.

**Floodplains:** The natural floodplain of a river is a wide, flat-to-gently sloping area contiguous with, and usually lying on both sides of the river channel and the channel itself. The floodplain, which is normally bounded on its outer edges by higher topography, is

gradually formed over a long period of time by the river during flood stage as that river meanders in the floodplain, continuously eroding material from concave banks of meandering loops while depositing it on the convex banks. The flow of a river onto its floodplain is a normal phenomenon and, in the absence of flood control works, can be expected to occur periodically. For planning and regulatory purposes, floodplains are defined as those areas subject to inundation by the 100-year recurrence interval flood event. This event has a 1% chance of being equaled or exceeded in any given year. Floodplains are generally not well suited for urban development because of the flood hazard, the presence of high water tables, and/or the presence of wet soils.



Chiwaukee Prairie area



Des Plaines River Floodplain area

**Shorelands:** Shorelands are defined by the *Wisconsin Statutes* as lands within the following distances from the Ordinary High Water Mark (OHWM) of navigable waters: 1,000 feet from a lake, pond, or flowage; and 300 feet from a river or stream, or to the landward side of the floodplain, whichever distance is greater. In accordance with state requirements, the Village shoreland and floodplain zoning requirements restricts uses in wetlands located in the shorelands, and limits the uses allowed in the 100-year floodplain to prevent damage to structures and property and to protect floodwater conveyance areas and the storage capacity of floodplains.

**Woodlands:** With sound management, woodlands can serve a variety of beneficial functions. In addition to contributing to clean air and water and regulating surface water runoff, woodlands help maintain a diversity of plant and animal life. The destruction of woodlands, particularly on hillsides, can contribute to excessive stormwater runoff, siltation of lakes and streams, and loss of wildlife habitat. Woodlands are defined as upland areas of one acre or more in area, having 17 or more trees per acre, each deciduous tree measuring at least four inches in diameter 4.5 feet above the ground, and having canopy coverage of 50% or greater. Coniferous tree plantations and reforestation projects are also classified as woodlands.



Primary Environmental Corridor in Prairie Springs Park

#### Natural Areas and Critical Species Habitat Sites:

A comprehensive inventory of important plant and animal habitats was conducted by SEWRPC in 1994 as part of the regional natural areas and critical species habitat protection

and management plan. The inventory systematically identified all remaining high-quality natural areas, critical species habitat, and sites having geological significance within the Region. Ownership of identified natural areas and critical species habitat sites and the size of each were reviewed and updated in 2006.

*Natural Areas:* Natural areas are tracts of land or water so little modified by human activity, or sufficiently recovered from the effects of such activity, that they contain intact native plant and animal communities believed to be representative of the landscape before European settlement. Natural areas are classified into one (1) of three (3) categories:

- NA-1: Natural areas of statewide or greater significance which includes over 400 acres within the Carol Beach and Chiwaukee Prairie State Natural Areas and the Kenosha Sand Dunes
- NA-2: Natural areas of countywide or regional significance which includes over 100 areas within Carol Beach Prairie areas and Tobin Road Prairie
- NA-3: Natural areas of local significance which includes over 400 acres within the Village, Lake Russo Prairie, Des Plaines River, Bain Station Railroad Prairie, Pleasant Prairie Railroad Prairie and Carol Beach Estates Prairie.

Specific details related to the above noted Natural Areas and Critical Habitat Sites in the Village can be found in Chapter 6 of the Village 2035 Comprehensive Plan.

The Chiwaukee Prairie-Carol Beach, which draws visitors from all over the Midwest, the Chiwaukee Prairie-Carol Beach area represents one of the Village's significant natural resource areas. The Chiwaukee Prairie-Carol Beach area is located along the Lake Michigan coast and east of Sheridan Road in the Village. The land area is approximately 1,825 acres, or roughly 8% of the Village's total acreage. The Chiwaukee Prairie-Carol







Beach lands are worth special mention because they comprise one of the outstanding natural resource areas in southeastern Wisconsin and in the upper Midwest. The land is characterized by a beach dune ridge and swale complex, high quality wetland and prairies, and two natural areas of statewide significance. Furthermore, Chiwaukee Prairie is classified as a National Natural Landmark and is recognized as one of the best remaining examples of Lake Michigan prairie land in the upper Midwest. Because of wet soils and other environmental limitations, development in the area is scattered, leaving much of this ecosystem preserved. The Village, County, State, and non-profit organizations such as The Nature Conservancy of Wisconsin and the Chiwaukee Prairie Preservation Fund have continued to extend their efforts to purchase and protect remaining private parcels of land on a willing seller-willing buyer basis. The DNR managed lands north of this area are fragmented by roads and homes.

There are a number of conservation organizations active in the Village, including the Kenosha/Racine Land Trust, Des Plaines Wetlands Conservancy, The Nature Conservancy of Wisconsin, and other non-profit conservation organizations including the Chiwaukee Prairie Preservation Fund. These organizations acquire lands for resource protection purposes. The Des Plaines Wetlands Conservancy owns approximately 644 acres for resource protection purposes. The Nature Conservancy of Wisconsin owns two (2) sites in portions of Chiwaukee Prairie and Barnes Prairie, together encompassing 159 acres.

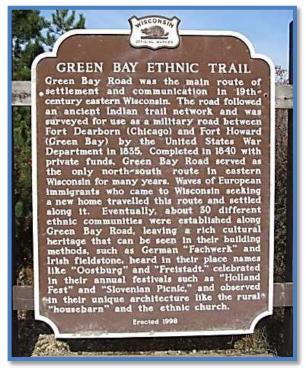
Several open space and environmentally sensitive sites in the Village are also protected under conservation easements. These easements are typically voluntary contracts between a private landowner and a land trust or governmental body that limit, or in some cases

prohibit, future development of the parcel. With the establishment of a conservation easement, the property owner sells or donates the development rights for the property to a land trust or governmental agency, but retains ownership. The owner is not prohibited from selling the property, but future owners must also abide by the terms of the conservation easement. The purchaser of the easement is responsible for monitoring and enforcing easement agreement for the property. the Conservation easements do not require public access to the property, although public access is generally required if Wisconsin stewardship funds or other DNR grant funds are used to acquire the property. There are three conservation easements located in the Pleasant Prairie including an easement on 445 acres held by The Nature Conservancy of Wisconsin as part of Prairie Springs Park in the Village



U.S. ACOE easement south of CTH C and west of I-94

**Historical Resources:** In 2009 there were three (3) historic places and districts in the Village listed on the National Register of Historic Places. All three (3) historic places and districts in the Village are prehistoric archeological sites and include the Barnes Creek Site, the Chesrow Site, and the Lucas Site. The Village is also home to four (4) Wisconsin State Historical Markers through a program administered by the Wisconsin Historical Society's Division of Historic Preservation. These historical markers are intended to identify, commemorate, and honor the important people, places, and events that have contributed to the State's rich heritage. The program serves as a vital educational tool, informing people about the most significant aspects of Wisconsin's past.



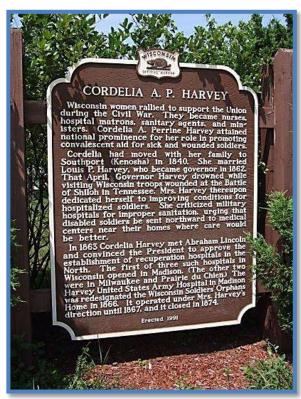
Green Bay Ethnic Trail Marker



History of the Word "Wisconsin" Marker



32<sup>nd</sup> Division Memorial Highway Marker



Cordelia A.P. Harvey Marker

Archeological Resources: Preservation of archaeological resources is also important in preserving the cultural heritage of the Village. Like historical sites and districts, significant prehistoric and historic archaeological sites provide the Village with a sense of heritage and identity, which can provide for economic opportunities through tourism if properly identified and preserved. Archaeological sites fall under two categories: prehistoric sites and historic sites. Prehistoric sites are defined as those sites which date from before written history. Historic sites are sites established after history began to be recorded in written form (the State Historical Society of Wisconsin defines this date as A.D. 1650)



Jambeau Trail glacial boulder historical landmark in Mompers Woods

The Barnes Creek, Chesrow, and Lucas

Sites as discussed above are prehistoric archaeological sites listed on the National Register of Historic Places in the Village. Other known archeological sites in the Village but not listed on the State or National Register include the Scott Site, Wispark XXX1 Site, Cabbage Patch Site, the Hasting Site and the Jambeau Trail.

#### **POPULATION AND DEMOGRAPHICS**

Demographic characteristics help to determine the types of park and recreational facilities that the Village will use now and in the future. Important demographic trends for Pleasant Prairie are as follows:

- Park and recreational facilities will need to accommodate approximately 31% population growth over the next 15 years (2025);
- Over the next 10-20 years 30.6% of village local residents will be moving into the 65-and-older age bracket.

**Population:** The Village has experienced a significant increase in population in recent years as shown in Table 2.1. From 1990 to 2010, the Village's population grew approximately 63.8% (34.1% between 1990 and 2000 and 22.2% between 2000 and 2010). This compares with a 30% increase in Kenosha County. The Village's daytime population is estimated to be between 40% and 50% higher due to the large number of people that commute into the Village for jobs, shopping, recreational events, and schools.

	F	Pleasant Prairie		Kenosha County					
Year	<i>a</i> Population	-	Change From Preceding Census Po		Change From I Censu	5			
		Number	%	_	Number	%			
1970	12,019			117,917					
1980	12,703	684	5.7	123,137	5,220	4.4			
1990	12,037	-666	-5.2	128,181	5,044	4.1			
2000	16,136	4,099	34.1	149,577	21,396	16.7			
2010	19,719	3,583	22.2	166,671	17,094	11.4			

TABLE 2.1POPULATION TRENDS LEVELS: 1970-2010

а

In 1989 the entire Town of Pleasant Prairie was incorporated as the Village of Pleasant Prairie and the Town of Pleasant Prairie ceased to exist. In addition, the decrease in population is attributed to the 1,500 acres of land detached from Pleasant Prairie and added to the City of Kenosha and the Town of Somers as part of the Village's incorporation. Figures for the period between 1970 and 1980 represent population in the former Town of Pleasant Prairie.

Source: U. S. Bureau of the Census.

The 2035 Village population projection of 31,205 was prepared as part of the multijurisdictional plan with the 2010 population adjusted based on U.S. Census data. Estimated population for each five (5) year interval from 2005 to 2030 are based upon estimated projections from the DOA. (The methodology used by the Wisconsin Department of Administration are developed after a well-known cohort-component method using fertility, mortality, and migration rates of population change.) As shown in Table 2.2, from 2000 until 2035 the Village's population is expected to nearly double from 16,136 to 31,204 which represents approximately 9.9% growth every five years and slightly less than 2% growth per year.

Year	Population						
		Number	%				
2000	16,136						
2005	18,701	2,565	15.9%				
2010	20,628	1,927	10.3%				
2015	22,721	2,093	10.1%				
2020	24,870	2,149	9.5%				
2025	26,952	2,082	8.4%				
2030	28,911	1,959	7.3%				
2035	31,205	2,294	7.9%				

TABLE 2.2PLEASANT PRAIRIE POPULATION PROJECTIONS

<sup>a</sup> Population for year 2000 and 2010 is based on the U.S. Census. Population for the years 2005 through 2030 are estimates and projections from the Wisconsin Department of Administration and adjusted for Census 2010 data. The 2035 population projection was derived by the Village based on trend data and analysis completed by SEWRPC as part of the Multi-Jurisdictional Comprehensive Plan for Kenosha County as discussed in Appendix 2-3 of the Village 2035 Comprehensive Plan.

*Source:* U.S. Bureau of the Census, Wisconsin Department of Administration and Village of Pleasant Prairie.

**Age:** In 2010, the residents of Pleasant Prairie had a median age of 41 years, which is older than Kenosha County (36 years). According to the 2010 Census, as shown in Table 2.3, the Village had 5,480 residents under the age of 20, comprising 27.9% of the total population. This proportion of younger residents in the Village has been very consistent since 1990 and 2000, (28.3% and 29.5% respectively). In 2010, there were 2,185 residents age 65 or older, or 11.1% of the population. In 1990 and 2010 this proportion of older residents was only slightly less at 10.5% and 10.6%, respectively of the population. This data depicts that the Village has maintained a relatively stable proportion of younger and older residents over the last 20 years. While there are significantly more young residents than older residents in the Village, 30.6% of the population will be moving into the 65-and-older age bracket within the next 15-20 years as the "baby boom" generation will move into this age group.

## TABLE 2.3POPULATION BY AGE GROUP AND MEDIAN AGE: 1990 AND 2010

			Pleasan	t Prairie			
Age Group	199	0 <sup>a</sup>	200	00	2010		
	Number	%	Number	%	Number	%	
Under 5	803	6.6	1,002	6.2	1,019	5.2	
5 through 9	877	7.3	1,241	7.7	1,413	7.2	
10 through 14	887	7.4	1,396	1,396 8.7		8.2	
15 through 19	834	7.0	1,106	6.9	1,431	7.3	
20 through 44	4,569	38.1	5,764	35.7	4773	20.2	
45 through 54	1,642	13.7	2,457	15.2	3,545	18.0	
55 through 64	1,132	9.4	1,455	9.0	2498	12.6	
65 and older	1,254	10.5	1,715	10.6	2185	11.1	
Total	11,998	100	16,136	100.0	19,719	100.0	
Median Age			37		41		

			Kenosha	County			
Age Group	1990 <sup>a</sup>		200	00	2010		
	Number	%	Number	%	Number	%	
Under 5	10,124	7.9	10,367	6.9	10,995	6.6	
5 through 9	9,785	7.6	11,640	7.8	11,850	7.1	
10 through 14	9,172	7.2	11,826	7.9	12,310	7.4	
15 through 19	9,260	3.9	11,106	11,106 7.4		7.8	
20 through 44	49,955	38.8	56,444	37.7	55,387	33.3	
45 through 54	13,087	10.2	19,257	12.9	26,161	15.7	
55 through 64	10,605	8.3	11,768	7.9	18,014	10.9	
65 and older	16,193	12.6	17,169	11.5	18,679	11.3	
Total	128,181	100.0	149,577	100.0	166,671	100.0	
Median Age			35		36		

<sup>a</sup> Population data by area from the 1990 Federal Census presented in this table are slightly different from the final 1990 population level of the Village presented in Table 2.2. The population levels presented in Table 2.2 represents final population counts from the 1990 Census. The Census Bureau is not expected to reconcile the data regarding population by area with the final total population counts.

Source: U.S. Bureau of the Census.

**Racial Distribution:** According to the U.S. Bureau of the Census, in 2010, Pleasant Prairie was characterized by a predominately "White" population (91.1%), as compared with 83.8% for Kenosha County as shown in Table 2.4. Furthermore, the proportion of "Black or African American" residents in Pleasant Prairie was considerably lower (2.5%) than the County (6.6%). The proportion of "Asian" residents was comparable to the rest of the County( 1.7% in the Village and 1.4% in the County). This data depicts a relatively homogeneous population.

	Pleasa Prairi		Kenosha County		
Race <sup>a</sup>	Number	%	Number	%	
White Alone	17,964	91.1	139,416	83.8	
Black or African American Alone	488	2.5	11,052	6.6	
American Indian & Alaska Native Alone	75	0.4	814	0.5	
Asian Alone	333	1.7	2,393	1.4	
Native Hawaiian & Other Pacific Islander Alone	12	0.1	89	0.1	
Some Other Race Alone	469	2.4.	7,880	4.7	
Two Or More Races	378	1.9	4,782	2.9	
Total	19,719	100	166,429	100.0	

# TABLE 2.4RACIAL COMPOSITION: 2010

*a* The Federal government does not consider Hispanic origin to be a race, but rather an ethnic group. Source: U.S. Bureau of the Census.

**Employment:** According to the U.S. Census Bureau, in 2010, 73.5 % of Pleasant Prairie's population age 16 and older was in the labor force which has increase from 70.4% in 2000. Of the 73.5% in the labor force in 2010, 73.4% were employed in the civilian labor force and 0.1% were employed in the armed forces. As compared to 2000 wherein 69.8% were employed in the civilian labor force and 0.6% were employed in the armed forces. Table

#### Percentage of **Employed Labor Force Occupation Group** 2000 2010 Management or Professional 33.6 36.9 Service Occupations 12.9 12.1 Sales and Office 26.2 27.6 0.0 Farming, Fishing, Forestry<sup>a</sup> 0.0 **Construction, Extraction and Maintenance** 10.4 8.7 **Production, Transportation** 16.9 14.7

TABLE 2.5Occupation Distribution: 2010

<sup>*a*</sup> Although there are active farmers currently living in the Village, the number is too small to represent as a percentage of population.

Source: U.S. Census Bureau.

2.5 shows the occupational distribution for the Village.

#### HOUSEHOLD CHARACTERISTICS

2000

2010

In 2010, the average household size in Pleasant Prairie was 2.68 persons per household, a decrease since the 1990 and 2000 Census count of 2.83 persons and 2.73 person, respectively. A household includes all persons who occupy a housing unit, which is defined by the Census Bureau as a house, apartment, mobile home, a group of rooms, or a single room that is occupied, or intended for occupancy, as separate living quarters. As shown in Table 2.6, the average household size in the Village was slightly higher than that of the County. A total of 83.6% of the households were owner-occupied in the Village in 2010 as compared to 68.0% in Kenosha County. While the number of households has increased steadily, the average number of persons per household has decreased in the Village since 1960 as shown in Table 2.6. This trend, which has occurred throughout Kenosha County, reflects the fact that family sizes (average number of children per family) have decreased and unmarried persons have increasingly tended to establish their own households rather than live with family.

		AVERAGE HO	USEHOLD SI	ZE	
			Pleasant Prai	rie	
Year	Number of Households	Household Population <sup>a</sup>	Average Household Size	% Occupied	% owner Occupied
1990	4,207	11,897	2.83		

2.73

2.68

15,904

19,493

96.2

93.8

82.6

83.6

TABLE 2.6							
HISTORICAL NUMBER OF HOUSEHOLDS AND							
AVERAGE HOUSEHOLD SIZE							

	Kenosha County									
Year	Number of Households	Household Population <sup>a</sup>	Average Household Size	% Occupied	% owner Occupied <sup>a</sup>					
1990	47,029	125,577	2.67							
2000	56,057	145,553	2.60	93.4	69.1					
2010	69,288	161,825	2.58	90.4	68.0					

*<sup>a</sup>* Excludes persons living in group quarters.

5,819

7,272

Source: U.S. Bureau of the Census and Wisconsin Department of Administration.

The number of households projected for the Village of Pleasant Prairie in 2035 is 11,889, as estimated by the Village as part of the multi-jurisdictional plan as described in Appendix 2.3 of the Village 2035 Comprehensive Plan. It is anticipated that the average household size will decrease slightly to 2.52 persons in 2035 from the 2.68 persons per household in 2010. Table 2.7 provides estimated projections for the Village in five year intervals between 2000 and 2035 based on projections the Wisconsin Department of Administration (DOA) (The methodology used by the Wisconsin Department of Administration was developed after a well-known cohort-component method using fertility, mortality, and migration rates of population change.)

Between 2000 and 2035 the number of households in the Village is expected to more than double from 5,819 to 11,889 which represents approximately a 10.8% growth every five (5) years and approximately a 2.2% growth per year to accommodate for the increase in population from 16,136 in 2000 to a projected 31,205 persons in 2035.

Year	Total	Household	Average Household	Households			
rear	Population	Population <sup>b</sup>	size	Number	Change From F	Preceding Five Years	
				-	Number	%	
2000	16,136	15,904	2.73	5,819			
2005	18,701	18,367	2.68	6,842	1,023	17.6	
2010	19,719	19,493	2.68	7,272	490	6.3	
2015	22,721	22,355	2.60	8,212	940	12.9	
2020	24,870	24,495	2.56	9,252	1040	12.7	
2025	26,952	26,535	2.54	10,231	979	10.6	
2030	28,911	28,427	2.52	11,085	854	8.3	
2035	31,205	29,901	2.52	11,889	804	7.3	

TABLE 2.7PLEASANT PRAIRIE HOUSEHOLD PROJECTIONS<sup>a</sup>

<sup>a</sup> Data for the year 2000 is based on the U.S. Census. Data for the years 2005 through 2030 are estimates and projections from the Wisconsin Department of Administration and adjusted for Census 2010 data. 2035 data was derived by the Village based on trend data and analysis completed by SEWRPC as part of the Multi-Jurisdictional Comprehensive Plan for Kenosha County as discussed in Appendix 2-3.

**b** Excludes persons living in group quarters as defined by the U.S. Bureau of the Census, includes all people not living in households. Two general categories of people in group quarters are recognized: (1) the institutionalized population, which includes people under formally authorized, supervised care or custody in institutions at the time of enumeration (such as correctional institutions, nursing homes, and juvenile institutions), and (2) the non-institutionalized population, which includes all people who live in group quarters other than institutions (such as college dormitories, military quarters, and group homes).

Source: U.S. Bureau of the Census, Wisconsin Department of Administration and Village of Pleasant Prairie.

#### **REVIEW OF EXISTING PLANS**

Another critical step in the park planning process is an examination of relevant planning efforts in the Village. A comprehensive understanding of how the Village has evolved over time and how it has been planning for the future establishes guidelines for the recommendations presented in this Plan. Moreover, a review of existing plans helps identify ways that this Plan should be adapted so that it is consistent with the Village's ongoing goals, objectives, and policies, and coordinated with regional planning efforts.

**A Park and Open Space Plan for the Kenosha Planning District (1980):** In 1980, the Southeastern Wisconsin Regional Planning Commission (SEWRPC) prepared the first Park and Open Space Plan for the City of Kenosha, the Town of Pleasant Prairie, and the Town of Somers. This Plan, which was subsequently updated in 1987, presented recommendations regarding the preservation, acquisition, and development of lands for parks and outdoor recreation. Based on community needs and projected population growth, SEWRPC recommended that the former Town of Pleasant Prairie:

- acquire and develop one regional park and seven community or neighborhood parks;
- provide additional facilities, such as baseball fields and playgrounds in three existing parks; and
- take additional measures to preserve important open space lands in the Town.

A 425-acre site along the Des Plaines River was identified as a potential location for the regional park. This site has since been acquired by Pleasant Prairie and is currently known as Prairie Springs Park. The sand and gravel pit operation that was formerly located on the site was transformed into the 110-acre spring-fed Lake Andrea and surrounding park land.

A Land Use Management Plan for the Chiwaukee Prairie-Carol Beach Area of the Town of Pleasant Prairie (1985): The Chiwaukee Prairie-Carol Beach area is located along the coast of Lake Michigan within the Village (formerly Town) of Pleasant Prairie. Home to a variety of ecosystems, including wetlands, beach dunes, and upland prairies, these lands have been characterized as one of the outstanding resource areas is southeastern Wisconsin. However, despite their ecological importance to the region, the future of these habitats has not always been clear. The land has faced substantial development pressures over the last several decades. Therefore, in the hopes of establishing a land use plan that would address the ongoing conflicts between development and preservation, SEWRPC completed a multi-year land use management plan under the direction of the Town of Pleasant Prairie and Kenosha County for the Chiwaukee Prairie-Carol Beach area.

This Plan proposed the following:

- The maintenance of a preservation corridor that connects the Kenosha Sand Dunes on the north end of the study area with Chiwaukee Prairie on the south end;
- The housing stock in the area should increase from 512 housing units in 1980 to about 1,460 units upon full development;
- The Town of Pleasant Prairie, Wisconsin Department of Natural Resources, and The Nature Conservancy should acquire platted, unimproved lots east of Sheridan Road for preservation under a willing seller-willing buyer basis;
- Appropriate land use and zoning controls should be implemented to preserve and protect lands that have been acquired in the public's interest.

**A Park and Open Space Plan for the Kenosha Planning District (1987)**: In 1987, SEWRPC updated the 1980 Kenosha County Park and Open Space Plan. This effort was initiated primarily to ensure that the County would maintain its eligibility for federal and state grant monies. Updated recommendations for this Plan included:

- Continue to acquire land along Lake Michigan as it becomes available, and provide improved access to the Lake for boating and other water-based activities;
- DNR and The Nature Conservancy should continue to acquire lands within identified environmental corridors and designated natural resource areas;
- Local units of government should plan for and provide park and open space sites located within their area of jurisdiction;
- The County should acquire about 1,273 acres of land along the Des Plaines River. This corridor should serve as the location for a variety of resource-oriented outdoor recreational facilities, including trails, picnic areas, river access points, and support facilities such as parking lots and restrooms.

**A Regional Bicycle and Pedestrian Facilities System Plan for Southeastern Wisconsin: 2010 (1994):** The Bicycle and Pedestrian Facilities Plan was completed in 1994 by SEWRPC. The intent of the Plan was to encourage the use of alternative modes of transportation by planning for the provision of facilities that accommodate bicycle and pedestrian travel. In cooperation with a Technical and Citizen Advisory Committee, SEWRPC proposed a total of approximately 113 miles of bicycle ways within Kenosha County. Roughly 27% of the proposed trails are located in off-street natural resource corridors. In addition, the bicycle ways are designed to link up major activity centers, including Pleasant Prairie parks.

**A Comprehensive Plan for the Kenosha Urban Planning District (1995):** In 1996, the Village adopted the Comprehensive Plan for the Kenosha Urban Planning District, prepared by SEWRPC and the represented communities. The Plan includes a park and open space chapter, which served as an update to the 1987 Park and Open Space Plan for the Kenosha Planning District. The chapter is divided into two components: one for the area-wide planning of regional and multi-community parks, and a second for the local planning of community and neighborhood parks.

**Prospectus for the Preparation of a Multi-Use Trail System Plan for the Village of Pleasant Prairie (1995):** Shortly after the completion of the Prairie Springs Park along the Des Plaines River, Pleasant Prairie citizens and officials recognized the need to begin to interconnect their Village park system by developing a long-range, multi-use trail system. In order to provide a framework for a future Multi-Use Trail Plan and a basis for park and recreational budgetary decision making, the Village established an advisory committee comprised of members of the Village Board, the Plan Commission, the Park Commission, SEWRPC, and other technical and professional individuals. The prospectus that resulted from the advisory committee's research addressed the need for, as well as the scope, content, organization, budget, and timeline of the work that would be necessary to prepare the Multi-Use Trail Plan.

The Village has since completed a preliminary bike trail system map for the purposes of conducting an impact fee assessment. Additional trail planning has been conducted as part of the 2006 Park and Open Space Plan and Village Pedestrian Trail Plan (2010).

A Regional Natural Areas and Critical Species Habitat Protection and Management Plan for Southeastern Wisconsin (1997): Beginning in 1991, SEWRPC staff conducted a natural areas and critical species habitat protection and management study to guide the preservation of the most crucial remaining natural areas and critical species habitats in Southeastern Wisconsin. The 1997 Plan that resulted from the study presented the locations of 12 natural area sites and four critical species habitat sites in the Village. None of the identified sites were under protective ownership at the time of the report. However, because these sites make significant contributions to the biodiversity, scenic beauty, and overall integrity of the region, it was recommended by SEWRPC that they be preserved, protected, and managed into the future (See Maps 1a and 1b).

**Kenosha County Park and Open Space Plan (1999)**: This 1999 amendment added one full paragraph to the 1987 Park and Open Space Plan, which recommended the development of an additional county park in the western portion of Kenosha County (outside Pleasant Prairie). The amendment also included an updated outdoor recreation map for the year 2020. The map depicts the location of a proposed trail within Pleasant Prairie running from the Wisconsin/Illinois border northward to Prairie Springs Park, around the eastern perimeter of the Park, and continuing north to meet up with the CTH C on-street bike route.

**A Comprehensive Plan for the Des Plaines River Watershed (2003):** The purpose of the Des Plaines Watershed planning program and the resulting report was to help mitigate water resource-related problems by developing a plan that would guide water resource conservation and management into the future. Ultimately the Plan included four elements: a land use and park and open space element, a floodland and stormwater management plan element, a water quality management plan element, and a fisheries management plan element. The land use and park and open space element reflected the recommendations included in both the Comprehensive Plan for the Kenosha Urban Planning District and the Regional Bicycle and Pedestrian Facilities System Plan for Southeastern Wisconsin: 2010.

The Plan positions the Village within the context of its larger bioregion (an area characterized by a particular ecological community and bounded by natural rather than artificial borders) and defines the connection between park and open space areas and the overall health of the Des Plaines watershed.

**A Land and Water Resource Management Plan for Kenosha County: 2000-2004:** Directed by the Kenosha County Land and Water Conservation Committee, the Land and Water Resource Management Plan for Kenosha County was intended to guide future natural resource-related programs in the County, particularly with respect to nonpoint source water pollution. The Plan seeks to identify and prioritize land and water-resource related problems, establish goals and objectives to address the problems, develop an implementation plan to meet the goals and objectives, and design public informational and educational programs to incorporate County residents. The Plan presents recommendations and priority actions by watershed. Some of the recommendations and priority actions that may relate to this Park and Open Space Plan are as follows:

- Support implementation of the Land Use Management Plan for the Chiwaukee Prairie—Carol Beach Area and the Natural Areas and Critical Species Habitat Plan;
- Establish conservation buffers around riparian corridors, shorelands, and wetlands; and
- Track the level of protection for environmentally significant lands, such as those identified in the regional Natural Areas and Critical Species Habitat Plan.

**Park and Open Space Plan 2006-2011:** Adopted in 2006 and in 2009 as a component of the Village Comprehensive Plan, this plan was used to proactively plan for the Village's future park and recreation needs and set forth specifics goals, objective and recommendations to guide the acquisition, preservation, and development of land for parks, recreation trails, and other open spaces to meet the needs of a growing and changing population. Although, the Plan addresses the long-range park and open space needs of the

community it also focuses on recommendations for development over the five year time frame (2006 and 2011).

**Village of Pleasant Prairie, Wisconsin 2035 Comprehensive Plan:** The Village of Pleasant Prairie 2035 Comprehensive Plan approved in December of 2009 provides a long-range guide for the Village officials, staff, and citizens to effectively address future development and while protecting natural resource in the Village through the year 2035, and sets forth Village planning goals and objectives. The 2035 Comprehensive Plan is an update to the 2010 Comprehensive Plan completed in 1996. In preparing this Comprehensive Plan update, the Village reviewed the existing land use and related plans, updated those plans as necessary to comply with the comprehensive planning law to reflect changes that have occurred since the plans were adopted.

The 2035 Comprehensive Plan, which meets the requirements of the *Smart Growth* laws in the State of Wisconsin, serves as a master or comprehensive plan for the Village and will continue to be used to guide development decision making and projecting the direction of future growth in the Village. This Comprehensive Plan provides information related to existing and planned land uses, protecting and guiding the existing and proposed population and housing growth, protecting the natural resource base, planning for future residential, commercial, industrial uses, and transportation facilities, public parks, recreational facilities, and other community facilities and utilities, and protecting and guiding the economic viability of the Village. The Plan strives to continue to achieve a well-balanced, quality of life for the community while promoting the Village as a "Great Place to Live, Work and Play".

The Village Board and affected Commissions and Committees of the Village shall refer to the Comprehensive Plan as a matter of course in their deliberations on planning issues and give the Plan due weight when making decisions on such matters, particularly with regard to the Statutory requirements for consistency between the Village Comprehensive Plan and the Village Zoning and Land Division and Development Control Ordinances. In addition, the Comprehensive Plan is intended to provide general awareness and understanding of Village planning goals and objectives by residents, landowners, developers, business community, and other private interests, and among the many government departments with land use related responsibilities within the Village.

**Village of Pleasant Prairie 2010 Bicycle and Pedestrian Trails Plan:** The 2010 Bicycle and Pedestrian Trials plan was adopted on April 19, 2010 to guide the development of shared-use paths, demarcate on-street facilities, provide design guidelines and policies for facilities and highlight funding opportunities for the Village to pursue. The Plan builds on the past bicycle and pedestrian planning, including that in the Village's 2006 Park and Open Space Plan and the Southeast Wisconsin Regional Planning Commission's (SEWRPC) Regional Transportation Plan for Southeastern Wisconsin: 2035, A Regional Land Use Plan for Southeastern Wisconsin: 2035, and Amendment to the Regional Bicycle and Pedestrian System Plan for Southeastern Wisconsin: 2020. The Plan adopted as a component of the Village 2035 Comprehensive Plan should be updated every five years to reflect the needs of the community and progress that has been made in implementing new facilities.

#### **CHAPTER 3**

#### **GOALS, OBJECTIVES AND STANDARDS**

#### INTRODUCTION

Planning is a rational process for formulating goals and objectives and through the preparation and implementation of plans, meeting those objectives. The formulation of objectives is an essential task which must be undertaken before a plan can be written. The

Village, as part of its 2035 comprehensive planning program completed in 2009, prepared a comprehensive set of park and related open space preservation, acquisition and development objectives. Because the 2035 Comprehensive Plan provides an overview of the park and open space facilities as an integral part of a community-wide system and includes objectives and standards had also been prepared which address regional, community and neighborhood, as well as open space facilities in the Village. This chapter updates the set of goals, objectives, and standards that will serve as the basis for the recommendations in this Plan.



Lake Andrea

#### **BASIC DEFINITIONS**

The following basic definitions are being used to explain the Park planning process:

**Goals:** Goals are broad statements that express the Village's general park and open space priorities. Goals are formulated based on the identification of keys issues, opportunities, and problems that affect the park system.

**Objectives:** Objectives are more specific than goals and are usually attainable through strategic planning and implementation activities. Implementation of an objective contributes to the fulfillment of a goal.

**Standards:** Standards are the quantitative measurements of how well the community's existing facilities are meeting the needs of its residents and to plan for future facilities based on projected population growth.

The following list of goals, objectives, and standards is based on the information that has been presented in this Plan through the public participation plan process.

#### GOALS:

Provide a sufficient number of parks, recreational facilities, and open space areas to enhance the health and welfare of all Village residents and visitors. Protect the many natural, historical, archeological sites and other archeological resources in the Village that contribute to the Village's history and its quality of life. Preserve and enhance the Village's natural resources, parks and open space sites. Provide a comprehensive system of parks and open spaces to enhance the quality of the environment and life and to allow residents with adequate opportunities to participate in resource and non-resource-oriented outdoor recreation activities.



Jerome Creek

#### **OBJECTIVES:**

- 1. Identify, acquire and provide land for quality public outdoor recreation sites and adequate open space in the Village.
- 2. Prepare marketing information and educate the community about the park and recreational opportunities and facilities available in the Village.
- 3. Preserve the Village's natural, cultural and archeological resources and amenities for the benefit of current and future community residents and businesses.
- 4. Provide off-street and on-street interconnected bike and walking trails and related public open space areas that will provide Village residents and community businesses with adequate opportunities to participate in a wide range of outdoor activities.
- 5. Continue to maintain and provide upgrades to the Village's existing park sites and park facilities.

#### **STANDARDS:**

1. **Regional Parks**: Regional parks are large outdoor recreation sites that are able to accommodate visitors from multiple counties. Such sites contain both active recreational and passive natural resources. That is, they provide amenities for active recreational opportunities and nature-based recreational activities for area residents to enjoy in the natural environment.



Prairie Springs Park

Service Area: 10-mile service radius.

Desirable Size: 250 acres or more.

**Acres per 1,000 Population**: 5.3 acres (the minimum per capita acreage requirements apply to the total population of the region).

- Active recreational facilities such as areas for swimming and boating, biking/walking/skiing trails, playfields, and indoor recreational facilities;
- Passive recreational facilities such as walking/running trails/paths, picnic/sitting areas, and nature study areas;
- Service buildings for shelter, storage, and restrooms;
- Lighting for security at night; and
- Adequate off-street parking spaces.



Donald H. Wruck Beach Pavilion at Prairie Springs Park

2. **Community Parks:** Community parks are intermediate in size and are able to accommodate visitors from the surrounding community and multiple neighborhoods. These sites focus on both the developed aspects of the park, such as playfields and tennis courts, as well as the natural-resource amenities.

Service Area: 2.0 mile service radius.

#### Desirable Size: 25-99 acres.

**Acres per 1,000 Population**: 2.2 acres (For community and neighborhood parks, the minimum per capita acreage requirements apply to the population of the region residing in urban areas).

- Active recreational facilities such as areas for swimming and boating, biking/walking/skiing trails, playfields, playgrounds, tennis courts, and basketball courts;
- Passive recreational facilities such as walking/running trails/paths, picnic/sitting areas, and nature study areas;
- Service buildings for shelter, storage, and restrooms;
- Lighting for security at night; and
- Adequate off-street parking spaces, where the size of the park and the neighborhood context allow.



Village Green Park



Sledding Hill under construction in Ingram Park



Fishing Pond under construction in Ingram Park

3. **Neighborhood Parks:** Neighborhood parks are small sites designed to accommodate the surrounding neighborhood. Like community parks, neighborhood parks may or may not contain natural resource elements. The parks provide playground equipment and playfields and can often be found in more densely populated areas.

**Service Area**: 0.5-1.0 mile service radius (A 0.5-mile service radius applies to high-density urban areas; a 0.75-mile service radius applies to medium-density urban areas; and a 1.0-mile service radius applies to lowdensity urban areas).

Desirable Size: Up to 25 acres.

Acres per 1,000 Population: 1.7 acres (For community and neighborhood parks, the minimum per capita acreage requirements apply to the population of the region residing in urban areas).



- Active recreational facilities such as playfields, tennis courts, basketball courts, playgrounds, and ice-skating rinks;
- Passive recreational facilities such as picnic/sitting areas and nature study areas;
- Service buildings for shelter, storage, and restrooms;
- Lighting for security at night; and
- Adequate off-street parking spaces.



Becker Park



Pleasant Prairie Park

4. **School Parks:** School parks have many of the same characteristics as neighborhood or community parks, depending on their size. As such, school parks primarily serve as locations for active recreational facilities associated with school functions; however, these sites can and do benefit the surrounding community during off-school hours. These parks may be owned and maintained by the Village or the Kenosha Unified School District (KUSD), but are open to all neighborhood residents.

Service Area: 0.5-1.0 mile service radius.

**Desirable Size**: Up to 99 acres (School parks are typically the size of neighborhood parks (up to 25 acres), but are sometimes as large as a community park (25-99 acres).

**Acres per 1,000 Population**: 1.6 acres (For Community and Neighborhood Parks, the minimum per capita acreage requirements apply to the population of the region residing in urban areas).

- Active recreational facilities such as playfields, tennis courts, basketball courts, playgrounds, and ice-skating rinks;
- Passive recreational facilities such as picnic/sitting areas and nature study areas;
- Service buildings for shelter and storage;
- Lighting for security at night; and
- Adequate off-street parking spaces.



Pleasant Prairie School Park



Prairie Lane School Park



Whittier School Park

5. **Special Open Space Areas:** Areas of open space that cannot be measured by a quantifiable standard because of their unique and diverse contributions to the community. Special Open Space Areas enhance an overall park and open space system by maintaining and improving the community's natural resource base, accommodating special activities that are not included in other parks, and providing interconnections between isolated parks and recreation areas.



Lake Michigan

Examples of Special Open Space Areas are varied but can include those lands that accommodate passive or special recreational activities,



Des Plaines River

such as golf courses, sledding/skiing hills, marinas, beaches, display gardens, arboreta, and outdoor amphitheaters, areas of historical of archaeological significance, as well as lands that have been protected for their environmental significance or sensitivity and provide limited opportunity for recreational use. Examples of the latter may include water bodies, floodplains, wetlands, shorelands and shoreland setback areas, drainageways, stormwater management basins, conveyance routes, environmental corridors or isolated natural areas, wildlife habitat, areas of rare or endangered plant or animal species, prairie remnants, and restoration areas.

Service Area: Variable-depends on function.

Desirable Size: Variable-depends on function.

**Acres per 1,000 Population**: Variable- most Special Open Space Areas are not included in the overall community calculation of park and recreation space per 1,000 persons.

Basic Facilities and Activities: Variable, but some may include:

- Active recreational facilities such as areas for swimming and boating, skiing hills, biking/walking/running/skiing trails and paths, skating rinks, and golf courses;
- Passive recreational facilities such as walking trails, picnic/sitting areas, and natural study areas;
- Service buildings for shelter, equipment storage/rental, concessions, and restrooms;
- Nature centers and educational facilities;
- Signage, trail markers, trash receptacles, information booths;
- Lighting for security at night; and
- Off-street parking spaces if appropriate to the area.

6. **Recreation Trails:** See the *Village of Pleasant Prairie 2010 Bicycle and Pedestrian Trails Plan* as adopted on April 19, 2010 by Ordinance #10-27 for information related to Recreational Trails.



Prairie Farms Trail under construction



Barnes Creek Bridge



Prairie Farms Trail under construction



Lake Andrea Trail



Kenosha County Bike Trail

7. **Outdoor Recreational Facilities:** The Southeastern Wisconsin Regional Planning Commission (SEWRPC) also developed, and this Plan endorses, standards for the provision of outdoor recreational facilities. Such standards are expressed as a ratio of the number of facilities recommended per 1,000 residents (Table 3.1).

OUTDOOR RECREATIONAL FACILITIES								
Facility	Facilities per 1,000 Residents							
Baseball Diamonds	0.10							
Basketball Goals	1.13							
Ice-skating Rink	0.15							
Playfield	0.50							
Playgrounds	0.42							
Softball Diamonds	0.60							
Tennis Courts	0.60							
Soccer Fields	0.86							
Swimming Pool	0.15							
G GEWINDG								

#### TABLE 3.1 PER CAPITA REQUIREMENTS FOR OUTDOOR RECREATIONAL FACILITIES

Source: SEWRPC.



Ball Field at Prairie Springs Park



Lake Andrea

8. Accessibility-Universal Design and ADA Compliance: Park and recreational facilities must be designed to be barrier-free and accessible to all park users whenever possible to meet the guidelines of the Americans with Disabilities Act (ADA). Furthermore, universal design standards serve as guidelines for complying with federal ADA regulations, and are required for most grant-in-aid programs.

### **CHAPTER 4**

#### **EXISTING PARK AND RECREATIONAL FACILITIES**

The following is a summary of the existing park and recreational facilities within the Village. These facilities are depicted on Maps 4.1 and 4.3 located at the end of this Chapter. A summary of the outdoor facilities within each Village Park is provided in Table 4.1 of this chapter.

#### **REGIONAL PARKS**

**Prairie Springs Park:** This 930-acre park is located in the western portion of the Village, along the Des Plaines River, north of 104th Street a/k/a State Trunk Highway (STH) 165 and west of 88th Avenue as shown on Map 4.1. Prairie Springs includes a 110-acre spring-fed lake, Lake Andrea, which features a beach with designated swimming areas. The Lake also accommodates fishing, wind surfing, sailing, canoeing, and kayaking. The park offers three (3) playgrounds, six (6) baseball/softball fields, nine (9) designated picnic areas, archery field, two (2) volleyball pits, two (2) miles of nature trails, an asphalt paved 2.3 mile path around the Lake and several parking lots providing In addition, the park provides areas of open land that can be used as 847 spaces. playfields or picnic areas. There are two (2) pavilions in the park. An open-air ballfields pavilion with a concession area and restrooms are located on the south side Lake Andrea. The Donald H. Wruck Pavilion is located on the west side of Lake Andrea and provides an indoor kitchen, wood fireplace, restrooms and seating accommodations for groups and gatherings up to 100 people. The Village-owned RecPlex recreational complex is located within Prairie Springs Park along the eastern shore of Lake Andrea, and will be discussed in more detail later in this chapter.



Fishing at Lake Andrea



Nigh game at Ball Fields



Donald H. Wruck Beach Pavilion



Ball Fields Pavilion

#### **COMMUNITY PARKS**

Anderson Park: This 95-acre park is situated just north of the Village's jurisdictional northern boundary in the City of Kenosha (8730 22nd Avenue) as shown on Map 4.1. The park is adjacent to Tremper High School and Jane Vernon Elementary School and is equipped to accommodate a wide variety of recreational activities serving the Kenosha/Pleasant Prairie area. Amenities include an asphalt paved, multi-use walking trail that extends within and around the perimeter of the entire park, one baseball diamond, one fastpitch softball diamond (Tremper High School girls home field), 20



soccer fields (some of which are open for public use by permit), a concessions building/restroom/open air pavilion, a football/soccer stadium (Ameche Field which is available only by permit), a water park with a swimming pool/pool lockers/concessions, a skateboard park, a pond that can be used for fishing in the warmer months, parking areas and a playground. Although this park is located just outside of the Pleasant Prairie Village limits, its proximity as well as its capacity to accommodate visitors for a variety of recreational opportunities from the surrounding community and multiple neighborhoods, justifies its mention in this Plan.

**Ingram Park:** This 30.7-acre park is located at the 5700 block of 95<sup>th</sup> Street as shown on Map 4.1 on land that was donated to the Village in 2008 by residents John and Dorothy Ingram. A Conceptual Park Plan (See Conceptual Plan in Chapter 8) was prepared and approved for this park in 2010. Existing improvements completed in 2011 and 2012 included the park roadway, entrance monument sign, fishing pond, sledding hill and gravel parking lots. In addition, the park provides areas of open land that can be used as playfields or picnic areas. Additional improvements such as an accessible fishing dock, dog run/park, and other passive amenities are also proposed to be developed as the park funding become available.



**Pleasant Prairie Park**: This 22-acre park is located at the intersection of 104th Avenue and Bain Station Road on the western edge of the Village about three-fourths of a mile east of the Des Plaines River as shown on Map 4.1. Amenities include two (2) softball diamonds and spectator stands, a playground, restrooms and two (2) parking areas to accommodate 125 vehicles. In addition, the park provides areas of open land that can be used as playfields or picnic areas.



**Village Green Park:** This 29-acre park is located south of 97<sup>th</sup> Street and east of Cooper Road on land that was dedicated by the developers of the Meadowdale Estates, Village Green Heights and Devonshire Subdivisions as shown on Map 4.1. The park is directly northwest of the Village's planned mix-use commercial/retail are referred to as the Village Green Center. Proposed park improvements shown on the 2006 approved Conceptual Park Plan (See Conceptual Plan located in chapter 7) include basketball, volleyball and tennis courts, baseball field, soccer field, playground, shelter and adjacent trails. Served by future parking and centrally located in the Village, this park would provide an ideal location for a trailhead. It is intended that the far western and eastern portions of the park will remain passive, connected by a multi-use path that will run the entire length of the park. This path will continue to link the Village Green Park with

other parks in Pleasant Prairie. With the park grading completed, playground equipment and walking trail were installed by the Village in 2012. In addition, the park provides areas of open land that can be used as playfields or picnic areas. Additional park improvements are also proposed to be developed as the park funding becomes available.



#### **NEIGHBORHOOD PARKS**

**Becker Park**: This small, 1.5-acre park is located at the intersection of 76th Street and 48th Avenue within the Mid-West Highlands Subdivision along the northern perimeter of Pleasant Prairie as shown on Map 4.1. Becker Park has one (1) youth softball field, a playground and open space for passive recreational activities.



**Carol Beach Park**: This 2.9-acre park is located at the intersection of 111th Street and 9th Avenue on the eastern side of Pleasant Prairie near the coast of Lake Michigan as shown on Map 4.1. Carol Beach Park offers a softball field, an open-air pavilion and adjacent picnic area, a basketball court, a playground and open space for passive recreational activities.



**Creekside Park (South):** This small 1.69acre park is located on the west side of Creekside Circle just north of 93<sup>rd</sup> Street at 62<sup>nd</sup> Court in the Creekside Crossing Development as shown on Map 4.1. Recent improvements completed in 2012 by the Village in Creekside Park include the installation of playground equipment. In addition, the park provides areas of open land that can be used as playfields or picnic areas.



**Rolling Meadows Park**: This 3.5-acre park is located at the intersection of 100th Street and 32nd Avenue in the eastern portion of the Village as shown on Map 4.1. Rolling Meadows Park features a playground, a picnic area, an open space area for passive recreational activities and direct access to the paved Kenosha County bike/walking trail.





**Lake Michigan Park**: This 4.5-acre linear park site area is located on Lakeshore Drive along the coast of Lake Michigan between 102nd and 113th Streets as shown on Map 4.1. Lake Michigan Park has one designated picnic area, a total of 550 feet of beach along the water's edge, and public access to the Lake.



Pleasant Prairie Park and Open Space Plan

**Woodlawn (Harrison) Park**: This very small, 0.14-acre park is located within a residential neighborhood on the northern edge of the Village at the intersection of Harrison Road and 50th Avenue as shown on Map 4.1. Woodlawn Park includes a small picnic area and playground equipment.



TABLE 4.1SUMMARY OF EXISTING OUTDOOR VILLAGE PARK AMENITIES

Park*	Park Size (Acres)	Archery Ranges	Baseball Diamonds	Softball Diamonds	Flag Football Fields	Basketball Goals (Half-court)	Soccer Fields	Beach (Linear feet)	Fishing	Playground	Sledding Hill	Volleyball Courts
<b>REGIONAL PARKS</b>												
Prairie Springs Park**	930	5	6		8		10	775	Y	3		2
COMMUNITY PARKS												
Ingram Park	30.7								Y		Y	
Pleasant Prairie Park**	22			2			4			1		
Village Green Park	29									1		
NEIGHBORHOOD PARKS												
Becker Park	1.5			1						1		
Carol Beach Park	2.9			1		1				1		
Creekside Park (South)	1.7									1		
Rolling Meadows Park	3.5									1		
Lake Michigan Park	4.5							550				
Woodlawn (Harrison) Park	0.2									1		

\*All parks have open space areas for picnics and playfields.

\*\*Baseball diamonds and softball diamonds are also being used for soccer fields and flag football fields. \*\*\*Indoor restrooms are open during games April through November, dawn to dusk.

# TABLE 4.1 (Cont.) SUMMARY OF EXISTING OUTDOOR VILLAGE PARK AMENITIES

		sli	ts is)	-s' (s	p	it) el	_	•	st ( p	s
Park*	Park Size (Acres)	Nature Trails (miles)	Parking Lots Paved (# of spaces)	Parking Lots- Gravel (# of spaces)	Paths-Paved (Miles)	Paths-Gravel (Linear Feet)	Pavilions (Open Air)	Pavilions (Enclosed)	Picnic Areas (Designated)	Restrooms
REGIONAL PARKS										
Prairie Springs Park	930	2	577	270	2.3		1	1	9	***
COMMUNITY PARKS										
Ingram Park	30.7			30		2,061				
Pleasant Prairie Park	22			125		462				***
Village Green Park	29					1,000				
NEIGHBORHOOD PARKS										
Becker Park	1.5									
Carol Beach Park	2.9		13				1		1	
Creekside Park (South)	1.7									
Rolling Meadows Park	3.5					600			1	
Lake Michigan Park	4.5			25					1	
Woodlawn (Harrison) Park	0.2								1	

\*All parks have open space areas for picnics and playfields.

\*\*Baseball diamonds and softball diamonds are also being used for soccer fields and flag football fields. \*\*\*Indoor restrooms are open during games April through November, dawn to dusk.

#### SPECIAL OPEN SPACE PASSIVE AREAS

There are a number of special or unique private and publicly owned open space park lands that add to the park and recreational significance of the community as shown on Map 4.1. These natural, cultural and recreational resources contain natural areas; critical species habitats; environmental corridors; and woodland and water resources enhance the quality of the environment and the quality of life for the community and provide for a number of opportunities to participate in natural resource oriented outdoor recreation activities.

There are a total of about 622-acres of privately owned open space in the Village or 3% of the Village's total land area. This acreage includes approximately 159-acres of privately-owned, preserved and managed lands by The Nature Conservancy in the Barnes Prairie and Chiwaukee Prairie areas in Carol Beach-Chiwaukee Prairie.

Publicly-owned open spaces lands throughout the Village are owned by the Village, Kenosha County, or State of Wisconsin. Although the lands are not formal parks, they have remained undeveloped either because they are unsuitable for development, they encompass significant natural resources, and they serve as conservancy lands for hunt clubs or will serve another function such as stormwater management. The Village contains a total of approximately 447 acres of publicly-owned open spaces, or 2% or the Village's total land

area.

Opportunities exist for both passive recreational and educational activities for these areas. These activities could include such actions as walking, hiking, bird watching, and environmental education classes/workshops.

**Chiwaukee Prairie-Carol Beach Open Space and Preservation Areas.** There are 482-acres of preservation lands in the Chiwaukee Prairie and it is home to one of the largest prairie complexes in the State and the largest coastal wetlands in southeastern Wisconsin. This area is generally located in the southeastern portion of

Village and extends the from approximately 85<sup>th</sup> Street on the north to the Wisconsin-Illinois State line on the south, from Lake Shore Drive on the east to the Union Pacific Railroad on the west. The southern portion of Chiwaukee Prairie south of 116<sup>th</sup> Street is currently managed by The Nature Conservancy, and the northern portion north of 116<sup>th</sup> Street is managed by the Wisconsin Department of Natural Resources, including the Kenosha Dunes, which contains open and stabilized sand dunes. The Chiwaukee Prairie is characterized by a beach dune ridge and swale complex. High quality wetlands and prairie area associated with the ridges and swales. Most of the area is identified by primary environmental corridor which contains the best remaining elements of the natural resource base-wetlands, prairies, natural and scientific conservancy areas. More than 400 vascular plant species and 75 grassland and wetland bird habitat areas are located in this area. The Wisconsin Scientific Areas Preservation Council has identified scientific and natural areas of statewide significance in this area. The Chiwaukee Prairie is recognized as a National Natural Landmark and is one of the best remaining examples of Lake Michigan shore low prairie in the upper Midwest.



Kenosha Sane Dunes



Chiwaukee Prairie

In addition, there are six (6) sites in Carol Beach that comprise over 60-acres owned by the Village. Although these sites are non-contiguous, they are critical to the continued preservation of prairie habitat, wetlands, and significant environmental sites. The locations of these six (6) open space sites are as follows:

- West of 7th Avenue and north of 85th Street;
- West of 3rd Avenue and south of 85th Street;
- East of 5th Avenue and north/south of 91st Street;
- southeast of 95th Street;
- East of the Union Pacific Railroad at approximately 101 Street; and
- East of Union Pacific Railroad at approximately 110th Street.

**Country Corner-Tobin Creek Open Space Land**: This 8.5-acre open space is located just north of the western portion of the Tobin Creek, east of 39th Avenue and north of 116th Street. The Tobin Creek and adjacent open space area lies within a secondary environmental corridor and also serves as a storm water collection basin for the adjacent Country Corner Subdivision.



**Des Plaines River Open Space Land**: This 750-acre open space area is generally located east of IH-94 generally between STH 50 (75<sup>th</sup> Street) and STH 165 (104<sup>th</sup> Street). This environmentally sensitive land is largely located within a primary environmental corridor and contains the Des Plaines River and water related resources including floodplains, shorelands, wetlands and wildlife habitat areas associated with the waterway.



**Momper's Woods**: This 26-acre site was donated to the Village in 2000 and is located in the central portion of the Village on the west of STH 31 just south of 108th Street. The area is primarily an oak woodland area and has been preserved for use as a future educational environmental resource center and park. Archaeological surveys completed by Great Lakes Archeological have confirmed that the site is historically significant, as it is home not only to preserved remnants of the Jambeau Indian Trail, but also to several prehistoric Native American campsites that span 10,000 years or more of human history. Currently, this area is not open to the public. However, the Village has future plans to preserve the significant woodlands and develop an educational nature center with outdoor classroom space while maintaining the significant historical trails (See the Conceptual Plan in Chapter 7).





**Prairie Trails West Open Space Land**: This 8acre area of open space is located within a secondary environmental corridor adjacent to a tributary of the Tobin Creek. The open space is east of 39th Avenue and north of 122nd Street and was dedicated to the Village during the platting of the Prairie Trails Subdivision.



**Sorensen Woods at Kildeer Farm:** This 6.56-acre conservancy land area is located at about the 11400 block on the west side of 47<sup>th</sup> Avenue. This future oak savanna park area was acquired in 2010 through a land donation from Village resident Jay Sorensen. The land was originally an open oak and hickory woodland and oak



savanna in 1835. The site filled in considerably in the 20<sup>th</sup> century to a wooded closed-canopy forest condition. There is a narrow band of wetlands traversing through the western portion of the park property. The land is located within a secondary environmental corridor and is intended to be preserved, protected, enhanced and restored as a significant woodland area. Currently this area is not open to the public and no parking areas are identified.

#### Other Significant Open Space Lands:

**Primary Environmental Corridors**: These lands have been identified for protection in the Village's Comprehensive Land Use Plan because they contain especially high value natural, scenic, historic, and recreational resources. Primary environmental corridors are, by definition, at least 400 acres in size, at least two (2) miles long and

at least 200 feet wide. The Village's 2010 land use inventory identified approximately 3,071 acres of primary environmental corridors, or 14% of the Village's total land area as shown on Map 4.2.

**Secondary Environmental Corridors**: These lands are often remnants of primary environmental corridors that have been partially developed or converted to agricultural use. By definition, they are at least 100 acres in size and at



Des Plaines River Floodplain part of the Primary Environmental Corridor

least one (1) mile long, unless serving to link primary environmental corridors. The Village's 2010 land use inventory identified approximately 897 acres of secondary environmental corridors, or 4% of the Village's total land area as shown on Map 4.2.

**Isolated Natural Resource Areas**: These lands represent smaller concentrations of high-quality natural resources that are separated from environmental corridors. Such areas must be at least five (5) acres in size and at least 200 feet wide. The Village's 2010 land use inventory identified a total of approximately 702 acres of isolated natural resource areas, or 3% of the Village's total land area as shown on Map 4.2.

#### SPECIAL OPEN SPACE ACTIVE AREAS

**Golf Courses/Driving Ranges**: There are two (2) privately-owned golf-related facilities located in Pleasant Prairie. The first facility is Big Oaks Golf Course, a 167.68-acre 27-hole course, driving range and club house is open to the public. Big Oaks is located just east of STH 31 along the Wisconsin/Illinois State line at 6117 123<sup>rd</sup> Place as shown on Map 4.1. The second facility is a 14-acre golf driving range known as the Transcendental Golf Driving Range, located in the southeastern portion of the Village just east of Sheridan Road at 1621 116th Street.



Big Oaks Golf Course

**Halter Wildlife, Inc.:** This 644-acre private hunting and fishing club and conservancy is privately-owned by the Des Plaines Wetlands Conservancy. Halter Wildlife is located at 9626 113<sup>th</sup> Street, just west of 88<sup>th</sup> Avenue (a/k/a CTH H) in the



Village as shown on Map 4.1. Bird hunting at the club includes chucker, pheasant, quail and turkey. Hunters bring their own dogs can and dogs/guides are also available. The membership-only club provides the following amenities: a clubhouse, open pavilions, playground, air а а restaurant and fishing pond.

**Prairie Harbor Yacht Club**: This 151-slip yacht club facility is a privately-owned marina that has a "condominium style" of boat slip ownership as well as slips for rent. Prairie Harbor Yacht Club is located at 12800 Lake Shore Drive adjacent to the Chiwaukee Prairie Nature Preserve in the southeastern corner of the Village as shown on Map 4.1. The club house provides a gathering space, kitchen, laundry facilities and heated pool. Prairie Harbor contracts with Skipper Marine Development, a division of Skipper Bud's, to professionally manage the facility.



#### **RECREATIONAL TRAILS**

**Chiwaukee Prairie Trails:** There are four (4) nature trails which meander through the Chiwaukee Prairie south of 116th Street and east of the Union Pacific Railway in the southeastern corner of the Village. Nature walkers can enjoy the trails however the picking of flowers, digging plants, or collecting seeds is prohibited. The trails follow the paths of the former Village roadways that were platted in the Chiwaukee Subdivision from the 1920's-1950's before conservancy efforts to preserve the prairie had started. The four (4) trails are named in honor of individuals who began efforts to preserve the Chiwaukee Prairie in the mid-1960s as shown on Map 4.3.



**The Gen Crema Trail:** A park bench built by a local Eagle Scout sits at the trailhead of the Gen Crema Tail. The tops of sawed-off telephone poles can be seen along the path. The poles were put up to block the vehicular traffic from entering on the trail. Plants have since grown over the tops of the poles. The footpath extends south and along the side are metal stakes with numbers that mark the various prairie plants. Maps are not available on-site, but the plants correspond to the numbers on a map key available on the Chiwaukee Prairie Preservation Fund, Inc. web site: www.chiwaukee.org

**The Al Krampert Trail:** From the trailhead at 121st Street and Second Avenue, the Al Krampert Trail can be traveled by vehicle. The trail heads north and ends at the Phil Sander Trail. A kiosk with prairie information stands at the start of the Al Krampert Trail.

**The Jim Olson Trail:** The Jim Olson footpath trail was formerly a dirt road. The footpath begins at 122nd Street and Second Avenue and extends west to the Union Pacific Railroad.

**The Phil Sander Trail:** Once known as 117th Street, the Phil Sander Trail is now a footpath that runs from the north end of the Krampert Trail south to the Gen Crema Trail.

**Kenosha County Bike Trail**: The only intercommunity bicycle trail in Pleasant Prairie is the Kenosha County Bike Trail which is located at approximately 30<sup>th</sup> Avenue in the Village as shown on Map 4.3. Recently paved by Kenosha County in 2012, this 10-foot wide trail runs from south of the Wisconsin/Illinois border and extends north through the Village into the City of Kenosha where it connects with the Pike Bike Trail and runs north to the Kenosha/Racine County Line.



**Prairie Farms Trail:** The existing eight (8) foot wide gravel trail constructed primarily in 2011, is approximately 1.75 miles long and serves a dual purpose for the Village—as a maintenance access to the sanitary sewer main and manholes for the recently installed sanitary sewer relay main and a pedestrian/walking trail for the community. The Prairie Farms Trail extends from Wilmot Road (CTH C) near Bain Station Road, south and east of the Des Plaines River and extends to the east terminating at 88<sup>th</sup> Avenue (CTH H) as shown on Map 4.3. The trail is not yet open to the public but is intended to be completed in 2013. Future plans including paving the trail and connecting it with trails in Prairie Springs Park.



**Prairie Springs Park Trails:** Prairie Springs Park currently has approximately two (2) miles of grass trails in the woodlands located just west of the Donald H. Wruck Pavilion and Lake Andrea as shown on Map 4.3. These trails are utilized for hiking and exploring nature. There is also a 2.3-mile eight-foot wide, paved multi-use walking/running and bicycle trail that encircles Lake Andrea also shown on Map 4.3. While this trail is a popular recreation facility in Prairie Springs Park, it is not currently connected with any larger network of pedestrian or bicycle facilities in the Village.



Walking Trails in Prairie Springs Park



Multi-use Trail around Lake Andrea

#### **BICYCLE LANES**

**Kenosha County Bicycle Lanes:** In 2013, on-street, striped and paved bicycle lanes are being designed by Kenosha County for installation on both sides of 88<sup>th</sup> Avenue (CTH H) extending from STH 50 (75<sup>th</sup> Street) south to the Wisconsin-Illinois State line.

**Village Bicycle Lanes:** In 2012, on-street, striped and paved bicycle lanes were created on the roadways with the Prairie Ridge commercial development are located north of Prairie Ridge Boulevard between 88<sup>th</sup> and 104<sup>th</sup> Avenues. In 2010, bicycle lanes were delineated on both sides of 85<sup>th</sup> Street between 39<sup>th</sup> Avenue and STH 31.

#### **OTHER VILLAGE OWNED RECREATIONAL FACILITIES**

**Village RecPlex**: The 302,000 square foot RecPlex facility is located at 9900 Terwall Terrace along the eastern shore of Lake Andrea within Prairie Springs Park as shown on Map 4.1. The recreational facility includes a 10 lane 50 meter Olympic size pool; two (2)



National Hockey League-size ice rinks; an 17,000 square foot indoor water park that include 4 lap lanes, a current channel, a play structure and slide; an 18,000 square foot fitness center, a 60,000 square foot field house which provides for indoor soccer fields, basketball courts, volleyball courts, and batting cages; a 1/6 mile suspend track; racquetball courts; a play zone for kids; two (2) concession areas; preschool and child care facilities; several multi-purpose rooms and massage therapy room. Table 4.2 below is a summary of the indoor facilities of the RecPlex



Main entrance of the Rec Plex



Ice Arena in the Rec Plex

#### TABLE 4.2 SUMMARY OF EXISTING INDOOR FACILITIES AT THE VILLAGE RECPLEX

Туре	Number			
Basketball Goals*	40 goals			
Volleyball	12 courts			
Indoor Soccer*	4			
Racquetball Courts	4			
Ice Rinks (NHL)	2			
Swimming Pool	1			
Water Park	1			

\*Various size courts can be accommodated within the 60,000-square foot field house.

The RecPlex is owned by the Village and managed by Pleasant Prairie's Recreation Department. The facilities are completely self-sustaining and are paid for through membership and usage fees. Recognized as one of the Village's most remarkable achievements by the League of Wisconsin Municipalities, RecPlex is the largest recreation facility in Southeastern Wisconsin and one of the largest municipally-owned recreational complexes in the country. Pleasant Prairie's Recreation Department and RecPlex staff coordinate all formal organized recreational activities in the Village. The Village hosts a variety of events including annual triathlons, swimming, wakeboarding and boating championship tournaments and many other events that bring thousands of athletes and spectators into the Village.



Aqua Arena



Field House



Fitness Center



Water Park

#### **PUBLIC SCHOOL PARKS**

Public School Parks and park and open space included land owned and maintained by the Kenosha Unified School District #1 (KUSD). The parks are fully accessible by the public and with the recreational facilities; it functions as a neighborhood park. There are three (3) public school parks in the Village with recreational facilities as described below and illustrate in Table 4.3 and shown on Map 4.1

**Pleasant Prairie Elementary School Park Land:** This 20-acre school park located on the south side of Pleasant Prairie Elementary School at 9208 Wilmot Road in the Village. Amenities include two (2) soccer fields, (a hard surface and a grass field); two (2) basketball goals where each goal is accompanied by a half court; a softball diamond, two (2) playgrounds, two (2) tetherball poles, a playfield, and a hard surface recreation area. The large amount of acreage (11-acres) for this school park facility was donated by VK Development as



a part of the Prairie Ridge Plat. The Village has a Memorandum of Agreement with KUSD that these outdoor park facilities are intended to specifically serve the Prairie Ridge Neighborhood.

Prairie Lane Elementary School Park Land: This 23-acre school park is located on the northeast side of the Prairie Lane Elementary School located at 10717 47<sup>th</sup> Avenue in the Village. It is accessible by the public and with the recreational facilities; it neighborhood functions as а park. Amenities include two (2) basketball goals where each goal is accompanied by a half court; a soccer field; a softball diamond; a volleyball court; a playground; and a playfield.

**Whittier Elementary School Park Land**: This 7-acre school park located on the west and south sides of Whittier Elementary School located at 8542 Cooper Road in the Village. It has public access points from both Cooper Road and 85<sup>th</sup> Street. Amenities include three (3) basketball goals where each goal is accompanied by a half court; two (2) soccer fields, a softball diamond, a volleyball court, a playground, and a playfield.



School Park*	School Park Size (Acres)	Softball Diamonds	Basketball Goals (Half-court)	Soccer Fields	Playground	Tetherball Poles	Volleyball Courts
Pleasant Prairie Elementary School Park	20	1	2	2	2	2	0
Prairie Lane Elementary School Park	23	1	2	1	1		1
Whittier Elementary School Park	7	1	3	2	1		1

TABLE 4.3SUMMARY OF EXISTING OUTDOOR SCHOOL PARK FACILITIES

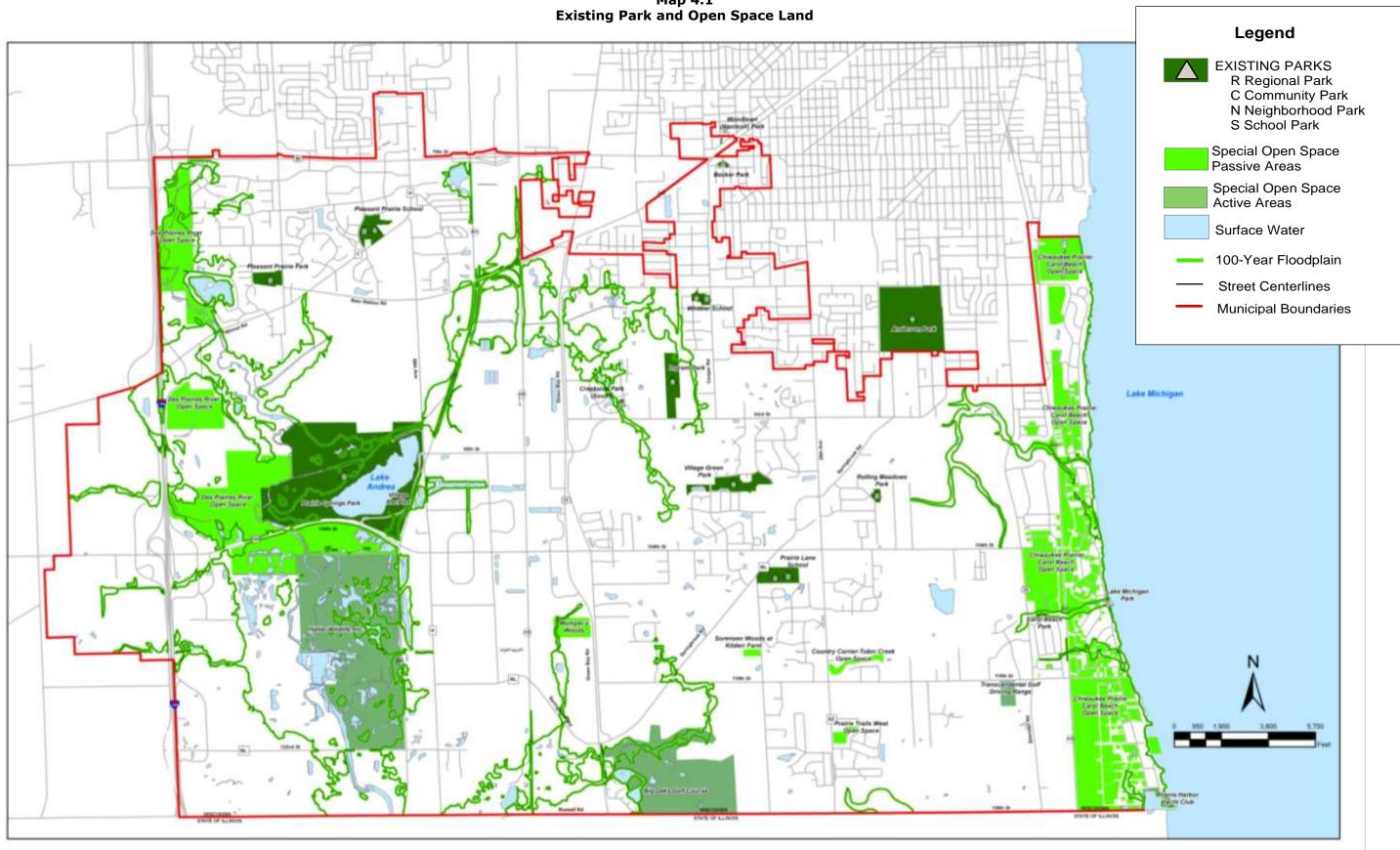
\*All parks have open space areas for picnics and playfields.

## ACCESSIBILITY-UNIVERSAL DESIGN AND ADA COMPLIANCE

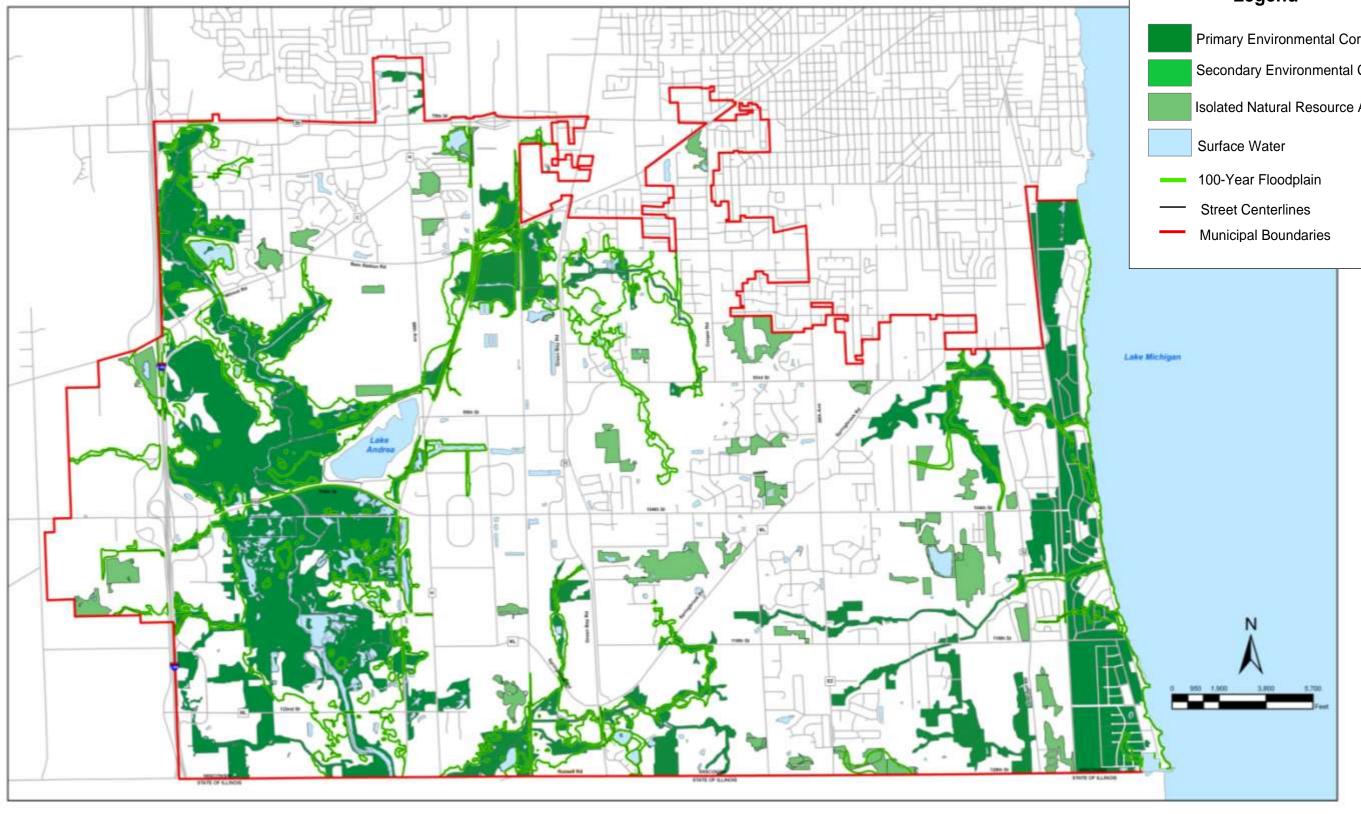
Park and recreational facilities must be designed to be barrier-free and accessible to all park users whenever possible to meet the compliance guidelines of the Americans with Disabilities Act (ADA). Furthermore, universal design standards serve as guidelines for complying with federal ADA regulations and are required for most grant-in-aid programs.

Currently, all facilities at Prairie Springs Park are ADA compliant, including the restrooms. However, the Village's other existing park and recreational facilities to some extent, will require some upgrades to make them more barrier-free and accessible to people with disabilities.

Map 4.1



Map 4.2 Environmental Corridors and Isolated Natural Areas

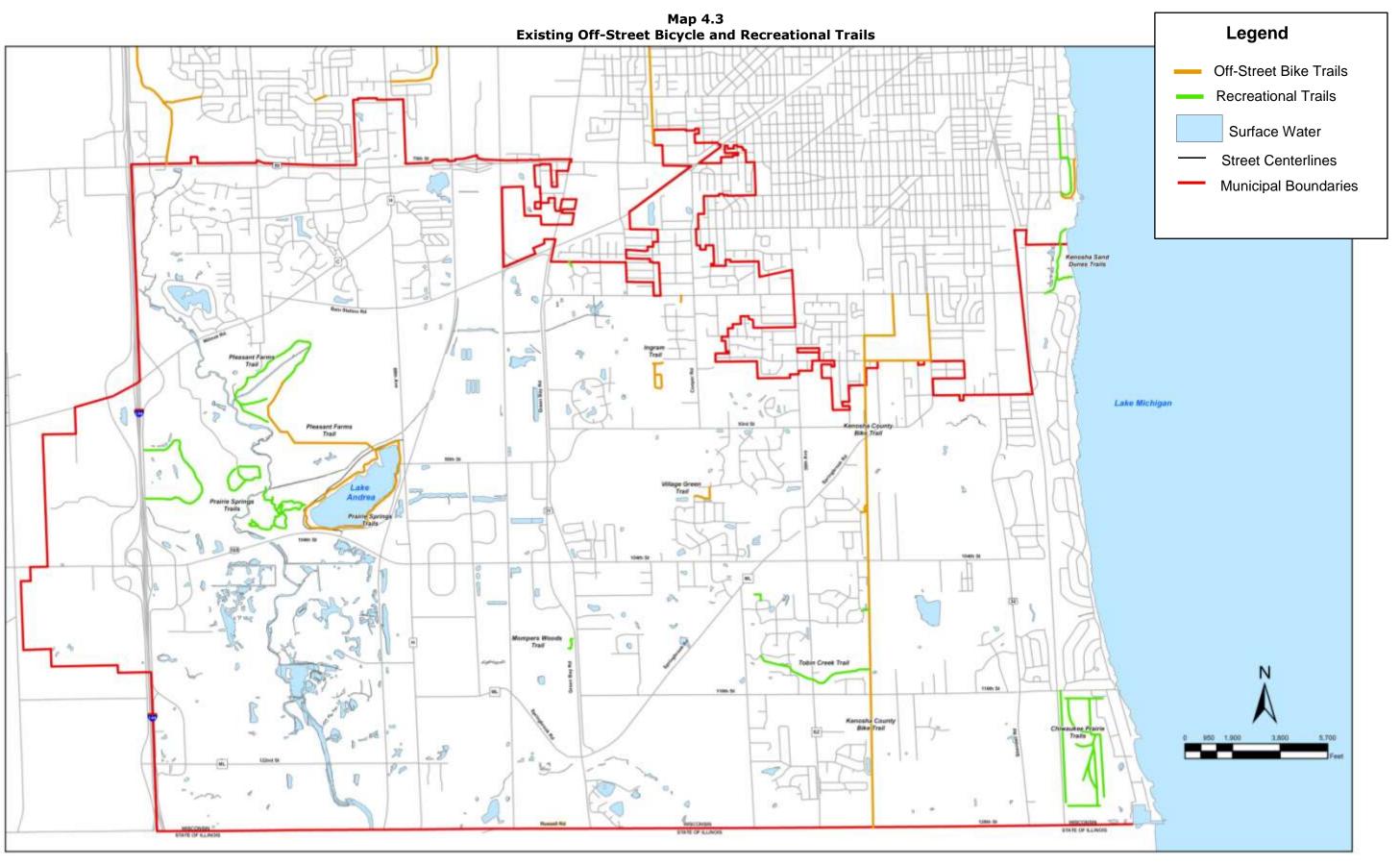


# Legend

Primary Environmental Corridors

Secondary Environmental Corridors

Isolated Natural Resource Areas



# CHAPTER 5

# PUBLIC PARTICIPATION AND PLANNING PROCESS

In the spring of 2011, as part of the planning process for this Park and Open Space Plan update, the Village's park planning consultant, Public Research Group along with their subconsultant, Design Perspectives Inc. began gathering data to be used to assist the community in identifying the Village's future park and recreational needs. A series of open meetings, visioning sessions and survey opportunities were provided by the Public Research Group for the Village residents, RecPlex members, Village Board, Park and Recreation Commissions and Village staff to offer input and share information regarding the existing and future needs of the Village's park and recreational facilities. Each section below provides details regarding the public participation activities conducted and the responses provided by the participants regarding the Village's park and recreational system.

#### COMMUNITY-WIDE SURVEY

The public opinions of the park and recreational service information were gathered through the conduct of a community-wide survey as shown in Appendix A that was compiled through mail, telephone and email surveys administered by the Public Research Group in July and August of 2011. Since this survey was also used for gathering information for the RecPlex Recreation Master Plan, the community-wide survey also included some City of Kenosha residents who were RecPlex members that live just outside of the Village's corporate limits. The primary focus of the park and recreational survey was to obtain information related to: the Village's park and recreational services, the respondent's recommendations to improve the parks and beaches, the specific amenities that the community would like to see added to the parks and the park project priorities.

**Number of Responses by Survey Method:** The community-wide written survey was administered to a random sample of 1,500 mail-to addresses, from which 405 households responded. The telephone survey was administered to 200 land line households, from which 105 households responded. An email survey was sent to 300 email addresses provided through the RecPlex membership listing, from which 53 responded. See Figure 5.1 below.

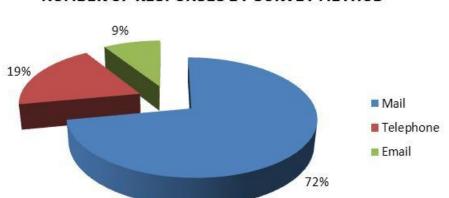


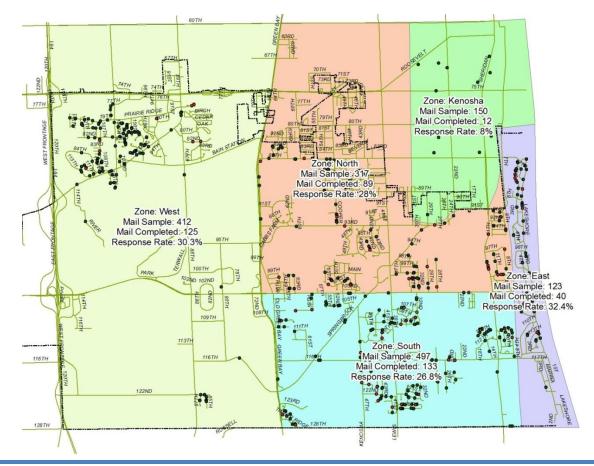
FIGURE 5.1 NUMBER OF RESPONSES BY SURVEY METHOD

The 563 total responses suggest that the community-wide survey was a representative sampling of the park and recreational facility usage behaviors and attitudes of the community within a margin of error of 4% at a 95% confidence level. The questions that were analyzed in this chapter were highlighted in yellow in the survey results as shown in Appendix A.

**Methodology:** There are two (2) primary methods of analyzing survey responses: quantitative and qualitative methods. Quantitative methods are a statistical analysis of survey response where the response can be assigned numeric values and statistics applied. There are many complicated forms of statistical analysis, but in this Plan, the analysis applied uses frequency distributions, one-sample and independent-sample means testing.

Qualitative methods use non-statistical analysis of the responses where statistics cannot be applied. These are where questions require open-ended narratives by the respondents. In the qualitative analysis, similar themes or like ideas are used when there is a recurring message or concept being presented.

**Demographics of the Respondents:** In the community-wide survey, the samples were drawn from both the Village and a small area of the City of Kenosha located in proximity to the northeastern corner of the Village. The following Map 5.1 below identifies the generalized location were the mail and telephone survey respondents reside. The sample survey population is shown as black dots within each of the five (5) zones on the map. The zones were created in the sampling process to verify that each area of the community would be represented in the survey.



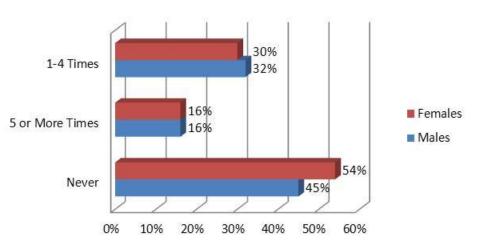
MAP 5.1 GENERALIZED LOCATIONS OF MAILED SURVEY RESPONSES RETURNED

The survey respondents were asked demographic questions which included: the respondent's age, the age of the other persons in the household, the respondents gender, how long they had lived in the Village and how far they commuted to work. The following table shows the average responses received. Table 5.1 below indicates that about two-thirds of all mail, telephone and email respondents were females, the average age of the respondents was 54.06 years and that the respondents lived in the Village an average of 16.77 years.

## TABLE 5.1 DEMOGRAPHIC QUESTIONS

Demographic Questions	Percent Responding	Average
Respondent's Age	96%	54.06 years
Respondent's Gender	99%	66% Female
		34% Male
How Long Have you Lived in Pleasant Prairie?	97%	16.77 years

Due to the popularity of Prairie Springs Park, one of the initial questions focused on Lake Andrea in the Park. The survey respondents were asked, "Whether anyone in the household used the beach at Lake Andrea?" Figure 5.2 below provides the responses. The findings show that 30% of females reported that someone in their household visited the Lake Andrea beach 1-4 times per year compared to only 32% of the males that reported the same 1-4 times per year. Responses to the use of the Lake Andrea beach 5 or more times a year were 16% for both females and males. A higher percentage or 54% of females compared to the 45% of males said no one in their household had ever visited the beach within the past year. One (1) percent of the respondents did not answer the question.

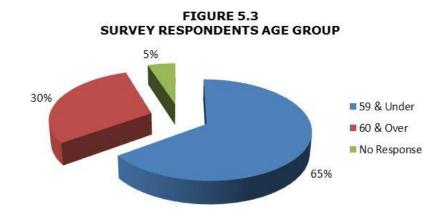


#### FIGURE 5.2 USE OF LAKE ANDREA AND BEACH BY GENDER

Note: 7% of males did not respond to this question.

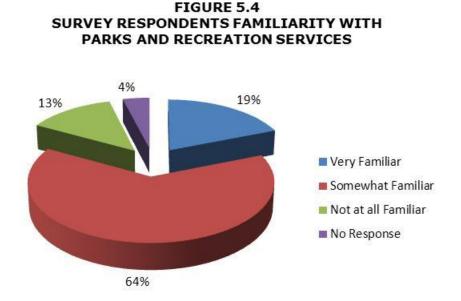
For survey analysis purposes, the age of the respondent does sometimes make a difference in behavior-type questions. Public Research Group has chosen the age 60 to be utilized and defined as the cut-off for an older respondent for survey analysis purposes. Further discussion will be provided in this chapter when comparisons are made between age groups.

Figure 5.3 below shows that the survey respondents age 59 and younger comprised 65% of the survey sample, while those survey respondents age 60 and over comprised 30% of the sample. A total of 5% of the sample did not respond to the question. The survey indicates that the survey is a good representative sampling of the age distribution in the community.



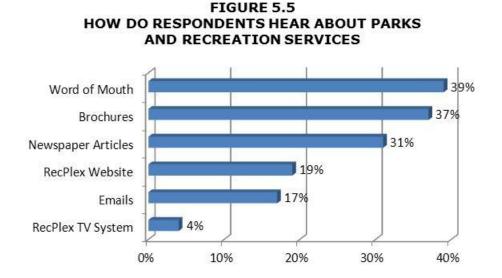
**Communication and Visibility:** Communication and visibility of the Village's park and recreational system are important to the overall usage of the park system. If the community residents are <u>unaware</u> of the public amenities or opportunities that are available to them, it is likely that those facilities will be <u>underutilized</u>.

Question 1 of the community-wide survey asked respondents "If they were familiar with the parks and recreation services offered by the Village". Figure 5.4 below indicates that 19% of mail, telephone and email respondents said they were very familiar compared to 64% of



the respondents who were somewhat familiar and 13% who were not at all familiar with the Village's parks and recreation facilities. Four (4) percent did not respond to the question.

Question 2 of the survey asked, "How do the respondents or other members of their household hear about the Village's parks and recreation services?" Figure 5.5 below shows that that the "word of mouth" response at 39% is the leading method for residents to hear about the Village's parks and recreation services, followed by brochures at 37% and newspaper articles at 31%. The Village's website and emails are used by the residents, but to a lesser extent. Based upon experiences learned by Public Research Group through other communities, marketing materials such as program brochures or the Village's monthly newsletter which is direct-mailed to the resident households is often times the leading method for making residents aware of opportunities in their own community.



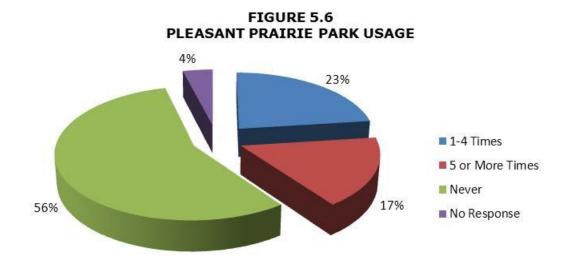
**Park Usage:** Question 11 of the survey asked, "How many times they would estimate that someone in their household used the Village's parks and beaches?" The specific parks that

were included in the community survey question were: Pleasant Prairie Park, Rolling Meadows Park, Becker Park, Carol Beach Park and Prairie Sprinas Park/Veterans Memorial Park. For those persons that responded to the question and indicated that they used the parks 1-4 times in the past 12 months are being defined in this survey as infrequent users. Those respondents that said they used the parks five (5) or more times are considered being defined in this survey as frequent users. Those respondents that said they never used the parks are considered non-users.

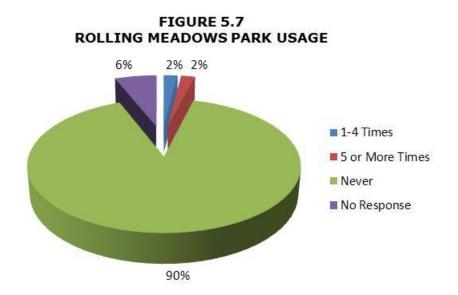


Veterans Memorial at Prairie Springs Park

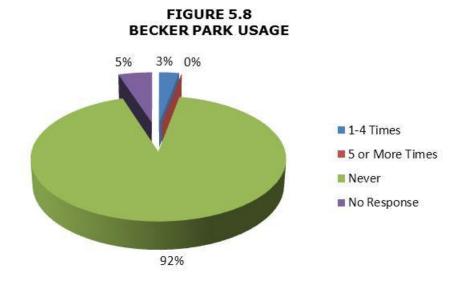
The pie chart in Figure 5.6 below shows that 40% of the respondents said they used Pleasant Prairie Park, located at the intersection of 104<sup>th</sup> Avenue and Bain Station Road, at least once during the past year, 23% use the park infrequently and 17% use the park frequently. Twenty persons did not respond to the question.



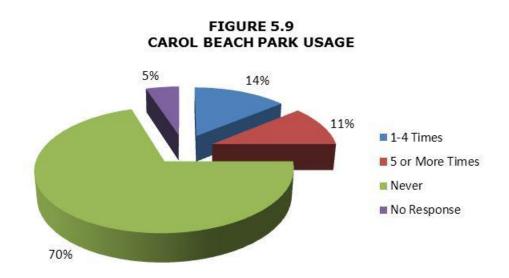
The pie chart in Figure 5.7 below shows that only 4% of the respondents said they used Rolling Meadows Park located at the intersection of  $100^{\text{th}}$  Street and  $32^{\text{nd}}$  Avenue at least once during the past year and 90% indicated they never used the park. A total of 6% did not respond to the question.



The pie chart in Figure 5.8 shows that only 3% of the respondents said they used Becker Park located at the intersection of  $76^{th}$  Street and  $48^{th}$  Avenue at least once during the past year and 92% indicated they never used the park. A total of 5% did not respond to the question.



The pie chart in Figure 5.9 shows that 25% of the respondents said they used Carol Beach Park located at the intersection of  $111^{\text{th}}$  Street and 9<sup>th</sup> Avenue at least once during the past year, 14% use the park infrequently and 11% use the park frequently. A total of 5% did not respond to the question.



The pie chart in Figure 5.10 shows that 37% of the respondents said that they used Prairie Springs Park/Veteran's Memorial located at the intersection of Terwall Terrace and 104<sup>th</sup> Street at least once during the past year, with 28% using the park infrequently and 9% using the park/memorial frequently. A total of 63% of the respondents indicated that they never have used the park, which is somewhat concerning due to the fact that this park/memorial is the largest and most visible community park in the Village and most of the organized recreational activities take place at this location.

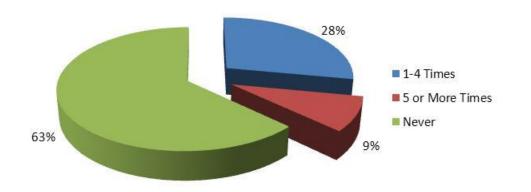


FIGURE 5.10 PRAIRIE SPRINGS PARK/VETERAN'S MEMORIAL USAGE

The pie chart in Figure 5.11 below indicates that 22% of the respondents said they used Chiwaukee Prairie State Natural Area located south of 116<sup>th</sup> Street and west of Lake Michigan at least once during the past year, 14% use the park infrequently and 8% use the park frequently. The Chiwaukee Prairie State Natural Area had lower usage than Prairie Springs Park and Pleasant Prairie Park. Again, it is interesting that 75% of the community never visited this environmentally impressive area of State and National significance in their own community. Three (3) percent of the respondents did not answer the question.

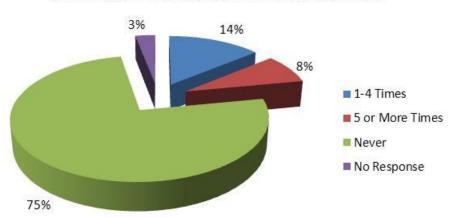


FIGURE 5.11 CHIWAUKEE PRAIRIE STATE NATURAL AREA

In order to calculate the usage of parks on an annual basis, the percentages of infrequent and frequent users can be multiplied by the number of households in the Village to determine how many of the households used each park. Figure 5.12 below compiles this information by park. These estimates can provide an indication as to which parks are being utilized by its residents and may provide some direction in prioritizing the Village's budgetary dollars when making expenditures on park capital improvements.

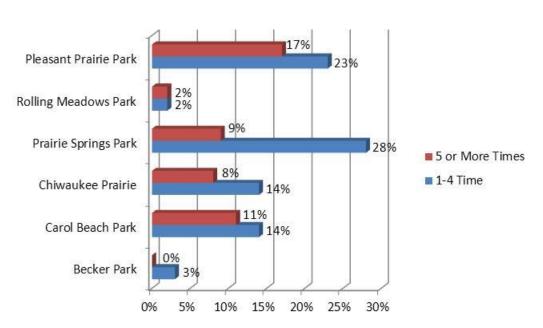


FIGURE 5.12 PLEASANT PRAIRIE HOUSEHOLDS USING EACH PARK DURING THE YEAR

*Note:* The lower usage in Prairie Springs Park my result from respondents no knowing the official name of the park this is the site of the RecPlex, Lake Andrea and many other outdoor amenities.

**Park Amenity Usage:** Park amenities are the physical, educational, and recreational improvements constructed in the parks, which draws the users to the parks. Certain park amenities as discussed in the community-survey questions include the pedestrian and

bicycle paths, playgrounds, picnic shelters, ball diamonds, beaches and archery ranges. Question 12 of the survey asked respondents, "How frequently they used each amenity, either 1-4 times over the past year, 5 times or more, or never".



Lake Andrea at Prairie Springs Park

For the purpose of this survey, bike and pedestrian (running/walking) paths are considered the same amenities but are used for different purposes. The following bar chart in Figure 5.13 below shows the usage of paths in the Village. The graph indicates that 18% of households have used the paths for bicycling 1-4 times during the past year or infrequent usage, compared to 27% of the respondents who were frequent users and 52% who were non-users. A greater percentage of runners/walkers or 55% used the paths over the bicyclists or 45% who used the paths.

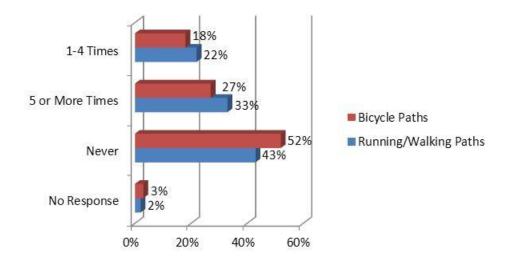


FIGURE 5.13 TYPES OF PATHS USED BY RESPONDENTS

To determine if the bicyclists are the same users as the runners/walkers, a correlational analysis was performed on the respondents who said they never bicycled on the paths or ran or walked on the paths. By using a bivariate analysis, one can determine what percentage of respondents would have answered no to both questions. That percentage represents the true non-users of the paths because some respondents may have used the paths for biking and others for running/walking and others for both. The respondents who answered no to both would be counted.



Multi-use Trail around Lake Andrea

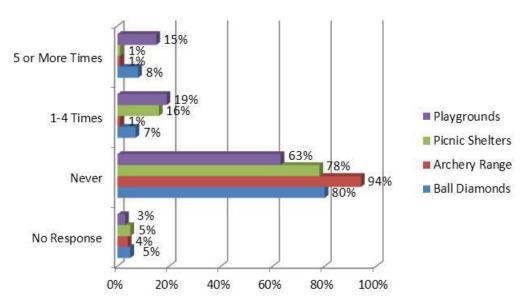
In this case, the Pearson Correlation Coefficient analysis completed by the Consultant was moderately strong at .452, suggesting that 45% of the respondents answered that they never used the paths for biking or running/walking. In other words, 55% of those who said they never ran or walked on the paths could have bicycled on them, and 55% who said they bicycled on the paths also ran or walked on them. Based upon the survey results, a conclusion can be drawn that the paths are a significant element to the community's park and recreational system for biking, running and walking.

Other amenities, such as playgrounds, picnic shelters, archery range and ball diamonds do not have nearly the same usage as the paths, but their significance to responders is shown in Figure 5.14. The graph shows that playgrounds at 34% and picnic shelters at 17% are the most frequently used park amenities after the paths. The highest usage of picnic shelters is in the 1-4 times a year category and playground usage is split between frequent usage (1-4 times a year) at 19% and infrequent usage (5 or more times a year) at 15%. Ball diamonds were reported to be used by 15% of the community-wide survey respondents at



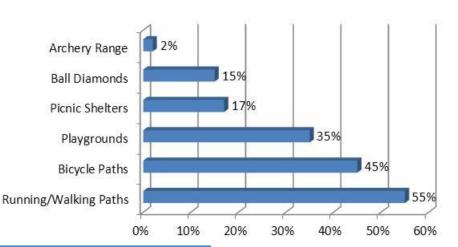
Ball Fields Pavilion area during Prairie Family Days

least one time a year. The archery range located in Prairie Springs Park was used by only 2% of respondents and 94% said they had never used the archery range.





The following bar graph shown in Figure 5.15 below compares the overall usage of the six (6) park amenities in survey Question 12 by totaling the frequent and infrequent usage of them. A total of 55% of the community-survey respondents indicated that they use the running/walking paths in the parks. Based upon the number of respondents answering this question, it appears that running/walking paths and bicycle paths should be a priority in both maintenance and upgrades in planning for improvements to park amenities in the Village's park and recreation system.



#### FIGURE 5.15 TOTAL PARK AMENITY USAGE



Picnic Area 1 at Prairie Springs Park



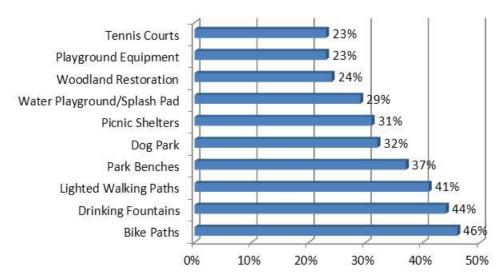
Paved Path at Prairie Springs Park



Playground at Prairie Springs Park

**Preferences for New Park Amenities:** Question 18 asked, "Which of the following amenities that the community-wide survey respondents would like to see added and/or renovated in the parks in the next five (5) years?" The list included a range of selections with the top ten identified in Figure 5.16 below. The graph shows that the top 10 answers provided by the respondents indicate that there is a strong interest in constructing more bike paths at 46%; drinking fountains at 44%; lighted walking paths at 41%; park benches at 37%; a dog park at 32%, picnic shelters at 31%; a water playground/splash pad at 29%; woodland restoration efforts at 24%; playground equipment at 23% and tennis courts at 23%. The additional responses are summarized below.

#### FIGURE 5.16 RESPONDENTS TOP TEN RECOMMENDED NEW PARK AMENITIES OVER THE NEXT FIVE YEARS



#### **OTHER COMMENTS/RESPONSES:**

- Bicycle Lanes and paths adjacent to main roadways.
- Campground with resident membership fees.
- Climbing wall.
- Concessions facilities at the baseball parks.
- Cross-country ski trails.
- Dog park and swim area.
- Fencing along the east walking path adjacent to Lake Andrea.
- Gun and trap shooting range.
- Horseback riding trails.
- Indoor soccer fields with turf.
- Lacrosse field.
- Nature trails.
- Outdoor pool.
- Park beautification features such as water features, sculptures, gardens.
- Performance pavilion.
- Ping pong tables.
- Plant additional shade trees along paths.
- Public golf course driving range and practice area.
- Restrooms in the parks near playgrounds and ball fields.

While the findings suggests that certain park facilities would have stronger support if they were built, the Village should examine and analyze a wide range of possibilities to accommodate many activities and interests in the park's system. To test this statement, the passive recreation amenities were sorted by two (2) age categories--the respondents' age being 59 and younger and those over 60 years of age. Figure 5.17 below indicates that while most park amenities had similar desires by both of the age groups, the bike paths, lighted walking paths, dog parks, and drinking fountains received slightly higher responses by the 59 and under age group. Certain answers that were nearly identical between the age groups were the more passive and cultural park amenities including picnic shelters, park benches, vegetable garden plots, more flower beds/landscaping and woodland restoration, indicating that all age groups desire a balance of both active and passive amenities and community beautification in the parks.

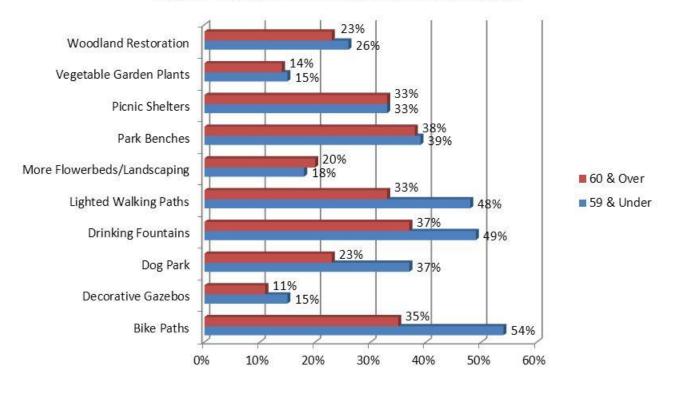


FIGURE 5.17 PASSIVE PARK AMENITIES USAGE BY AGE GROUP

**Beach Usage:** Question 11 asked, "About how many times would the survey respondents estimate they or members of their family used the Lake Andrea beach and the Lake Michigan Park/beach?" These parks/facilities were selected because they are popular water features and associated beach areas in the community. While the Lake Andrea beach tracts attendance, it is difficult to track the park and beach attendance at Lake Michigan Park/beach, therefore, asking these beach usage questions provides an opportunity to project a calculated attendance by multiplying the percentages of respondent households multiplied by the number of households in the Village.

The pie chart in Figure 5.18 below shows the general public use of Lake Andrea to be 46%, of which 30% are frequent users (5 or more times per year) and 16% are infrequent users (1-4 times per year). This high percentage of Lake Andrea beach users could be attributed to the fact that a RecPlex Membership provides unlimited access to the Lake Andrea beach as a membership benefit without incurring additional entrance fees.

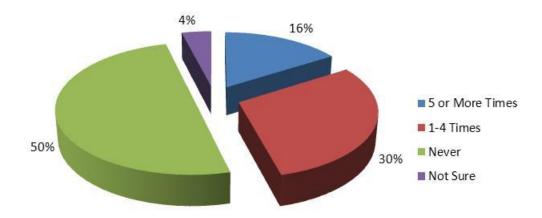
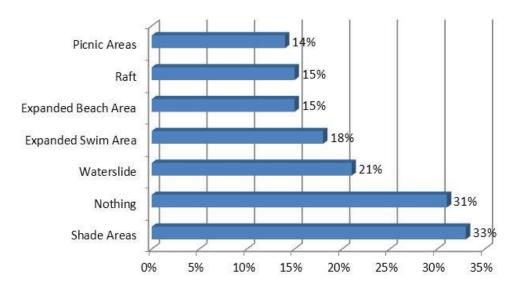


FIGURE 5.18 RESPONDENTS USAGE OF LAKE ANDREA BEACH

Question 14 asked, "What amenities would survey respondents like to see added to the Lake Andrea beach that would cause them to use the beach more frequently?" Respondents responded to the following amenities list shown in Figure 5.19 below. The bar graph shows that a total of 33% of the respondents indicated shade areas, 31% indicated "nothing"; 21% indicated a waterslide; 18% indicated an expanded swim area; 15% indicated an expanded beach area; 15% indicated a raft-type pier; and 14% indicated picnic areas. The most significant of these responses appears to be that respondents want more shade areas

#### Figure 5.19 RESPONDENTS RECOMMENDATIONS FOR IMPROVEMENTS TO LAKE ANDREA BEACH



at the beach. Currently there are really no opportunities to be at the beach in a shaded area unless beach umbrellas are brought. Also, about one-third of the respondents indicated that <u>no new improvements</u> would affect whether they increased beach usage.

Question 11 of the community wide survey asked, "How many times did the respondent or Members of their household use Lake Michigan Beach over the past 12 months?" The respondents were allowed to choose that they used it 1-4 times, they used it 5 or more times or they never used the beach. The following graph shown in Figure 5.20 compares the usage of Lake Michigan Beach to Lake Andrea Beach. The graph shows nearly comparable usage for the two (2) beaches among the respondents, suggesting that even though Lake Michigan beach has many fewer park amenities, it is a popular destination during the warm summer months. And the fact that there is no admission charge, it may attract many more general public attendees than Lake Andrea Beach where there is an admission charge for non-RecPlex members. If the Village were to make Lake Michigan Park/Beach improvements, they would likely benefit are larger segment of the population, especially non-RecPlex Members.

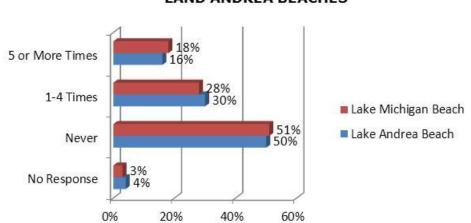


FIGURE 5.20 COMPARATIVE USEAGE OF LAKE MICHIGAN AND LAND ANDREA BEACHES

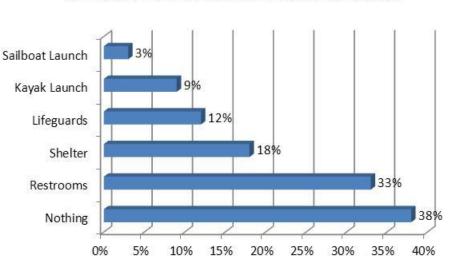


Lake Andrea Beach Prairie Springs Park



Lake Michigan Beach

Question 15 specifically asked about, "What type of Lake Michigan beach improvements would the respondents recommend to encourage them to use the Beach more frequently?" Figure 5.21 below shows that 38% of the respondents indicated that nothing or no improvements were recommended. Interestingly, lifeguards were only suggested by 12% of the respondents. Restrooms were the most popular response at 33% and a shelter was suggested by 18% of the respondents. If funding allowed for some park/beach amenities to be constructed, it is recommended that restrooms and a shelter be considered.



#### FIGURE 5.21 RESPONDENTS RECOMMENDATIONS FOR IMPROVEMENTS TO LAKE MICHIGAN BEACH





**Special Events in the Parks:** Question 16 of the community-wide survey asked the respondents, "What activities that they would like to see in the parks in the future?" The results as shown in Figure 5.22 indicate that 66% of the respondents would like to see farmers markets, 58% indicated music in the park; 42% indicated crafts/festivals; 31% indicated theater in the park; 29% indicated ethnic festivals and 12% indicated nothing.

These responses are very significant in that one-third to two-thirds of the respondents felt that the Village is missing out on different types of creative and more inclusive park and recreational opportunities to expand the usage of the parks beyond the active, organized sports activities coordinated by the Recreation Department.

Also, even though the Village has operated a community-wide festival for 16 years, it appears that either the annual festival needs to be reinvented or redefined, the community is seeking something new and different which is focused on a different target audience or the market is saturated with the same or similar type of summer events during the time period.

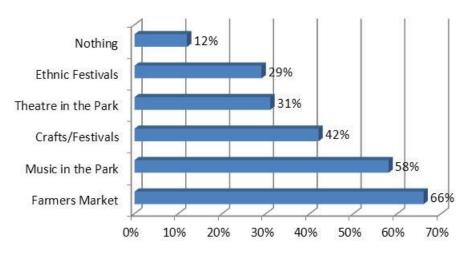
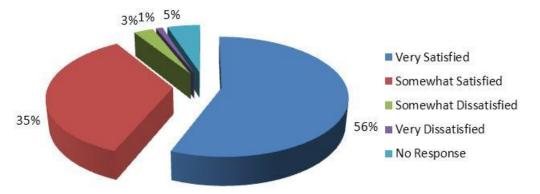


FIGURE 5.22 RESPONDENTS RECOMMENDATIONS FOR NEW ACTIVITIES IN THE PARKS

**Areas of Improvement:** Question 21 asked the respondents to, "Discuss any unpleasant experiences that they had at the parks or recreational facilities in the past year." These responses are compiled into general categories and listed in Appendix B of this Plan. Generally, the responses indicate that many of the concerns were about the RecPlex recreational facility and pertained to parking and children's behaviors at the facility. Some of the more minor issues related to facility maintenance and clean-up.

**Maintenance of the Parks and Recreational Facilities:** In Question 13, the respondents were asked, "How satisfied were the respondents with the condition and maintenance of the parks and recreational facilities?" Respondents were provided with the following possible answers: very satisfied, somewhat satisfied, somewhat dissatisfied, or very dissatisfied. The pie chart in Figure 5.23 below shows that 91% of those surveyed were either very satisfied or somewhat satisfied with park and recreational facilities maintenance. Only 4% were somewhat or very dissatisfied with the maintenance, indicating that the Village's Park Department is doing a very good work in keeping the Village's parks and recreational facilities maintained.

## FIGURE 5.23 RESPONDENTS SATISFACTION WITH PARKS AND RECREATIONAL FACILITIES MAINTENANCE



**Priorities for the Future:** Question 17 in the community-wide survey asked the respondents to, "Rate the importance of maintaining the existing recreation programs, improving the recreation programs, acquiring new park land, improving existing parks and facilities, and building new facilities on a scale of 1 to 5, where 5 was the most important. The following bar graph in Figure 5.24 compares the priorities. The graph shows the highest priorities are maintaining the existing recreation programs, improving the existing recreation programs and improving the existing parks and recreation facilities.

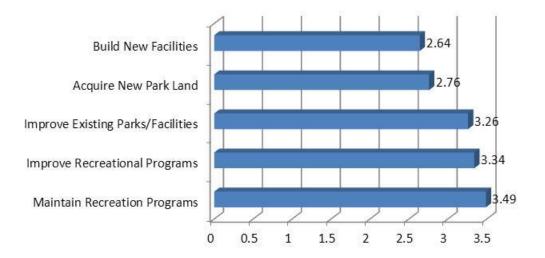


Figure 5.24 RESPONDENTS PRIORITIES FOR THE FUTURE

While maintaining the existing park infrastructure was a priority, overall the respondents indicated that it was also important to plan for the future and examine the acquisition of new park land and build new facilities. It is likely that the respondents were somewhat reluctant or concerned with the costs associated with new parks and amenities during the current economic recession and the worries about frozen budget levy limits and placing any additional tax burdens on the residents for the parks and amenities. That being said, there are various opportunities open to the Village through donations, dedications, grants and other special programs that could assist the Village in acquiring and developing new park lands without placing an undue burden on the Village tax payers.

Also, there was support by the respondents in the community-wide survey to adding additional bicycle and walking/running paths and beach amenities at Lake Michigan and Lake Andrea beaches. The survey further supported some very specific recommendations including adding non-capital intensive passive amenities to the parks such as farmers markets, and theater and music in the park special events.

**Final Comments:** Additional open ended comments were provided by many of the respondents. Generally, the comments provided a great deal of good feedback regarding the Village's parks and recreational programs. Overall, the survey indicated that the Village residents are pleased with the Village parks and the direction that the Village is going in providing parks services. Many respondents identified specific concerns along with amenities, and services that they would like to see addressed in the Village's parks in the future. These responses are summarized in Appendix B.

#### **VISIONING SESSIONS**

**Parks Department:** Public Research Group held a brainstorming visioning session with the Parks Department staff on July 12, 2011 to gather input on Village park maintenance and improvements matters. The four (4) most significant issues recommended by the Park's Department staff included:

- Develop a five-year capital improvements and maintenance plan which includes projects associated with specific maintenance projects for each park and improvements for new park developments.
- Prepare detailed park maintenance guidelines and standards for the Village's park and outdoor recreational facilities.
- Re-evaluate the Park's Department staffing levels based upon the existing and proposed acreages and facilities of the Village's park and outdoor recreational facilities.
- Identify and examine opportunities for education and training for the park and recreational facilities equipment and machinery.

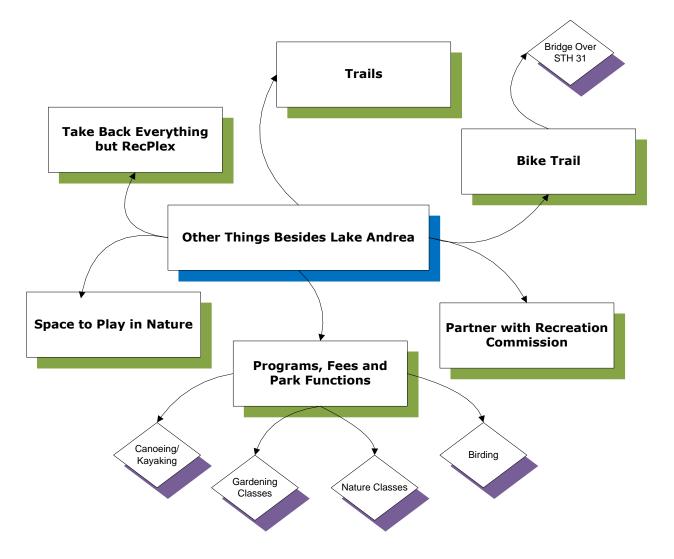
**Park Commission and Recreation Commission:** Public Research Group held the first Park Commission visioning session in June, 2011, wherein the potential community-wide survey questions were evaluated and discussed.

A second meeting was held in July, 2011 and focused on a visioning workshop session. The primary discussion points that were covered included:

- What methods should the Village utilize to attract residents to the Village parks?
- As the Village grows and becomes more diverse, actions need to be taken to understand the future demands for park and recreational services.
- Current economic conditions will require communities to be more creative in acquiring and maintaining their park and recreational systems.
- The Village needs to develop standards and guidelines for annual maintenance to the existing park and recreational facilities system.
- Park planning, especially community and neighborhood parks are essential to serve the needs of the growing Village community.
- Each park should establish its own unique identity and draw from the community.
- The Village parks and community centers need to be interconnected with a network of bicycle and walking/running paths.

At the meeting, the Research Group conducted a mind mapping exercise and had the group conceptualize ideas within the Village's park system outside of the Prairie Springs Park/Lake Andrea regional park amenities. Figure 5.25 below sets for the activities discussed.





A final exercise conducted by the Research Group with the Park Commission focused on concerns expressed by the group. Generally, the Park Commission reiterated the same concerns as set forth at the last visioning event. The primary new additional issue that when funding allows, that the Park Commission should encourage and promote activities that enhance the park maintenance and development and exploring opportunities to adding new activities and special events within the Village's Park's system. In addition, the Park Members recommended that as a policy recommending body to the Village Board, that a Mission Statement be prepared to identify and share a common vision for the Village's Parks with the community. The Park Commission <u>Mission Statement</u> is provided below:

"In partnership with our citizens, the function and duty of the Village of Pleasant Prairie Park Commission is to provide for an enhanced quality of life for all residents and members of the community by facilitating the development, preservation, acauisition, protection, maintenance, management and promotion of park lands, open space areas, recreational facilities and natural resources in an orderly and uniform process. The Commission, as stewards, shall provide long-term and short-term planning and supervision of park-related development projects and shall support the implementation of those plans. The Commission shall promote the wellness benefits of park lands in the best long-term interests and benefits of the Village and its residents of all ages, abilities and for those people with handicaps or disadvantages. The Commission will strive to enhance parks and open spaces to enrich the quality of life for present and future generations through the provision of safe and secure environments."

A third and final meeting was held in September, 2011 and was a joint meeting with both the Park Commission and the Recreation Commission. The purpose of this meeting was to discuss the community-wide survey findings and proposed recommendations to be included in this Plan and presented in this Chapter.

## PUBLIC INFORMATIONAL MEETINGS

Public Information Meetings were held by the Village on July 20 and August 17, 2011 wherein members of the Park and Recreation Commissions were in attendance, along with the Village staff and members of the public. These meetings provided for the opportunities to have open discussions on the issues surrounding the future of parks and open spaces in the Village. An overview of the considerations and recommendations in preparing the Park and Open Space Plan Update is listed below. Interesting to note, many of these same concepts have been introduced through the previous park planning processes associated with this update:

- Evaluate the existing parks maintenance and develop guidelines for regular, ongoing maintenance activities.
- Interconnect the existing and future parks and community gathering spaces via a network of bicycle and pedestrian paths.
- Review the survey results to determine whether there are additional underserved park-related needs in the community.
- Work closely with new developments to coordinate the dedication and donation of future park and open space lands with the community.
- Re-examine the parks facilities for developing new special event programs and activities.

- Investigate various grant opportunities with the State and federal government and to obtain funding to further develop the Village parks and bicycle and pedestrian transportation network.
- Continue marketing efforts to identify and promote the Village's park and open spaces and recreational facilities and special events to the residents.
- Examine opportunities to create unique active (dog parks) and passive (community gardens) recreational opportunities in the Village parks. Not all parks need to have the same amenities.
- Create new opportunities for passive recreational opportunities to improve the existing features in existing Village parks such as community gardens, floral gardens, and woodland and wetland restoration projects.

The results of all of the public participation efforts completed and discussed in this Chapter contributed to the development of the goals, objectives and standards described in Chapter 2, the recommendations for the analysis of the existing park and recreational facilities in Chapter 6 and corresponding new Conceptual Plans developed in Chapter 7 of this Plan.

# CHAPTER 6

# ANALYSIS OF EXISTING PARK AND RECREATIONAL FACILITIES

This chapter presents an analysis of how well the Village's park, open space and recreational facilities satisfy the current needs of community based upon the goals, standards and objectives discussed in Chapter 3 and taking into account the public participation activities and community-wide survey results discussed in Chapter 5 of this Plan. The adequacy of the Villages existing park and open space and recreation system will be specifically evaluated by:

- Examining quantitative park, open space and facility service standards developed in Chapter 3 to the reliable population projections for the Village for the plan design year 2017 and to the comprehensive plan design year 2035;
- Analyzing the qualitative analysis of the Village's park system based upon the community-wide survey results, Park and Recreation Commission visioning events, Village Board and staff input and public informational meetings;
- Reviewing the geographic distribution and accessibility of the park and open space areas in the community; and
- Comparing it with the Wisconsin State Comprehensive Outdoor Recreation Plan.

The results of this analysis will serve as the basis for the recommendations presented in the next chapters of this Plan.

## QUANTITATIVE ANALYSIS

The following charts present a comparison of Southeastern Wisconsin Regional Planning Commission's (SEWRPC's) park and recreation standards to the Village's existing facilities. In Figures 6.1 and 6.2, the standards per 1,000 persons were applied to the Village's current 2010 population estimate of 19,719. In Figures 6.3 and 6.4, that same standard was applied to the Village's 2035 population projection estimate of 31,205 under the intermediate population growth scenario. In the Figures, the regional population estimate used was 166,671 persons, which was based upon the 2011 Kenosha County population and the 2035 population projection estimate was 210,078.

In Figure 6.1, the Existing Acreage for Regional Parks was derived by applying the standard per 1,000 persons to an estimated regional population; however, the total existing acreage for regional parks only accounts for Prairie Springs Park and does not include any acreage located outside Village boundaries. Therefore, it is important to note that although the following figure indicates a deficiency in regional park acreage, there are several regional parks located in the communities that surround Pleasant Prairie in Kenosha County. When considered together, these parklands meet SEWRPC's regional park standard. Furthermore, although there is no quantitative standard, Figure 6.1 lists that there is 6,127 acres of Special Open Space Acreage in the Village.

The data presented in Figures 6.1 and 6.2 indicate that the Village needs to develop approximately 42 acres of community park land in order to satisfy SEWRPC's park standards. These figures identify whether the facility is located indoors or outdoors. A policy decision has been made to include a percentage of indoor courts and facilities in meeting the standards.

#### FIGURE 6.1 EXISTING PARK ACREAGE NEEDS IN THE VILLAGE FOR 2010

Park Type	Standard (acres per 1,000 persons)	Required Acreage	Total Existing Acreage	Park Acres Needed
Neighborhood Parks	1.7	34	14.3	19.7
School Parks	1.6	32	50.0	0.0
Community Parks*	2.2	43	59.7	0.0
Regional Parks	5.3	883	930.0**	0.0
Special Open Space***	N/A	N/A	6,127.0	N/A***

\* Includes Ingram Park and Village Green Park recently acquired by the Village.

(Note: While Anderson Park as a community park benefits the Village residents through its many park amenities and programs, it is located in the City of Kenosha, so it is not included in the total acreage calculations).

\*\* This number only reflects the number of regional park acres located within the Village. Pleasant Prairie does not plan to develop any additional regional park acreage since there are other regional parks found within Kenosha County.

\*\*\*The Village will continue to acquire environmentally, scientifically, and archaeologically significant special open space lands as donations and grant funding becomes available.

Park Type	Standard (# facilities per 1,000 persons)	# Facilities Required (based on Std. per 1,000 persons)	Total # of Existing Outdoor Facilities****	# of Existing Indoor Facilities	# Facilities Needed*
Baseball Diamond	0.10	2	6	0	0
Basketball Goal	1.13	22	8	40	0
Ice Skating Rink	0.15	3	0	2	1
Playfield*	0.50	10	13	0	0
Playground	0.42	8	14	0	0
Softball Field	0.60	12	3	0	9
Tennis Court	0.60	12	0	0	12
Soccer Field	0.86	17	19**	4	0
Swimming Pool	0.15	3	0**	2***	1

### FIGURE 6.2 EXISTING PARK FACILITY NEEDS IN THE VILLAGE FOR 2010

\* Playfields are considered open space areas in the parks that can be used for unorganized activities. Playfields are located within every Village Park and School Park.

\*\* There are 20 outdoor soccer fields and one (1) outdoor swimming pool located at Anderson Park in the City of Kenosha. These facilities help the Village to meet its recreational standards but the facilities are not controlled by or located within the community.

\*\*\* Includes the RecPlex aqua arena and waterpark/current channel and swim lanes.

\*\*\*\* Includes amenities at the school park facilities.

Figures 6.3 and 6.4 suggest that in the future the Village recently acquired two (2) community parks areas—Ingram Park and Village Green Park, however, construction of amenities has just recently started in these parks. As such, the Village needs to continue to identify and plan for additional neighborhood parks and school parks, and the development of the community parks to meet the needs of its growing population.

Park Type	Standard (acres per 1,000 persons)	Required Acreage	Total Existing Acreage	Park Acres Needed
Neighborhood Parks	1.7	53.1	14.3	38.8
School Parks	1.6	49.9	50.0	0.0
Community Parks*	2.2	68.7	59.7*	9.0
Regional Parks	5.3	1113.4	930.0**	N/A**
Special Open Space***	N/A	N/A	6,127.0	N/A***

#### FIGURE 6.3 PARK ACREAGE NEEDS IN THE VILLAGE FOR 2035

\* Includes Ingram Park and Village Green Park recently acquired by the Village.

(Note: While Anderson Park as a community park benefits the Village residents through its many park amenities and programs, it is located in the City of Kenosha, so it is not included in the total acreage calculations).

\*\* This number only reflects the number of regional park acres located within the Village. Pleasant Prairie does not plan to develop any additional regional park acreage since there are other regional parks found within Kenosha County.

\*\*\*The Village will continue to acquire environmentally, scientifically, and archaeologically significant special open space lands as donations and grant funding becomes available.

	Standard (# facilities per	# Facilities Required (based on Std.	Total # of Existing	# of Existing	<i>"_</i>
Park Type	1,000 persons)	per 1,000 persons)	Outdoor Facilities*	Indoor Facilities	# Facilities Needed*
Baseball Diamond	0.10	3	6	0	0
Basketball Goal	1.13	30	8	40	0
Ice Skating Rink	0.15	4	0	2	2
Playfield*	0.50	13	13	0	0
Playground	0.42	11	14	0	0
Softball Field	0.60	16	3	0	13
Tennis Court	0.60	16	0	0	16
Soccer Field	0.86	23	19**	4	0
Swimming Pools	0.15	4	0**	2***	2

## FIGURE 6.4 PARK FACILITY NEEDS IN THE VILLAGE FOR 2035

\* Playfields are considered open space areas in the parks that can be used for a wide variety of active sporting activities. Playfields are located within every Village park.

\*\* There are 20 outdoor soccer fields and one (1) outdoor swimming pool located at Anderson Park in the City of Kenosha. These facilities help the Village to meet its recreational standards but the facilities are not controlled by or located within the community. Also, there are five (5) soccer fields which are used as practice fields at the local school parks.

\*\*\* Includes the RecPlex aqua arena and waterpark and channel lanes.

## **QUALITATIVE ANALYSIS**

Although the quantitative standards provided by SEWRPC provide a good basis for formulating the recommendations in this Plan, a thorough assessment of the Village's existing facilities must include a more subjective analysis that takes into consideration those characteristics that make Pleasant Prairie unique from other communities. Furthermore, the following qualitative analysis will identify park system deficiencies that are not captured by universal standards, and will ensure that future facilities meet the specific needs of the Village.

Important qualitative factors gathered from the community-wide survey results; Park and Recreation Commission visioning events; and public informational meeting input revealed the following information when analyzing park needs:

- There are three large corporate/industrial parks in the Village—LakeView Corporate Park East/West, PrairieWood Corporate Park and the future Abbott Campus. Currently over 80 businesses generate over 8,000 employees, many of which are looking for recreational opportunities for their workers.
- Pleasant Prairie occupies a large land area relative to its population, which increases the number of trails and bike paths miles needed to interconnect areas of existing and future development.
- The Village is uniquely located by Lake Michigan and Lake Andrea and should take advantage of constructing and promoting additional park and recreational amenities at the two (2) beaches.
- Segregated land uses (residential, commercial and industrial) need to be interconnected to each other and to the parks through a network of walking and running trails; bike lanes and paths; and recreation trails. These new trails should offer amenities such as trail markers, benches, garbage facilities and water fountains along the trails.
- Open space preservation, woodland restoration projects and educational opportunities offer the Village chances to partner with area conservancy groups and the local universities through land donations made to the Village—Momper's Woods and Sorensen Woods at Kildeer Farm.
- In addition to its formal park system, the Village has a significant amount of Special Open Space Areas that should be recognized as an important component of the overall park system. Although the existence of these lands does not reduce the need for parks, a consideration of these lands should not be left out of a comprehensive discussion and analysis of the Village's park, recreation, and open space system. Special Open Space Areas provide many benefits to a community and enhance ecological integrity and human health by maintaining natural processes. Public Special Open Space Areas should be considered specifically when planning trail networks, as these corridors can provide ideal locations for such facilities. In addition, these lands easily accommodate trailheads and can help establish a framework for a Village-wide trail system.
- Through careful planning, land use and zoning controls, Pleasant Prairie preserves concentrations of high-quality habitat and natural resources, particularly in the environmental corridors and isolated natural areas of the Chiwaukee Prairie-Carol Beach area and the Des Plaines River Watershed.

- The Village's park system needs to accommodate the influxes of athletes and spectators for triathlons, swimming events, volleyball and basketball tournaments, hockey and figure skating events, along with other Villagesponsored special events having region-wide impacts.
- The RecPlex and Prairie Springs Park provides for the majority of recreational opportunities in the Village. Although these facilities make a significant contribution to the Village's overall recreation system, it should be considered that they are primarily indoor facilities and are not open to the general public free of charge. In addition, the RecPlex concentrates all of these facilities in one location. Although most of the Village's recreational needs are currently being met as a result of the facilities provided at the RecPlex, efforts should be made to expand and enhance the number of outdoor recreational opportunities, particularly in the central (east of STH 31) and eastern portions of the Village.
- Two (2) recent community park land donations were made to the Village for Ingram and Village Green Parks. As donations, grant funding and budgetary conditions allow, park and recreational amenities should be added to these parks.
- With the parks being more defined and park land opportunities increasing for the Village, consideration should be given for expanding on the outdoor recreational programing which focuses on activities such as: farmers markets, community gardens, music and art in the park, and other programs.
- The Village completed an impact fee assessment in 2005, which identified the general locations of new parks, recreational facilities, and recreation trails. Because Pleasant Prairie's current impact fees are based on the assumption that these parks will be developed within the next 10-15 years and two new parks have been added, this Plan recommends that the impact fee assessment and recommendations be re-examined.
- Marketing efforts should be undertaken to make the residents and business community aware of the park and recreational services being offered in the Village.

# GEOGRAPHIC ANALYSIS

The location and distribution of parks and recreational facilities also provides a good indicator of how well the existing park system is meeting the needs of Village residents. To illustrate this distribution, Map 6.1 (found at the end of this Chapter) depicts the service areas of the Village's existing parks. These service areas are based on the SEWRPC's standards identified in Chapter 3 of this Plan.

An analysis of Map 6.1 suggests that the <u>central and southeastern portions</u> of the Village are not as well served with park improvements in terms of neighborhood parks or community parks. As mentioned previously that although Anderson Park serves the northeastern portion of the Village, the park is not located in the Village and is not entirely open for public use as special permits are needed to use the soccer, softball and football field facilities. Therefore, it important that these areas of the Village are supplemented with a sufficient number of neighborhood parks to meet residents' recreation and park needs. The southwestern and western portions of the Village are served by Prairie Springs Park and two (2) other neighborhood parks, which offer many of the amenities needed by the community.

## PUBLIC INPUT

The results of the visioning workshop and public informational meetings are discussed in Chapter 3 of this Plan. These discussions indicated that the Village's existing recreational facilities could use improvements or updates in the following areas:

- Walking/running and biking paths/lanes and recreation trails to interconnect the residential, recreational and commercial areas;
- Active outdoor recreational facilities in the Ingram and Village Green community parks;
- Passive outdoor recreational/educational facilities in the Momper's Woods and Sorensen Woods at Kildeer Farm special use parks;
- Public access and amenities for the Lake Michigan Park and Lake Andrea Beach, particularly with respect to beach/swimming areas, shade, concessions and restrooms;
- Handicapped accessibility for the playground and restroom facilities in the neighborhood and community parks.
- Focus should be placed on improving amenities in Village Parks, other than Prairie Springs Park.

# REVIEW OF THE WISCONSIN STATEWIDE COMPREHENSIVE OUTDOOR RECREATION PLAN (SCORP)

The last component of the needs assessment involves a review of the 2011-2016 Wisconsin Statewide Comprehensive Outdoor Recreation Plan (SCORP) which was prepared by the Wisconsin Department of Natural Resources (WI DNR). Every five (5) years, Wisconsin publishes the SCORP as required by the Federal Land and Water Conservation Fund (LWCF) Act of 1965. The document is used to help allocate federal funds equitably among local communities. The plan also presents information on the various levels of outdoor recreation and policies. At the national level, the SCORP recognizes the America's Great Outdoors (AGO) initiative based on the idea that the protection of the country's natural heritage is a non-partisan objective shared by all Americans. The AGO encourages state and local communities to develop local grassroots conservation and recreation initiatives.

This Plan shows how Pleasant Prairie is following the State's vision in preserving and improving recreational opportunities in the community and how that fits within the State's and national initiative of conservation and recreation. The SCORP describes and quantifies some of the most important benefits of recreation and established goals to improve outdoor recreation across Wisconsin. This Plan achieves those same objectives. The SCORP further targets assessments on several key relationships that include: 1) public health and wellness, 2) access to outdoor recreation and 3) public and private partnerships—all of which this Plan and previous Village planning efforts have focused on for years.

Public participation was an extensive component of the State's planning process. An External Review Panel comprised of nine members participated in several phases of the state's planning process. Members representing a broad range of recreational providers and experts contributed their expertise to initiatives such as identifying and prioritizing significant statewide outdoor recreation issues and determining possible solutions. In addition, seven (7) focus groups were held across the State that represented a broad spectrum of recreation an conservations interests.

Statewide recreation demand surveys conducted between the years 2005 and 2009 were designed to collect information regarding the types of recreational activities that people engaged in most frequently, as well as residents' level of satisfaction with the State's existing facilities. Refer to Appendix B of the 2011-2016 SCORP for a detailed discussion of the outdoor recreation demand survey methodology. The SCORP Appendix describes the methods and results of the 1994-2009 National Survey on Recreation and Environment (NSRE) which has been conducted on a continuing basis since 1994. The NSRE conducted the an-home survey to over 90,000 households across all ethnic groups throughout the United States, 3000 of which were from Wisconsin.

In Appendix C of the SCORP, the WI DNR categorized and evaluated recreation activity intensities, caloric expenditures by body weight, by appropriate location and by participation. The top or tier one activities had between a 50 and 90 percent participation rate. These activities primarily included walking for pleasure, visit nature centers, general gardening, view/photograph natural scenery, family gathering, view/photograph other wildlife, driving for pleasure, view/photograph wildflowers, and sightseeing. The middle or tier two activities had participation rates that are between 30 and 50 percent. These include general bicycling, picnicking, visit historic sites, yard games, gather mushrooms, berries, etc., golf, boating, swimming in lakes/streams, view/photograph birds, general hiking pool swimming, visit a wilderness, visit a farm, warm water fishing, attend outdoor concerts/plays, soccer, running/jogging, and mountain biking. The bottom or tier three activities have participation rates up to 30 percent. The primary tier 3 activities included sledding, view/photograph fish, camping, racketball/handball, visiting a waterslide, football, off-highway vehicle driving, snowmobiling and visiting a waterslide.

The top 10 growing recreation activities in the State of Wisconsin between 1994 and 2009 were:

- 1. Outdoor soccer
- 2. View/photograph other wildlife
- 3. Golf
- 4. Handball or racketball outdoors
- 5. Walk for pleasure
- 6. Attend outdoor sports events
- 7. Bicycling
- 8. Day hiking
- 9. Running/jogging
- 10. View/photograph birds

The SCORP noted that "walking" is by far the most popular outdoor activity in the State. The SCORP indicated that 40-50% of Kenosha County residents are within ½ mile walk of a public park. And, Kenosha County ranks fourth in the top ten counties in Wisconsin for pedestrian park and trail access opportunities.



2012 Softball Game during Prairie Family Days



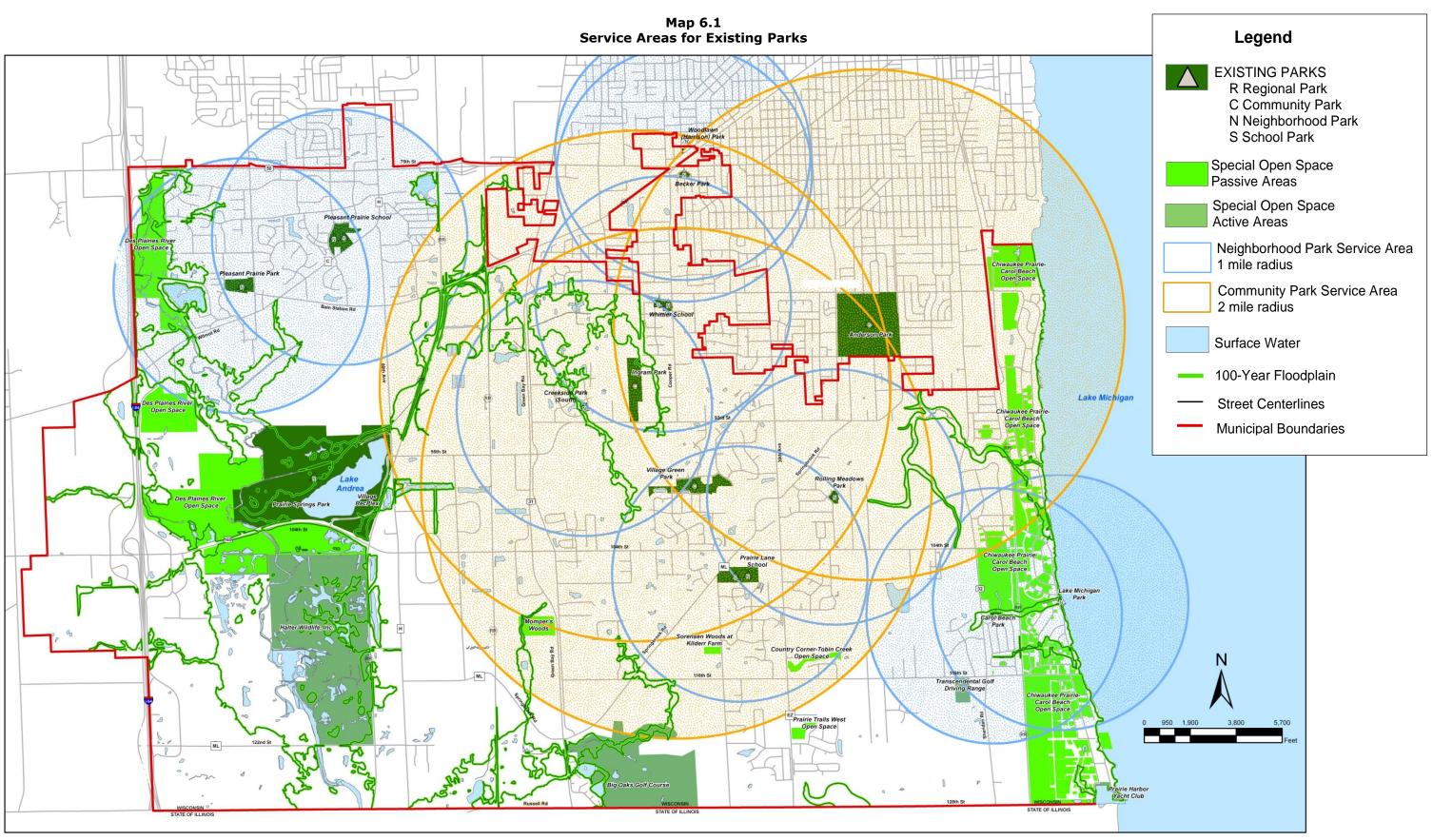
Soccer game at Pleasant Prairie Park

The SCORP also noted that gardening is a popular recreational activity which provides many benefits, including improved access to fresh produce, increased physical activity and community building. A recent State recreation survey indicated that almost 70% of Wisconsin adult's garden or landscape for pleasure. Gardening activities can be divided into two categories: backyard and community. The extent of backyard gardens is between 25-49 acres in Kenosha County.



Community Garden at Prairie Springs Park

Recommendations and implementation strategies presented in the Village's Plan will generally reflect residents' needs for facilities that accommodate tier one and tier two activities as reflected in the Wisconsin SCORP.



# CHAPTER 7

# POLICY RECOMMENDATIONS AND PROGRAMS IMPLEMENTATION

## INTRODUCTION

The following policy recommendations and programs were developed through the research, discussion and evaluation of the park and open space facilities by the Village Board, Village Park Commission, and Village staff as a part of the comprehensive and detailed park planning efforts. In addition, public input was gathered by the Public Research Consultant firm through the public participation park meetings and survey work process to help assist in determining perceived park needs and priorities in the Village.

The recommendations are based upon the recent demographic changes, growth projection rates and park system inventoried deficiencies within the Village. Using the 2035 population projections, these recommendations would satisfy the park and recreation standards for the community. The timing of park and open space land acquisitions and park facilities development should coincide with not only the actual demand for the recreational park and open space facilities, but also the current financial and budgetary limitations of the Village. As such, in order to accomplish the goals and objectives of the Park Plan, this chapter sets forth the policy recommendations and programs that will be used to implement the Plan over the next five years.

## **BASIC DEFINITIONS**

The following basic definitions are being used to explain the park planning implementation process:

**Recommendations**: Recommendations are specific policies that establish the courses of action used to ensure plan implementation. Policies may often accomplish a number of objectives.

**Programs:** Programs are specific budgetary action items or programs to be implemented to carry out the Plan.

# POLICY RECOMMENDATIONS AND PROGRAMS

1. **Objective**: Identify, acquire and provide land for quality public outdoor recreation sites and adequate open space in the Village.

#### Policy Recommendations:

- a. Continue Neighborhood Planning to identify future land uses, road and lot layouts, and appropriate locations for public parks, open space corridors, and trails systems. All new residential development should be within one (1) mile of a neighborhood, community or regional park.
- b. Continue to plan for a diversity of park sizes and park types based on the demographic characteristics and needs of the Village.
- c. Design future parks so that they are barrier-free and accessible to persons with disabilities.

- d. Locate new parks and open space areas that complement the existing and future surrounding land uses and natural resource features.
- e. Design parks to enhance community cohesion and to provide opportunities for community gathering places. All parks should have multiple access points from various transportation modes from the surrounding neighborhoods.
- f. Continue to acquire park and open space lands in advance of or in coordination with new development in order to provide for reasonable acquisition costs and to facilitate site planning. Park and open space areas in undeveloped areas should be acquired through land developer dedications, where feasible.
- g. Parks should be integrated into future neighborhood designs and linked by a network of sidewalks, pedestrian/walking/bicycle trails, and open space corridors.
- h. Support the WI Department of Natural Resources (WI DNR) and The Nature Conservancy's purchase of the remaining conservancy lots in Chiwaukee Prairie acquired on a willing seller-willing buyer basis as referenced in the Land Use Management Plan for the Chiwaukee Prairie-Carol Beach area of Pleasant Prairie.
- i. Accept land donations for the future expansion of Village Green Neighborhood Park.

**Programs:** (proposed to be implemented within next five (5) years as funding allows)

- a. Prepare an update to the Pleasant Farms Neighborhood Plan which presents revised lot and street layouts, park areas and the revised location of Prairie Farms Trail.
- b. As the opportunities arise and funding becomes available, acquire additional lots adjacent and abutting to Pleasant Prairie Park per the Comprehensive Plan.
- c. Accept land donations and acquire land as funding allows for specific properties within the Des Plaines River Watershed for the expansion for park/recreational opportunities and open space conservancy purposes.
- 2. **Objective**: Prepare marketing information and educate the community about park and recreational opportunities and facilities available in the Village.

#### **Policy Recommendations:**

- a. Continue to promote a Village-wide park system that addresses the needs and desires of the existing and future Village residents and businesses.
- b. Create public awareness of the Village's parks facilities and open space preservation areas by promoting them through maps, signage, and other paper and electronic media materials.
- c. Educate the public on the locations and benefits of unique park elements within Village parks and open space areas.
- d. Continue to provide and create new opportunities for park and open space areas for special events in the Village.
- e. Develop feedback methods to assess the existing and future park service needs of Village residents, schools and businesses.
- f. Identify handicapped accessible park and recreational facilities that can accommodate individuals and groups with special needs.

- g. Continue to evaluate and identify opportunities to share park and recreational facilities with public and private school needs where appropriate and feasible, by joint planning, maintenance and facility sharing agreements.
- h. Continue to work with the Kenosha Area Convention and Visitors Bureau in the creation of and promotion of new recreational activities and venues to promote tourism in the Village.
- i. Identify locations where public access is and can be provided to lakes, rivers and other water resources in the Village.

**Programs:** (proposed to be implemented within next five (5) years as funding allows)

- a. Promote park and open space areas of the community by providing maps, details and locations of outdoor, planned recreational events in the parks such as: "Music by the Lake", "Music in the Park", farmers markets, Prairie Family Days, etc. through various electronic media methods including special interviews on Channel 25, Village e-newsletter and web-site and via social media networking.
- b. Create electronic hyperlinks from the Village's web-site to the various non-profit private and public interest group web-sites that provide educational and informational details on the Chiwaukee Prairie, Chiwaukee Prairie Trails, Kenosha Sand Dunes and other environmentally significant areas of the Village.
- c. Prepare hard copy brochures, park maps and other quality of life marketing materials and promotional items to display on information racks in Village municipal buildings, Chamber of Commerce and for Village welcome packets.
- d. Create marketing materials targeting various age groups which explain the health benefits of the utilizing the Village parks and trail system.
- e. Work with non-profit, school and special interest groups to promote an "adopt a beach" clean-up program similar to "adopt a highway" program.
- f. Speak at the local grade schools to promote Earth Day activities at the parks.
- g. Develop new ways to advertise opportunities to solicit funding for the "Memorial Tree Program" in honor or in memory of a loved one in the Village Parks.
- h. Continue the tradition of promoting and hosting the annual Veterans Day tribute at the Veterans Memorial Ballfield area at Prairie Springs Park.
- i. Continue to market, promote and host the annual Village-wide Prairie Family Days event as to showcase the park and recreational activities in Prairie Springs Park.
- j. Encourage and work in cooperation with Village residents and businesses in the creation of a "Friends of Pleasant Prairie Parks" group or other non-profit organization or club that can:
  - Assist in identifying, promoting, raising funds and developing park facilities for community garden or farmers market projects.
  - Work to develop policies for Friends' volunteers.
  - Encourage businesses and their employees to support a Friends group.

3. **Objective**: Preserve the Village's natural, cultural and archeological resources and amenities for the benefit of current and future community residents and businesses.

#### Policy Recommendations:

- Continue to evaluate and utilize alternative means of reserving lands required for open space to ensure that lands are obtained at the lowest cost to the public (e.g. non-profit organizations, conservation easements, purchase of developments rights).
- b. Consider applying for grants and other State and federal funding resources to preserve natural and scientific resource conservancy areas.
- c. Work with non-profit groups to encourage their Purchase of Development Rights (PDR) or Transfer of Development Rights (TDR) programs to protect environmental corridors, natural areas, and critical species habitat sites.
- d. Encourage the protection of high-quality open space lands through public and nonprofit conservation organization fee simple purchases and conservation easements.
- e. Continue to work with other public and private non-profit agencies to identify, preserve and protect archeological and historical resources.
- f. As new development advances in the Village, continue to encourage the preservation of primary environmental corridors, secondary environmental corridors, and isolated natural resource areas within parks and open spaces areas of the Village, where possible.
- g. Work with community businesses, non-profit groups and residents in the preparation and implementation of oak opening and savanna woodland restoration plans.

**Programs:** (proposed to be implemented within next five (5) years as funding allows)

- a. Prepare a conceptual development plan and feasibility study for the development a new Nature and Educational Center at Momper's Woods.
- b. Work with the Kenosha/Racine Land Trust, Inc., to acquire land and prepare conservation easements for the protection of environmental corridors, natural areas, and critical species habitat sites in the Des Plaines River Watershed.
- c. Work with area residents and volunteers to establish partnerships in the development and implementation of an Oak Opening Woodland Restoration Plan for the Sorensen Woods at Kildeer Park.
- d. Work with area residents and volunteers to establish partnerships in the development and implementation of an Oak Opening Woodland Restoration Plan and Master Plan for the development of the Momper's Wood's Nature Center.
- e. Partner with the Natural Resource Conservation Service (NRCS), WI DNR, U.S. Fish and Wildlife Service (USFWS), and non-profit organizations such as Ducks Unlimited to create new wetlands, enhancements, and restorations within the Des Plaines River Watershed.
- f. Consider working with public State and federal agencies and non-profit agencies to apply for Kenosha County, State and federal grants to acquire properties in the 100-year floodplain along the Jerome Creek east of STH 31.

- g. Identify passive, low impact open space viewing opportunities within Prairie Springs Park that provide for an overlook or scenic viewing of the flora and fauna.
- h. Evaluate the impacts of how new development projects may affect historic properties and archaeological sites that are eligible for listing on the National or State Registers of Historic Places.
- i. Preserve archaeological sites inventoried in the Village's Comprehensive Plan or identified through various federal, State or local surveys, studies, and reports prepared for the Village or areas within the Village.
- j. Encourage the development of educational facilities such as a museum, nature center or cultural center in or near the Village parks to promote the historical and archeological significance of the Village.
- k. Have the Park Commission work in a cooperative effort with the Pleasant Prairie Historical Society in developing and distributing educational materials about historical or archeological areas of significance in the Village.
- I. Install a historical marker for the Jambeau Trail that discusses the importance of the ethnic trail.
- 4. **Objective**: Provide off-street and on-street interconnected bike and walking trails and related public open space areas that will provide Village residents and community businesses with adequate opportunities to participate in a wide range of outdoor activities.

## Policy Recommendations:

- a. Identify specific bike trail capital improvement projects as set forth in with the Village of Pleasant Prairie, 2010 Bicycle and Pedestrian Trails Plan (Appendix E).
- b. Design an internal trail system within parks and open space areas so that they are linked to the larger Village-wide trail network.
- c. Develop and execute agreements with other trail building and trail maintaining partners, that identify the sections that each group would build and the financial responsibilities of each party.
- d. Identify public and private funding sources for the trail system within the Village.
- e. Continue to apply for State and federal grants and work to assemble matching resources to fund construction of the specific trails.
- f. Provide an interconnecting system of trail-oriented facilities, such as bikeways, hiking trails, in the Village and where possible, to adjacent communities.
- g. Continue the development, enhancement, and management of recreational trail facilities to ensure connectivity of such facilities to Regional Park and open space areas.
- h. Provide for safe convenient and paved pedestrian bike connections between the Kenosha County Bike Trail, Prairie Springs Park, Lake Michigan Park and other park and open space facilities.
- i. Discuss the interconnection of the Pleasant Prairie's on-street trail network with on-street trails in the City of Kenosha and Lake County, Illinois.

**Programs**: (proposed to be implemented within next five (5) years as funding allows)

- a. Implement the grant award for the development of Prairie Farms Trail and the adjacent parking lot and landscaping.
- b. Obtain permits and complete the construction of the extension of Prairie Farms Trail over the backwater discharge of the Des Plaines River.
- c. Design and evaluate funding sources for the construction of the Des Plaines River Trail extending from State Trunk Highway (STH) 50 to STH 165.
- d. Construct a portion of the Village Green Trail located within the Village Green Neighborhood Park located east of future Cooper Road.
- e. Work with Kenosha County to construct, pave and/or stripe the on-street bike lanes located within River Road, CTH C (Wilmot Road), Prairie Farms Trail (the former Sewer D drive road) to the connection to the Des Plaines River Trail and to the paved off-street paths at CTH C and I-94.
- f. Develop plans for a bicycle/pedestrian bridge crossing over I-94 to facilitate employer-employee connections.

# 5. **Objective**: Continue to maintain and provide upgrades to the Village's existing park sites and park facilities.

#### **Policy Recommendations:**

- a. Continue to implement measures to ensure that existing park facilities are upgraded to comply with ADA design guidelines.
- b. Evaluate the existing parks for the development of special recreational facilities such as dog parks, cross country ski trails, sledding hills, etc. through more detailed park planning and development efforts.
- c. Continue to collect and utilize park impact fees as set forth in the Village municipal code for the expansion and development of park capital improvements.
- d. Continue to utilize existing Public Works Department staff and/or volunteers to complete minor capital improvements in the parks to ensure a cost effective approach to construct and to complete projects.
- e. Continue to maintain and upgrade existing parks facilities for the safety and convenience of the age groups that use them.
- f. Continue to evaluate the costs associated with the development new parks with the need to upgrade and maintain existing park sites.
- g. Continue to utilize recycled material available to the Village for park construction activities.
- h. Incorporate green building technology into capital projects where feasible.
- i. Continue to apply for WI DNR Stewardship funds, Wisconsin Conservation Management Program (WCMP) grants, and other State and federal funding.
- j. Continue to review implementation mechanisms such as impact fees and subdivision dedication requirements for park and open spaces areas during the platting and development review process.

- k. Review the park development capital improvement projects each year as part of the Public Works budgetary process.
- I. Evaluate funding sources for the construction of soccer fields and associated parking facilities along the south side of the east-west segment of the Prairie Farms Trail.
- m. Prepare an annual Capital Improvements Program (CIP) to help identify major public park projects, including land acquisition, equipment acquisition, building maintenance and development, and other park projects; and the funding sources to implement desired projects. Implement capital improvement projects based on available resources as determined by the Village Board.
- n. As funding becomes available, implement the conceptual park development improvements as shown in this Plan for Prairie Springs Park, Ingram Park, Momper's Woods Nature and Educational Center, Carol Beach Park, Lake Michigan Park, North Creekside Park, Rolling Meadows Park, Village Green Park, Becker Park, and Woodlawn Park.

**Programs:** (proposed to be implemented within next five (5) years as funding allows)

- a. Review the Impact Fee Ordinance and recommend updates to the needs analysis to determine whether additional impact fees should be collected for capital improvement projects.
- b. Continue to support and implement the Kenosha County Gypsy Moth Suppression Programs and Emerald Ash Borer Programs.
- c. Continue to work with nonprofit conservation organizations to support implementation of methods to control the spread of invasive species (e.g. cutting, spraying and controlled burns), with a focus along major environmental corridors through the Village, such as the Des Plaines River watershed, Chiwaukee Prairie, Kenosha Sand Dunes, and Carol Beach natural areas.
- d. Complete the seeding and install handicapped accessible playground equipment in the Village Green Neighborhood Park.
- e. Install handicapped accessible playground equipment and create a playfield in Creekside Park within the Creekside Crossing Development.
- f. Plant trees, landscape monument sign and pave entrance; construct a dog park and construct a fishing pond; install new perimeter fencing and construct a sledding hill and cross country ski trails in Ingram Park.
- g. Construct a new restroom/concession building; grade and seed the park to create two full-sized soccer fields; install irrigation, fencing and soccer goals; and repair lighting for the redevelopment of Pleasant Prairie Park.
- h. Install irrigation system for south ballfields along STH 165 in Prairie Springs Park.
- i. Construct a parking lot expansion adjacent to the north ballfields in Prairie Springs Park.
- j. Construct a pavilion at Picnic Area 2 in Prairie Springs Park.
- k. Extend sanitary sewer and water to serve the new concessions building and pave the north ballfields parking lot in Prairie Springs Park.
- I. Install a park identification sign for Harrison Road Park (Woodlawn Park).

- m. Install curb and gutter, paving and landscaping within and adjacent to the existing south gravel parking lot in Prairie Springs Park.
- n. Begin constructing site improvements in the Momper's Woods Nature and Educational Center.
- o. Apply for grants/funding for the development of soccer fields on Village-owned land adjacent to Park Drive at the northwest corner of Prairie Springs Park.

## IMPLEMENTATION OF PROGRAM RECOMMENDATIONS

The implementation of the program recommendations shall be based upon the projected and anticipated available staff time, budgeted financial resources, community donations, and volunteer participation. In reality, the programs would be implemented in an incremental basis over the next several years. The Village Board, Commissions and the various Village Departments including—Parks, Public Works, Recreation (RecPlex), Administration, Community Development, Engineering, Information Technology, etc. should all be considered stakeholders involved in the Plan implementation, depending upon the specific activity or program event.

The final Chapter, Chapter 8, will specifically set forth the recommended park and open space acquisition areas; park, open space and trail conceptual plans; specific proposed improvements for several of the Village's park, open space and trail areas and the locations identified for future neighborhood parks, school-park neighborhood parks and community park land acquisitions.

# CHAPTER 8

# RECOMMENDED PARK, OPEN SPACE AND TRAIL CONCEPTUAL PLANS, PROPOSED IMPROVEMENTS AND FUTURE PARK AND OPEN SPACE AREAS

The following recommendations are based on the Village projected growth rates and distributional service area deficiencies identified in the preceding chapters of this Plan. In total, this Plan recommends adding 14 new parks throughout the Village and making improvements to 10 existing parks over the next approximate 25 years, which reflects the timing of the Village's 2035 Comprehensive Plan. Maps 8.1 and 8.2 as reference in this Chapter on found at the end of this Chapter.

Future Parks are shown on Map 8.1 and Table 8.1 below. Based upon the 2035 population projections, these recommendations, which would likely occur incrementally over time, would satisfy SEWRPC's and the Village's park and recreation standards. The timing of park land acquisitions and park development should coincide with the actual demand for park and recreational facilities to address current needs and the needs for the newly developing areas of the community.

Future Park			
NEIGHBORHOOD PARKS			
Carol Beach Unit W Park			
Creekside Park (North) Park			
Pleasant Farms Park			
Pleasant Homes Park			
Sheridan Woods/Prairie Trails East Park			
Sheridan Woods Park			
Green Hill Farms Park			
NEIGHBORHOOD SCHOOL/PARKS			
Barnes Creek Neighborhood Elementary School			
Pleasant Homes Neighborhood Elementary School			
Village Green Neighborhood Elementary School			
Tobin Road Neighborhood Elementary School			
COMMUNITY SCHOOL/PARKS			
Prairie Lane Neighborhood Elementary and Middle School			
Highpoint Neighborhood Middle School			

TABLE 8.1PROPOSED NEW PARKS

As reflected in the Village's 2035 Comprehensive Land Use Plan, Map 8.2 in this chapter shows by means of concentric rings the planned park and recreational facilities service areas based upon the existing, recently acquired and proposed future Neighborhood Parks, Neighborhood Schools/Parks, Community parks and Regional Park areas in the Village. As new park areas are acquired by the Village though donations, dedications or land purchases, detailed Conceptual Plans are prepared for their eventual development.

#### PROPOSED PARK CONCEPTUAL PLAN DEVELOPMENTS AND RECOMMENDATIONS

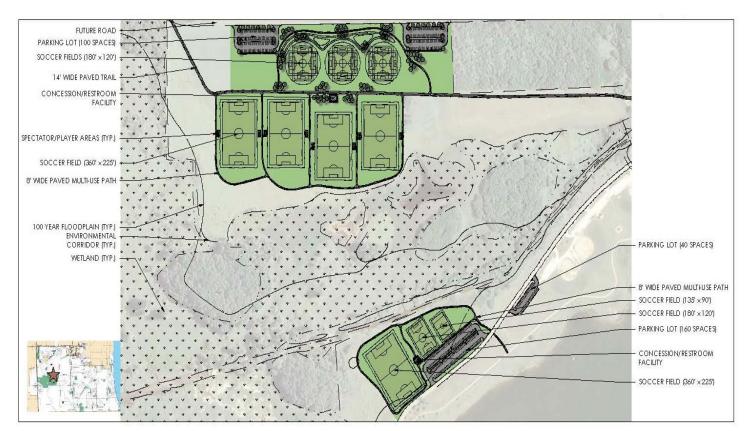
As a part of this and previous park planning processes, there have been numerous Park Conceptual Plans prepared to further guide the development and enhancement of the Village's park, recreation and trails system. The Conceptual Plans include an analysis of the capital improvements needed to construct improvements in the parks at the Regional, Community and Neighborhood levels. The implementation of these Conceptual Plans will be the Village's priority over the next five (5) years as the budgetary means will allow for their development. General descriptions of the park recommendations are listed below. In addition see Appendix D for larger Conceptual Plans described below.

**Existing Regional Park:** It is recommended that the Village work with other public agencies to expand recreational and parking facilities that would benefit not only the users of Prairie Springs Park but also the larger community. Both outdoor recreational improvements such as new soccer fields and the expansion of parking through a proposed Park and Ride parking lot across from the Village RecPlex would benefit the IH-94 traveling public, local commuters as well as the recreational users of the Park.

#### Prairie Springs Park:

- Acquire and develop ten (10) new, outdoor soccer fields on the northwest corner of Prairie Springs Park west of 88<sup>th</sup> Avenue (CTH H) and north of Park Drive. One (1) soccer area would be adjacent to a future proposed public High School site that has been identified on the Village's and KUSD's Comprehensive Plans and one (1) soccer area adjacent to the existing Park Drive in Prairie Springs Park. A portion of the land is currently privately owned by a local land owner farmer, with a portion of the site located with the Village's existing regional park. The development would specifically include four (5) 360' x 225' soccer fields; (4) 128' x 120' soccer fields; and one (1) 135' x 90' soccer field; two (2) 100 space parking lots and one (1) 160 space parking lot; and two concession/restroom facilities; and a 14 foot wide paved walking trail which wraps around the soccer fields. The Conceptual Plan for the proposed soccer complex is shown in Figure 8.1.
- Work with the State and Kenosha County to construct a 300-car, paved Park and Ride commuter parking lot on the south side of Terwall Terrace, adjacent to STH 165 in Prairie Springs Park.
- Obtain grant funding to assess the condition of natural resources within the park and develop a natural resources/woodland restoration management plan for the Park.
- Extend the trail system within the park and create interpretive exhibits that highlight the natural resources.
- Create a monument signage tribute to Phil Sander at Park entrance.
- Design trails and firebreaks to accommodate 5K and 10K triathlons.

Figure 8.1 Conceptual Plan for additional improvements at Prairie Springs Park



**Existing Community Parks:** It is recommended that the Village continue to develop or redevelop four (4) existing Village-owned community parks over the next 5-10 year time frame—Ingram Park, Village Green Park, Pleasant Prairie Park, and Mompers Woods. The land for both Ingram Park and Mompers Woods were recently donated to the Village in 2010 and 2000, respectively. Both the Highpoint and Stahl Community Parks are proposed to be developed in coordination with KUSD. These developments may occur over the next 10-20 years, depending upon community growth. These parks are generally located on Map 8.1.

<u>Ingram Park</u> – The Village just recently completed a pond, sledding hill and gravel parking lot for this park located in the 5700 block of  $95^{th}$  Street. (See Conceptual Plan Figure 8.2)

- Construct a handicapped accessible fishing ramp/dock.
- Design/develop a dog run/park.
- Add other passive amenities such as picnic and play areas.
- Pave the parking lot and entrance roadway.
- Connect walking path from west end of 89<sup>th</sup> Street into the park.

Figure 8.2 Ingram Park Conceptual Plan



<u>Pleasant Prairie Park</u> - This 22-acre park is located at the intersection of 104th Avenue and Bain Station Road on the western edge of the Village about three-fourths of a mile east of the Des Plaines River. (See Conceptual Plan Figure 8.3).

- Acquire additional parcels of land abutting 104<sup>th</sup> Avenue and abutting to 108<sup>th</sup> Avenue for park expansion purposes.
- Expand the existing activities recreational amenities by improving the baseball/softball facilities and add soccer fields and basketball court.
- Explore opportunities to develop more unique outdoor facilities such as a dog park, disc golf course and a skate park.
- Develop and incorporate a system of boardwalks and interpretive exhibits into the park's adjacent wetland areas. Interpretive displays could include information of the surrounding environment such as details of plant and wildlife that may be observed along a trail or within the park or the history of the land.

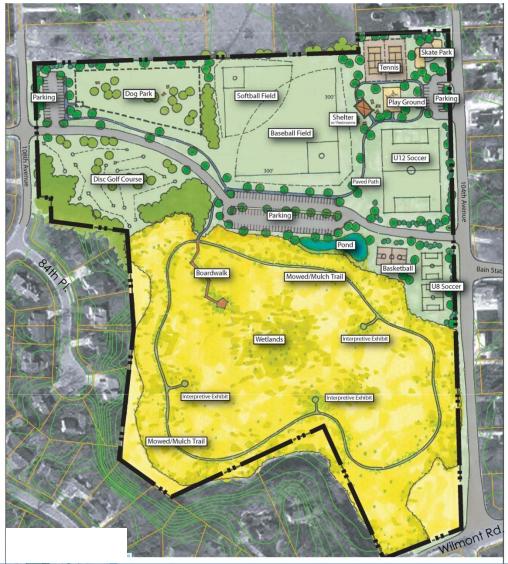


Figure 8.3 Concept Plan for Pleasant Prairie Park

<u>Village Green Park</u> – This 29-acre park is located south of 97th Street and east of Cooper Road on land that was dedicated by the adjacent residential subdivisions. The park is directly northwest of the Village's planned mix-use commercial/retail area referred to as the Village Green Center. (See Conceptual Plan Figure 8.4).

- Install basketball and tennis courts.
- Create a volleyball court.
- Develop a baseball field, soccer field,
- Design a shelter/restrooms facility.
- Connect existing park pedestrian/walking trails to adjacent trails and establish a trailhead.
- Construct a parking lot.
- Utilize open land areas for playfields or picnic areas.

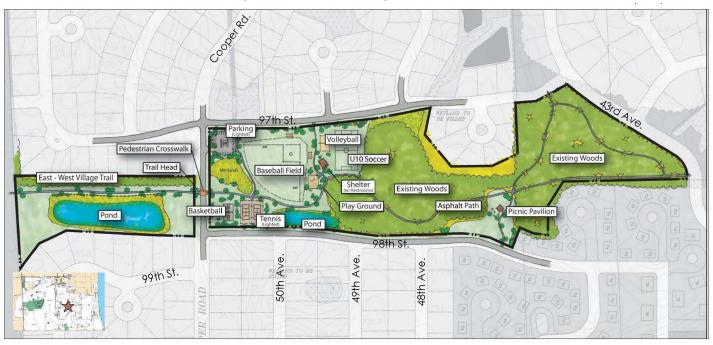


Figure 8.4 Conceptual Plan for Village Green Park

**Existing Neighborhood Parks:** It is recommended that the Village make modifications or improvements to the following Neighborhood Parks over the next five (5) year time frame. These parks are generally located on Map 4.1.

<u>Becker Park</u> - This small 1.5-acre park is located at the intersection of 76th Street and 48th Avenue within the Mid-West Highlands Subdivision along the northern perimeter of Pleasant Prairie. (See Conceptual Plan Figure 8.5).

- Re-grade the land to improve storm water drainage and to prevent collection of water in the middle of the park.
- Improve the condition of the softball field.

- Trim the trees on the perimeter of the park for traffic visibility.
- Upgrade the playground equipment to be handicapped accessible.



Figure 8.5 Conceptual Plan for Becker Park

<u>Carol Beach Park</u> - This 2.9-acre park is located at the intersection of 111th Street and 9th Avenue on the eastern side of Pleasant Prairie near the coast of Lake Michigan. (See Conceptual Plan Figure 8.6).

- Replace the basketball goals.
- Upgrade the playground equipment to be handicapped accessible.
- Improve the condition of the softball field.
- Upgrade the pavilion.

Figure 8.6 Conceptual Plan for Carol Beach Park



<u>Creekside Park (South)</u> - This small 1.69-acre Park is located on the west side of Creekside Circle just north of 93rd Street at 62nd Court in the Creekside Crossing Development. (See Conceptual Plan Figure 8.7).

- Acquire land for Creekside North Park a future park as shown in Figure 8.7 as discussed below
- Construct a walking/pedestrian trail that will link the northern and southern Creekside Parks.

<u>Lake Michigan Park</u> - This 4.5-acre linear park site area is located on Lakeshore Drive along the coast of Lake Michigan between 102nd and 113th Streets. Lake Michigan Park has one designated picnic area, a total of 550 feet of beach along the water's edge, and public access to the Lake. (See Conceptual Plan Figure 8.8).

- Acquire additional land adjacent to the park.
- Explore alternatives for providing controlled and monitored swimming areas and restricting motor boat and jet-ski access through the park.
- Develop permanent restroom facilities.

Figure 8.7 Conceptual Plan for Creekside Park

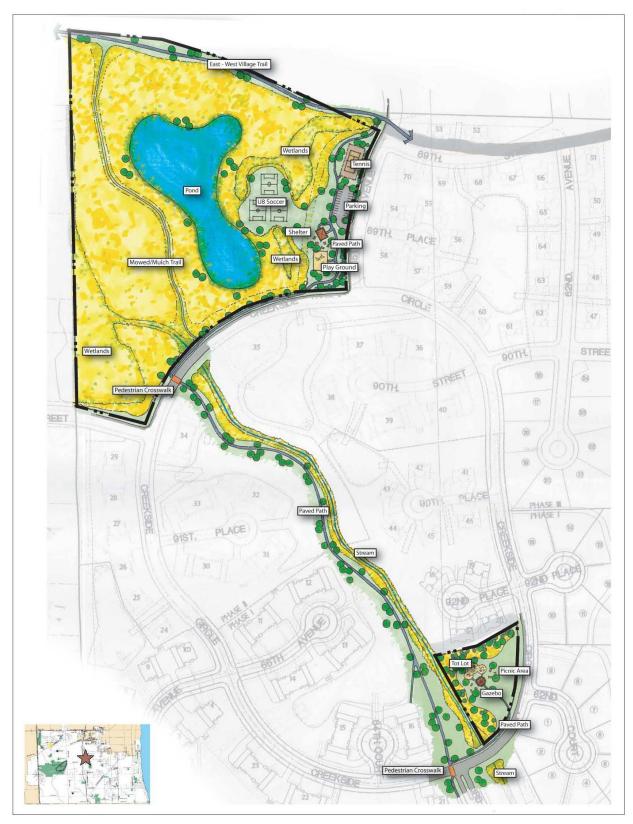
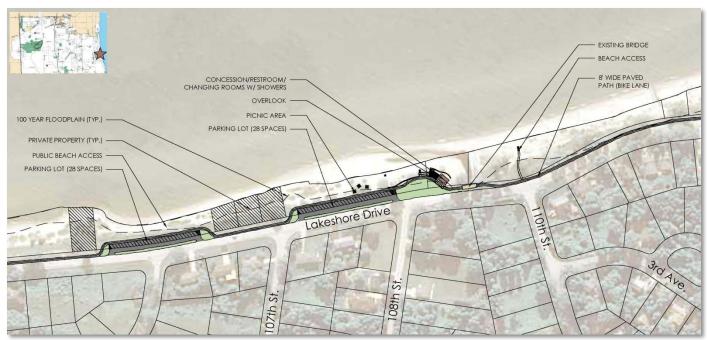


Figure 8.8 Conceptual Plan for Lake Michigan Park



<u>Rolling Meadows Park</u> - This 3.5-acre Park is located at the intersection of 100th Street and 32nd Avenue in the eastern portion of the Village. (See Conceptual Plan Figure 8.9).

• Create a trailhead for the Kenosha County Bike Trail.

Figure 8.9 Conceptual Plan for Rolling Meadows Park



<u>Woodlawn (Harrison Road) Park</u> - This very small, 0.14-acre Park is located within a residential neighborhood on the northern edge of the Village at the intersection of Harrison Road and 50th Avenue (See Conceptual Plan Figure 8.10).

- Modify park signage consistent with other Village parks.
- Upgrade the playground equipment to be handicapped accessible.



Figure 8.10 Conceptual Plan for Woodlawn Park

**Proposed Future Village Neighborhood Parks:** It is recommended that the Village develop five (5) proposed future neighborhood parks over the next 25-year time frame as referenced in the 2035 Comprehensive Land Use Plan as new areas are platted for residential land use development purposes. These parks are generally depicted on Map 8.2.

<u>Carol Beach Unit W Neighborhood Park</u> - As shown on the Village's 2035 Comprehensive Land Use Plan this proposed park is generally located north end of 5<sup>th</sup> Avenue just north of 90<sup>th</sup> Street in the Carol Beach Estates Unit W Subdivision. This is primarily a wooded area proposed for some active amenities. The future park land is currently under private ownership. (See Conceptual Plan Figure 8.11).

<u>Creekside Park (North)</u> - As shown on the Village's 2035 Comprehensive Land Use Plan this proposed park is located north of Creekside Circle in the 6400 block within the Creekside Crossing Development. The future park land is currently under private ownership however, shows the development of soccer fields, tennis courts, a playground and parking with the construction of a walking/pedestrian trail that will link the northern and southern Creekside Parks. (See Conceptual Plan Figure 8.7).

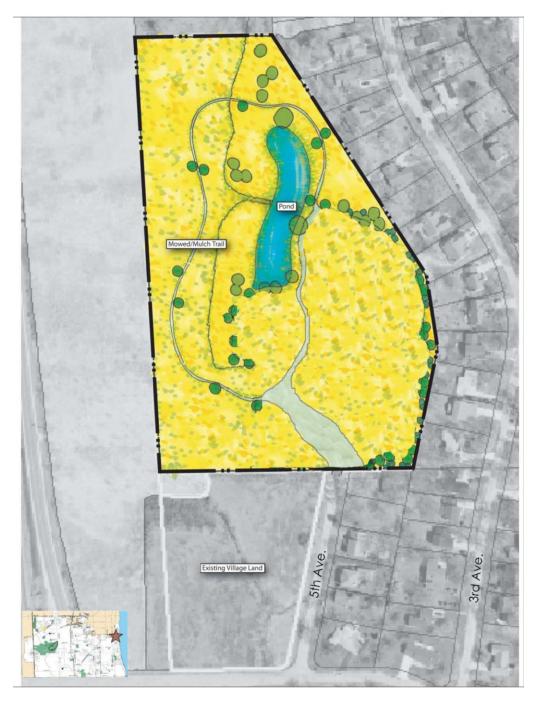


Figure 8.11 Conceptual Plan for Carol Beach Unit W Neighborhood Park

<u>Pleasant Farms Neighborhood Park</u> – As shown on the Village's 2035 Comprehensive Land Use Plan this proposed park is generally located south of Bain Station Road/east of 88<sup>th</sup> Avenue. This is primarily a wooded area proposed for some active amenities. The future park land is currently under private ownership and is anticipated that this land would be dedicated to the Village as the surrounding land develops for residential purposes. <u>Pleasant Homes Neighborhood Park</u> – As shown on the Village's 2035 Comprehensive Land Use Plan this proposed park is generally located west of 47<sup>th</sup> Avenue/south of 123<sup>rd</sup> Street. This is primarily a wooded area proposed for some active amenities. The future park land is currently under private ownership and is anticipated that this land would be dedicated to the Village as the surrounding land develops for residential purposes.

<u>Sheridan Woods/Prairie Trails East Neighborhood Park</u> – As shown on the Village's 2035 Comprehensive Land Use Plan this his proposed park is generally located east of the County Bike Trail at the south end of existing 26<sup>th</sup> Avenue. This is primarily a wooded area that is proposed for more passive recreational amenities. The future park land is currently under private ownership and is anticipated that this land would be dedicated to the Village as the surrounding land develops for residential purposes.

<u>Sheridan Woods Neighborhood Park</u> - As shown on the Village's 2035 Comprehensive Land Use Plan this proposed park is generally located east of the Tobin Woods Subdivision and south of 116<sup>th</sup> Street. This park area is proposed for both active and passive recreational amenities. The future park land is currently under private ownership and is anticipated that this land would be dedicated to the Village as the surrounding land develops for residential purposes.

<u>Green Hill Farms Neighborhood Park.</u> As shown on the Village's 2035 Comprehensive Land Use Plan this proposed park is generally located east and north of Green Hill Farms Subdivision. This park area is proposed for both active and passive recreational amenities. The future park land is currently under private ownership and is anticipated that this land would be dedicated to the Village as the surrounding land develops for residential purposes.

**Proposed Future Neighborhood School/Parks:** There are three existing Neighborhood School/Parks in the Village developed in cooperation with the community. These facilities include: Pleasant Prairie Elementary School, 9208 Wilmot Road; Prairie Lane Elementary School, 10717 47<sup>th</sup> Avenue and Whittier Elementary School, 8542 Cooper Road.

It is recommended that the Village continue to work with the KUSD in the development of future elementary schools and related parks to serve the growing community. A total of five (5) new Neighborhood School/Parks have been identified on the 2035 Comprehensive land Use Plan. These Neighborhood School/Parks would provide for a 15-acre minimum elementary school/neighborhood park. Each park would identify outdoor amenities to serve not only the school but the surrounding neighborhood. Amenities for the parks would include: basketball goals, baseball/softball and soccer fields, tetherball poles, playgrounds and picnic areas. The existing and proposed future neighborhood school /parks in the Village are shown on Map 8-2 include:

# Elementary School/Neighborhood Parks:

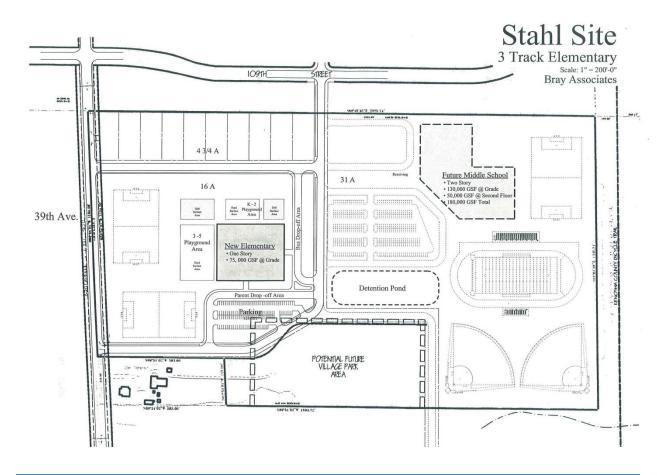
1. <u>Barnes Creek Neighborhood Elementary School/Park</u> to be located generally west of Sheridan Road, north of 104<sup>th</sup> Street.

- 2. <u>Pleasant Homes Neighborhood Elementary School/Park</u> to be located generally southeast of Springbrook Road and 116<sup>th</sup> Street.
- 3. <u>Village Green Neighborhood East Elementary School/Park</u> to be generally located east of 39<sup>th</sup> Avenue and south of 93<sup>rd</sup> Street.
- Tobin Road Neighborhood Elementary School/Park to be generally located west of Sheridan Road and north of 116<sup>th</sup> Street.

## Middle School/Community Parks:

 Prairie Lane Neighborhood School/Community Park – This proposed park would be located south of 110th Street and east of 39th Avenue (CTH EZ) and would be developed in coordination with the KUSD. This development may occur over the next 15-25 years with the development of a future public Elementary School and Middle School. The total size of the park is recommended to be approximately 10acres of Village owned land. An additional approximately 47-acres of adjacent land would be owned and maintained by KUSD for school events and recreational purposes. Stahl Park is proposed to serve as one (1) of the Village's primary outdoor active recreational facilities, potentially providing baseball/softball and soccer fields, basketball goals, tennis courts, and ample parking. (See Conceptual Plan Figure 8.12).

Figure 8.12 Conceptual Plan for Prairie Lane Neighborhood School/Community Park



2. <u>Highpoint Neighborhood School/Community Park</u> – This proposed park would be located east of Old Green Bay Road mid-way between 95th and 104th Streets and would be developed in coordination with the KUSD. This development may occur over the next 15-25 years with the development of a future public Middle School. The total size of the park is recommended to be 65-acres of Village owned land. An additional 22.5 acres of adjacent land will be owned and maintained by KUSD for school events and recreational purposes. Highpoint Park is proposed to serve as one (1) of the Village's primary outdoor active recreational facilities, potentially providing baseball/softball and soccer fields, basketball goals, tennis courts, and ample parking to accommodate large crowds of spectators and athletes. (See Conceptual Plan Figure 8.13).

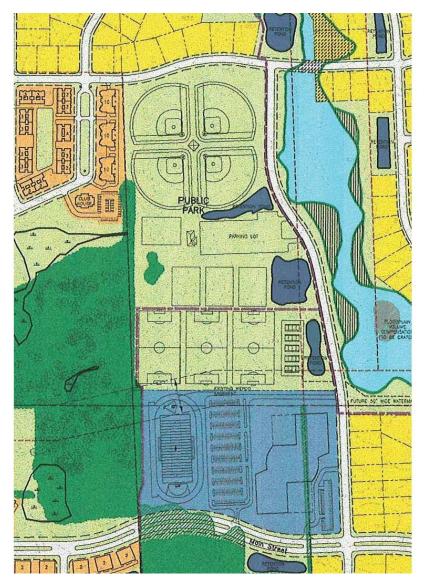


Figure 8.13 Conceptual Plan for Highpoint Neighborhood School/Community Park

# High School/Regional Park:

 Prairie Farms Neighborhood High School/Regional Park – This proposed park area would be located immediately north of Prairie Springs Park and west of 88<sup>th</sup> Avenue (CTH H). In a cooperative planning effort between the Village and the Kenosha Unified School District it is proposed that outdoor sports fields and other amenities would be available to the public through the development of a future 90-acre high school site located in the southern portion of the Prairie Farms Neighborhood. This site is immediately adjacent to proposed soccer fields shown on the conceptual plan for Prairie Springs Park. While this site has been identified in the long range planning reports for both the Village and KUSD, it is likely that this high school development will not occur for at least 15-20 years. (See Neighborhood Plan Figure 8.14).

Proposed Kul S.D. Kul

Figure 8.14 Conceptual Plan for Prairie Farms Neighborhood High School/Regional Park

#### **RECOMMENDATIONS FOR SPECIAL OPEN SPACE AREAS**

In addition to developing community and neighborhood parks, the Village will continue to acquire environmentally, scientifically, historically and archeologically significant lands as donations and dedications are made and as funding becomes available. The Village will also support other federal, State, county, regional and non-profit agencies in such acquisitions. These acquisitions should focus, in particular, on the lands surrounding the Des Plaines River corridor and the Chiwaukee Prairie preservation areas. As such, the Village should work with other public, private and non-profit entities to develop a coordinated lands acquisition plan for the permanent preservation of the corridors adjacent to the Des Plaines and as may be needed with an amendment, expansion or modification of the Land Use Management Plan for the Chiwaukee Prairie-Carol Beach Area.

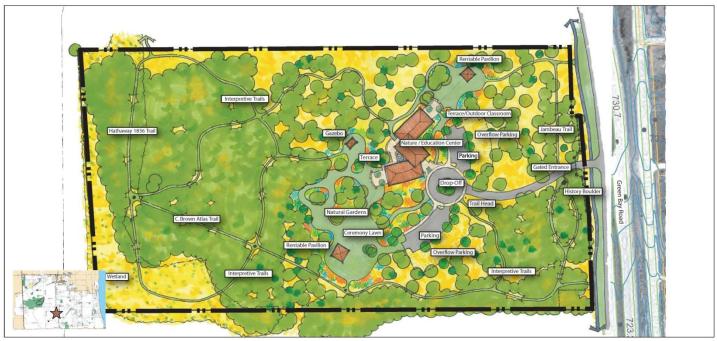
- Work collaboratively with The Nature Conservancy (TNC), Chiwaukee Prairie Preservation Fund, UW-Parkside, WI DNR, and the Lake County Forest Preserve to establish uniform interpretive signage and exhibits throughout the Chiwaukee Prairie-Carol Beach area, including the Kenosha Sand Dunes.
- Support The Nature Conservancy's efforts to transfer TNC-owned parcels within the Chiwaukee Prairie-Carol Beach Area to the WI DNR for long term preservation and management.
- Work collaboratively with The Nature Conservancy, Kenosha-Racine Land Trust and WI DNR and other non-profit groups to establish uniform interpretive signage, trails and exhibits throughout the Des Plaines River corridor.

In addition, two areas currently owned by the Village are proposed to be improved as discussed below.

<u>Mompers Woods</u> – The land for the 26-acre Mompers Woods was acquired by donation in 2000. This special open space, passive community park is located in the central portion of the Village on the west side of STH 31, just south of 108th Street. (See Conceptual Plan Figure 8.15).

- Add park signage that identifies Momper's Woods as a Village park.
- Remove decayed materials and preserve this park as a special open space area.
- Continue to solicit input from local environmentalists, historians and archeologists on how to appropriately manage the park site.
- Prepare more detailed development plans focusing on creating a trail system and establishing parking on the site.
- Create interpretive exhibits to begin the process of transforming the property into a working outdoor classroom and nature study area and showcase the historical significance of the site.
- Explore opportunities to construct a nature center or indoor classroom space on the tree-cleared space on the park site.
- Provide outdoor meeting and reception areas in the park.
- Examine funding opportunities to implement the Conceptual Plan.

Figure 8.15 Conceptual Plan for Mompers Woods



<u>Sorensen Woods at Kildeer Farms Park</u> – This recently acquired 6.56-acre Park land is located west of 47th Avenue and north of 116th Street.

- Create a public-private partnership and work with the local university to create enthusiasm and possible funding source for the restoration of Sorensen's Woods.
- Prepare and adopt woodland oak savanna restoration, preservation and trail plan.
- Establish a small, off-street parking area.
- Add park signage that identifies Sorensen's Woods as a Village Park.
- Start management efforts to restore the woodlands.

One additional area that will function as a special open space area for the community is the future Village Green Center special space and park areas. The Village Green Center, which is proposed to be located north of STH 165 and east of 39<sup>th</sup> Avenue (CTH EZ), is the future downtown area of the community. Proposed special or green spaces will serve to compliment the planned mixed use commercial/retail and residential area proposed to be located in the Village Green Center. The green spaces will be interconnected by a series of walkways, paths and trails and the open areas will serve as civic gathering spaces for future Village-sponsored or other private-group sponsored events, performances and activities.

### **PROPOSED RECREATION TRAILS AND IMPROVEMENTS**

The Village should make use of the existing and future utility, railroad and conservation easements to accommodate future trails whenever possible. The trail system should follow the Village adopted "*Bicycle and Pedestrian Trails Plan*" adopted by the community in April, 2010 (See Appendix E). The trail system, as noted in the Bicycle and Pedestrian Trails Plan, was strategically developed so that the system of trails, provide a link between the existing and future parks and open space areas of the community. The trail system also provides proposed interconnections to the commercial and industrial areas of the Village.

## IMPLEMENTATION

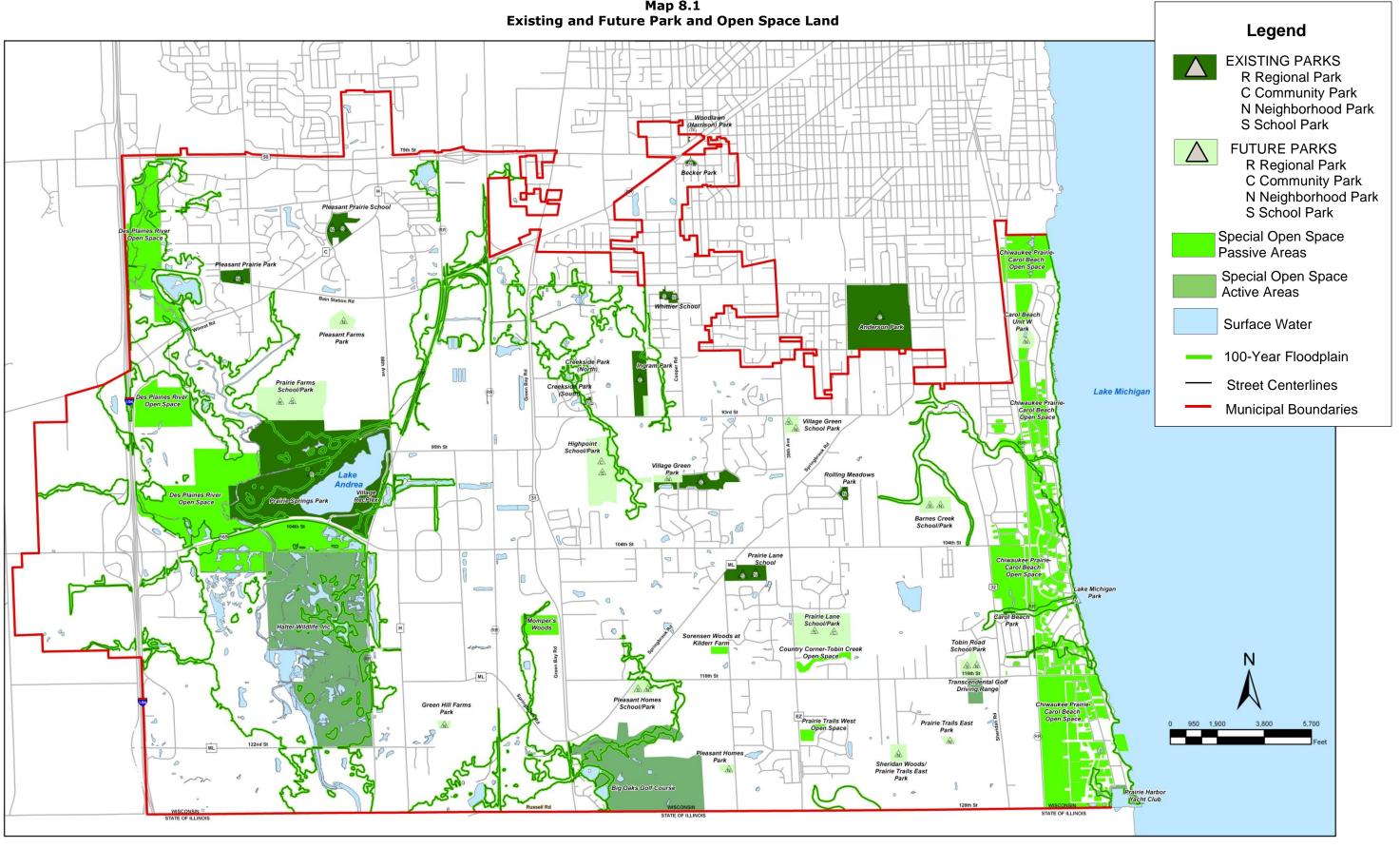
The recommendations presented in this Plan will be phased in over time. The phasing will be dictated by several factors, including private landowner decisions to develop their property for residential use, the availability of grants and donations, and by the availability Village budgetary resources to make the necessary acquisitions or improvements.

There are a number of potential funding sources available to help finance the implementation of the programs identified in this Plan. These funding sources include Village impact fees, donations and those sources listed in Appendix C. It should be noted that funds from many of these programs are subject to change due to fluctuations with current federal, State and local budgets. The park and open space improvements recommended in this Plan should be incorporated incrementally into the regular capital plans and programs of the Village as the budgets allow. This Plan must be updated every five (5) years to ensure that it reflects the evolving needs of the community and retains its WI DNR certification.

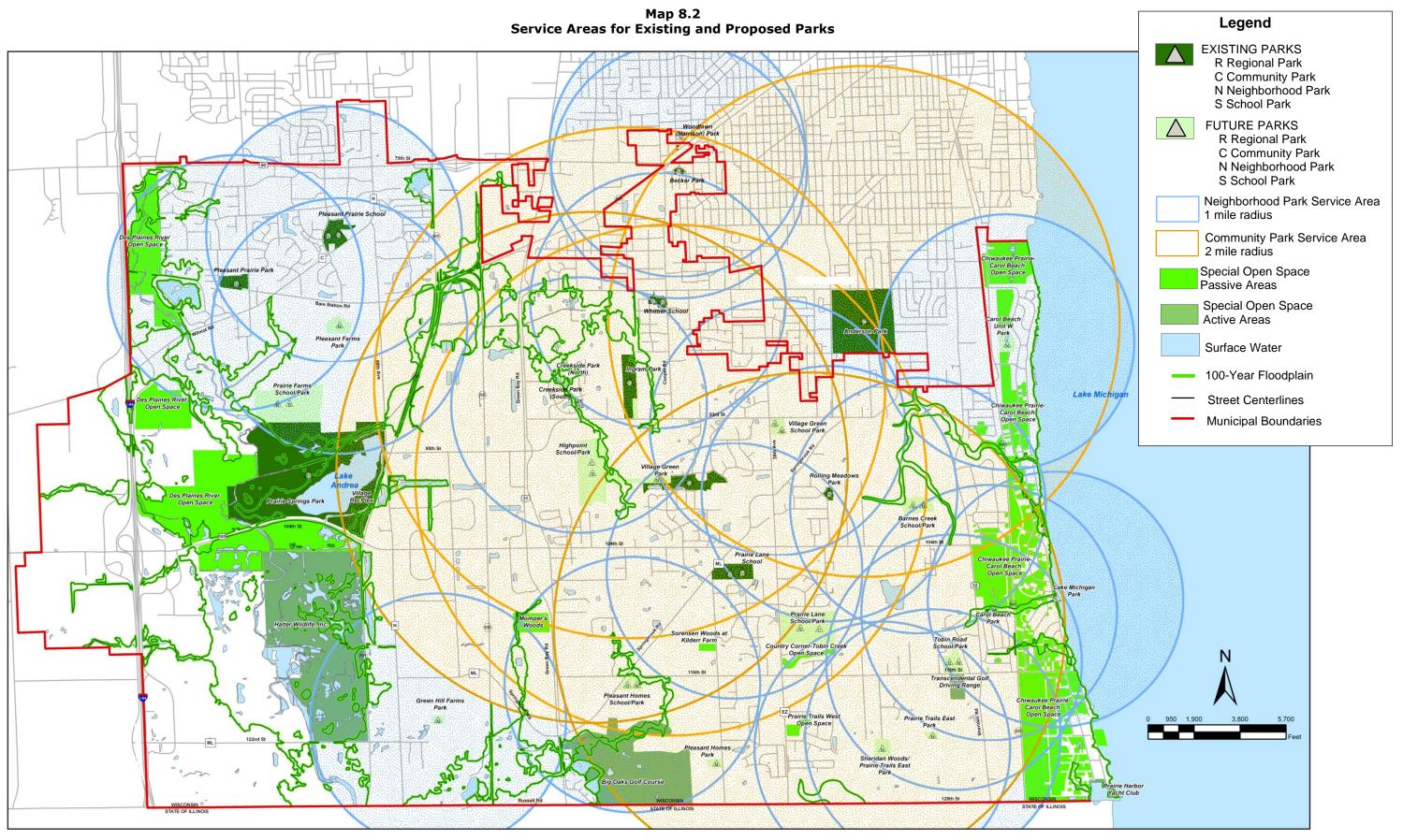
The Village should continue to see intergovernmental cooperation with the State, Kenosha County, KUSD, WI DNR, and other public agencies and private and non-profit agencies (The Nature Conservancy) to help fund and implement the recommendations presented in this Plan.

Pleasant Prairie should continue to utilize its existing planning framework and regulations to implement the recommendations in this Plan. In particular, the Village's Land Division and Development Control Ordinance and development review process, in particular, provide opportunities for the Village to secure parklands as new development occurs. The Village Board, Commissions and the various Village Departments should all be involved in the Plan implementation, depending upon the specific activity or program event.

Map 8.1



Map 8.2



# **APPENDIX A**

# **Pleasant Prairie Parks and Recreation Survey**

# **Pleasant Prairie Parks and Recreation Survey**

Thank you for taking time to complete this survey. Please consider all members of your household when completing this survey. Please respond as soon as you can and return this survey in the enclosed envelope by August 15, 2011. Please call us at 262-925-6730 if you have any questions.

Are you familiar with the Parks a	nd Recreation Services offered	l by the Village of Pleasant Pr	airie?	
Very Familiar	Somewhat Familiar	Not at all F		
How do you or others in your ho	usehold hear about Pleasant F	rairie Park and Recreation Se	ervices?	
PPRP E-mails Word of Mouth Other (Please Describe)	PPRP Program Brochu Newspaper Articles	Ires PPRP Webs	site rision Syste	m
Are you or is anyone in your hou	sehold a member of the Pleas	ant Prairie RecPlex?		
Yes	No	I Don't Know		
How would you characterize you	r satisfaction with the prograr	ns the Pleasant Prairie RecPle	ex offers?	
Very Satisfied Very Dissatisfied	Moderately Satisfied No Opinion	Moderately Dissatisf	ied	
How do you register for program	s at the RecPlex?			
Online Telephone	In Person I Don't	Mail		
		ften do you think you or som	neone in yo	our household use
Never More than 10 Times	2-5 Times I'm Not Sure	6-10 Times	i	
Which of the following special ev spectator or both)	ents do you or members of yo	our household participate? (P	lease checl	k as a participant o
Pleasant Prairie Triathlon Danskin Triathlon Kids Expo Volleyball Tournaments Basketball Tournaments Hockey Tournaments	Participant         Spectator	Trek Triathlon Kids Triathlon Halloween Fest Swim Meets Ice Shows Prairie Family Days Other (Please Specify)	Participant	Spectator
	<ul> <li>Very Familiar</li> <li>How do you or others in your how</li> <li>PPRP E-mails</li> <li>Word of Mouth</li> <li>Other (Please Describe)</li> <li>Are you or is anyone in your how</li> <li>Yes</li> <li>How would you characterize you</li> <li>Very Satisfied</li> <li>Very Dissatisfied</li> <li>How do you register for program</li> <li>Online</li> <li>Telephone</li> <li>Regardless of if you are a member</li> <li>the fitness center in the RecPlex</li> <li>More than 10 Times</li> <li>Which of the following special everspectator or both)</li> <li>Pleasant Prairie Triathlon</li> <li>Danskin Triathlon</li> <li>Kids Expo</li> <li>Volleyball Tournaments</li> <li>Basketball Tournaments</li> </ul>			How do you or others in your household hear about Pleasant Prairie Park and Recreation Services?

8. Do you or anyone in your family participate in the following recreation programs offered at the Pleasant Prairie RecPlex or anywhere else? (Please check all that apply) RecPlex

Elsewhere

RecPlex Elsewhere

	General Fitness Center Usage	Group Fitness Classes	
	Fitness Programs for Kids	Indoor Triathlons	
	Massage Therapy	Swimming Lessons	
	General Pool Usage	Lap Swimming-Waterpark	
	Lap Swimming-Aqua Arena	Preschool Programs	
	Children's Day Care	Before & After School Care	
	Adult Day Care	Therapeutic Recreation	
	Special Needs Care	Adult Basketball	
	Youth Basketball	Men's Softball	
	Youth Baseball	Co-Ed Softball	
	Youth Soccer	Co-Ed Soccer	
	Adult Volleyball	Ice Skating Camp	
	Summer Camp	Adult Hockey	
	Programs for the Arts	Crafts Programs	
	Youth Hockey	Other (Please Specify)	
	Personal Trainers		
		_	
9.	Why do you go elsewhere for recreation serv		
	The RecPlex doesn't offer ther		
	Prices are cheaper	Convenience	
	Our friends go there	Other (Please Specify)	
10.	What could we do to get your to enroll in mo	e of our recreation programs?	
	Nothing, I don't have the time	Offer programs at cheaper prices	
	Offer them at convenient time	Offer them at different locations	
	Offer different programs (Plea	e Specify)	
	Other (Please let us know how	L	
11	About how many times would you estimate a	ou or other family members used each of the following facilities over the pas	+
<u></u>	12 months? (Please check the box to the left		•
	Lake Andrea Beach	ake Michigan Park/Beach	
	□ 1-4 times	1-4 times	

- $\hfill\square$  5 or more times
- $\ \ \square \ \ Never$

#### **Pleasant Prairie Park**

1-4 times

#### $\square$ 5 or more times

 $\square$  Never

#### **Rolling Meadows Park**

1-4 times

#### $\square$ 5 or more times

Never

#### Chiwaukee Prairie State Natural Area

 $\Box$  1-4 times

#### $\hfill\square$ 5 or more times

 $\hfill\square$  Never

5 or more times
Never
Becker Park
1-4 times
5 or more times
Never
Carol Beach Park
1-4 times
5 or more times

#### $\square$ Never

#### **Prairie Springs/Veterans Memorial Park**

- $\Box$  1-4 times
- $\hfill\square$  5 or more times
- $\hfill\square$  Never

12. About how many times have you or other family members used the following park facilities or activity areas in Pleasant Prairie over the past 12 months? (Please check the box of the correct answers)

	Bicycle Paths	Archery Range	
	□ 1-4 times	□ 1-4 times	
	5 or more times	5 or more times	
	Never	Never	
	Running/Walking Paths	Ball Diamonds	
	□ 1-4 times	□ 1-4 times	
	5 or more times	5 or more times	
	□ Never	□ Never	
	Playgrounds	Marina	
	□ 1-4 times	□ 1-4 times	
	5 or more times	5 or more times	
	□ Never	Never	
	Picnic Shelters	Beaches	
	□ 1-4 times	□ 1-4 times	
	5 or more times	5 or more times	
	□ Never	Never	
13.	How satisfied are you with the condition ar	nd maintenance of the parks and facilities?	
		t Satisfied Somewhat Dissatisfied Very Diss	satisfied
	Very Satisfied Somewhat		
<mark>14.</mark>		ers of your household use the lake Andrea Beach more frequently	
<mark>14.</mark>	What amenities would make you or member	ers of your household use the lake Andrea Beach more frequently	
<mark>14.</mark>	What amenities would make you or member Expanded Beach Area	ers of your household use the lake Andrea Beach more frequently More Shade Areas	
<mark>14.</mark>	What amenities would make you or member	ers of your household use the lake Andrea Beach more frequently More Shade Areas More Picnic Areas	
<mark>14.</mark>	What amenities would make you or member Expanded Beach Area	ers of your household use the lake Andrea Beach more frequently More Shade Areas	
<mark>14.</mark>	What amenities would make you or member          Expanded Beach Area          Expanded Swim Area	ers of your household use the lake Andrea Beach more frequently More Shade Areas More Picnic Areas	
14.	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide	ers of your household use the lake Andrea Beach more frequently More Shade Areas More Picnic Areas Nothing, I don't use it	
<mark>14.</mark>	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide	ers of your household use the lake Andrea Beach more frequently More Shade Areas More Picnic Areas Nothing, I don't use it	
	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft	ers of your household use the lake Andrea Beach more frequently More Shade Areas More Picnic Areas Nothing, I don't use it	? 
	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft	ers of your household use the lake Andrea Beach more frequently More Shade Areas More Picnic Areas Nothing, I don't use it Other (Please Specify)	? 
	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft         What amenities would make you or membra          Lifeguards	ers of your household use the lake Andrea Beach more frequently More Shade Areas More Picnic Areas Nothing, I don't use it Other (Please Specify)  ers of your household use the Lake Michigan Beach more frequen	? 
	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft         What amenities would make you or membra          Lifeguards          Kayak Launch	ers of your household use the lake Andrea Beach more frequently           More Shade Areas          More Picnic Areas          Nothing, I don't use it          Other (Please Specify)          Other (Please Specify)          Restrooms          Sailboat Launch	? 
	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft         What amenities would make you or membra          Lifeguards          Kayak Launch          Beachfront Shelter	ers of your household use the lake Andrea Beach more frequently           More Shade Areas          More Picnic Areas          Nothing, I don't use it          Other (Please Specify)          Restrooms          Sailboat Launch          Nothing, I don't use it	? 
	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft         What amenities would make you or membra          Lifeguards          Kayak Launch	ers of your household use the lake Andrea Beach more frequently           More Shade Areas          More Picnic Areas          Nothing, I don't use it          Other (Please Specify)          Restrooms          Sailboat Launch          Nothing, I don't use it	? 
15.	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft         What amenities would make you or membra          Lifeguards          Kayak Launch          Other (Please Specify)	ers of your household use the lake Andrea Beach more frequently           More Shade Areas          More Picnic Areas          Nothing, I don't use it          Other (Please Specify)          Restrooms          Sailboat Launch          Nothing, I don't use it	? 
15.	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft         What amenities would make you or membra          Lifeguards          Kayak Launch          Other (Please Specify)         Which of the following activities would you	ers of your household use the lake Andrea Beach more frequently           More Shade Areas          More Picnic Areas          Nothing, I don't use it          Other (Please Specify)          Restrooms          Sailboat Launch          Nothing, I don't use it	? 
15.	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft         What amenities would make you or membra          Lifeguards          Kayak Launch          Beachfront Shelter          Other (Please Specify)         Which of the following activities would you          Ethnic Festivals	ers of your household use the lake Andrea Beach more frequently           More Shade Areas          More Picnic Areas          Nothing, I don't use it          Other (Please Specify)          Restrooms          Sailboat Launch          Nothing, I don't use it	? 
15.	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft         What amenities would make you or membra          Lifeguards          Kayak Launch          Beachfront Shelter          Other (Please Specify)         Which of the following activities would you          Ethnic Festivals          Theatre in the Park	ers of your household use the lake Andrea Beach more frequently           More Shade Areas          More Picnic Areas          Nothing, I don't use it          Other (Please Specify)          Restrooms          Sailboat Launch          Nothing, I don't use it          Craft Festivals          Craft Fistivals          Nothing, it's fine now	? 
15.	What amenities would make you or membra          Expanded Beach Area          Expanded Swim Area          A Waterslide          A Raft         What amenities would make you or membra          Lifeguards          Kayak Launch          Beachfront Shelter          Other (Please Specify)         Which of the following activities would you          Ethnic Festivals	ers of your household use the lake Andrea Beach more frequently           More Shade Areas          More Picnic Areas          Nothing, I don't use it          Other (Please Specify)          Restrooms          Sailboat Launch          Nothing, I don't use it	? 

17. Please rate each of the following Pleasant Prairie priorities in terms of how important it would be to you and your family.

1	L. Maintaining Existing Recreation Programs	5	4	3	2	1
	2. Improving Existing Recreation Programs	5	4	3	2	1
	3. Acquiring New Park Land	5	4	3	2	1
4	I. Improving Existing Park/Facilities	5	4	3	2	1
5	5. Building New Facilities	5	4	3	2	1
	he following amenities would you like to see added	d and/or i	renovated	<mark>l in the pa</mark>	<mark>rks in the</mark> l	next 5 years
(Please che	eck all that apply)					
	Bike Paths		Ice Sk	-		
	Skate Park			round Equ		
	Lighted Walking Paths		-	tball Cour		
	Outdoor Fitness Stations		-		ind/Splash	pad
	Softball/Baseball Fields with Natural Grass			Shelters		
	Soccer Fields with Natural Grass		Decor	ative Gaz	ebos	
	Football Fields with Natural Grass		Dog P	ark		
	Softball/Baseball Fields with Artificial Grass		Veget	able Gard	en Plots	
	Soccer Fields with Artificial Turf		More	Flowerbe	ds/Landsc	aping
	Football Fields with Artificial Turf		Wood	lland Rest	oration	
	Athletic Field Lighting		Tenni	s Courts		
	Frisbee Golf Course		Drinki	ing Founta	ains	
	Park Benches					
	nave you lived in Pleasant Prairie or your current re		,		pecify)	
How long c		ork?	,m	years inutes		ne past year
How long c	nave you lived in Pleasant Prairie or your current re does it take you to commute from your home to w	ork?	,m	years inutes		ne past year
How long c	nave you lived in Pleasant Prairie or your current re does it take you to commute from your home to w	ork?	m	years inutes arks or fac		ne past year 
How long o Please tell What ame	nave you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in	ork?	m	years inutes arks or fac		ne past year 
How long of Please tell What amen Are you a f	nave you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in female or male?	ork?	m	years inutes arks or fac		ne past year 
How long o Please tell What ame	nave you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in female or male?	ork?	m	years inutes arks or fac		ne past year 
How long of Please tell What amen Are you a f	nave you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in male Male	ork? a Pleasant n the nex	. Prairie pa	years inutes arks or fac		ne past year 
How long of Please tell What amen Are you a f	nave you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in female or male?	ork? a Pleasant n the nex	. Prairie pa	years inutes arks or fac		ne past year
How long of Please tell What amen Are you a f	nave you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in male Male	ork? a Pleasant n the nex	. Prairie pa	years inutes arks or fac		ne past year' 
How long of Please tell What amen Are you a f Eeginning v	have you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in female or male? hale Male with yourself, what are the ages of those in your h	ork? a Pleasant n the next	r Prairie pa	years inutes arks or fac	ilities in th	
How long of Please tell What amen Are you a f Fem Beginning v Please add	nave you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in male Male	ork? a Pleasant n the next	r Prairie pa	years inutes arks or fac	ilities in th	
How long of Please tell What amen Are you a f Fem Beginning v Please add	have you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in male Male with yourself, what are the ages of those in your h Male	ork? a Pleasant n the next	r Prairie pa	years inutes arks or fac	ilities in th	
How long of Please tell What amen Are you a f Fem Beginning v Please add	have you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in male Male with yourself, what are the ages of those in your h Male	ork? a Pleasant n the next	r Prairie pa	years inutes arks or fac	ilities in th	
How long of Please tell What amen Are you a f Fem Beginning v Please add	have you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in male Male with yourself, what are the ages of those in your h Male	ork? a Pleasant n the next	r Prairie pa	years inutes arks or fac	ilities in th	
How long of Please tell What amen Are you a f Fem Beginning v Please add	have you lived in Pleasant Prairie or your current re does it take you to commute from your home to w us about any unpleasant experiences you've had a nities would you like to see added to the RecPlex in male Male with yourself, what are the ages of those in your h Male	ork? a Pleasant n the next	r Prairie pa	years inutes arks or fac	ilities in th	

# **APPENDIX B**

# Open Ended Responses to Community Survey Questions 21 and 25

# **Open Ended Responses to Community Survey Questions 21 and 25**

**Question 21**: "Please tell us about any unpleasant experiences you've had at Pleasant Prairie parks or facilities in the past year."\*

**Question 25**: "Please add any comments that you or your family would like to share with the Pleasant Prairie parks and Recreation Departments."\*

### **Comments Related to Parks and Outdoor Parks Facilities**

### Positive Comments:

- Appreciate being asked for input on community services.
- Enjoy walking on the paths at Prairie Springs Park.
- Every event at Prairie Family Days is fun.
- Excellent job with parks.
- Great parks and recreation services. Everything is kept clean and new looking.
- Residents love living in the Village.
- Parks staff is friendly, courteous and responsive.
- Thanks to all of those you make Pleasant Prairie so pleasant.
- The current parks are beautiful-keep it up.
- Village is doing a good/great job.

### Experiences, Recommendations and Comments:

- Add benches in parks to watch softball practices.
- Baseball fields need irrigation and lighting.
- Basketball courts are needed in parks.
- Beach needs clean-up, noise and other rules enforced at Lake Michigan Beach.
- Bicycle paths are needed off-street throughout Village for safety.
- Bicycle speed needs to be enforced at Prairie Springs Park.
- Boating is restricted on Lake Andrea, yet several special events use motor boats.
- Bow hunting areas are needed in the Village.
- Bring in more special events especially with music.
- Commercial services are needed in Prairie Springs Park (near the former Common Grounds) such as restaurants and other services.
- Conflicts with walkers and bicyclists on paved paths.
- Construct more walking paths to Prairie Springs Park and RecPlex.
- Dog leash and clean-up laws need to be enforced in parks and on paths.
- Fireworks and laser show desired at Prairie Family Days.
- Focus resources on the other parks other than Prairie Springs Park/RecPlex.
- Garbage cans are needed in more areas and garbage must be picked up more often in the Lake Michigan and Carol Beach Parks.
- Ice skating outdoors should be provided at a Village park.
- Inadequate parking areas at Prairie Springs Park during RecPlex special events.
- More park amenities playgrounds should be added to other parks, not just Prairie Springs Park.
- Lake Andrea beach area needs to be enlarged.

- Lake Andrea being closed to public windsurfing during the many special events is a problem.
- Lake Andrea should be stocked with fish.
- Lake Andrea is too small for electric trolling motors.
- Lake Andrea parking stickers should not be required.
- Lake Andrea shade is needed at the beach.
- Lake Michigan Beach is overcrowded with out-of-state users, charge out-of-state residents.
- Lake Andrea lifeguards are easily distracted at the beach.
- Lights are needed in the parks Carol Beach and Rolling Meadows Parks.
- Marketing materials/brochures about the Village's parks and recreational facilities are needed and should be sent out to residents.
- Maintenance of Village roads would improve accessibility to Prairie Springs Park.
- Outdoor pool is needed, along with a lazy river.
- Parents need to supervise their children more closely at the playgrounds.
- Park rental fees should be reduced for Village residents.
- Park users should not be parking vehicles in the 104<sup>th</sup> Avenue travel lanes.
- Paths and sidewalks need to be accessible in the winter months.
- Prairie Family Days is a great event and should continue in some form for residents.
- Property taxes should not increase to make park improvements.
- Provide dog stations with bags in the parks, especially along trails.
- Regular Lake Michigan beach clean-up needed.
- Restrooms in parks should be cleaner and open in the spring.
- Scheduling and traveling issues experienced during summer triathlons.
- Scoreboard is not working at the ballfields.
- Special events, especially triathlons impose on park, facility and Village roadways.
- Special event variety is needed other than exercise related e.g. free music festivals free car shows, 4<sup>th</sup> of July celebration, sail boat regatta.
- Spray for mosquitoes in the parks, especially Prairie Springs Parks.
- Tennis courts are needed outside.
- Triathlons should look for other routes-stop closing roads so often.
- Vandalism in the public parks and beach areas, including portable toilets being knocked over.
- Village residents should be provided with free passes to the Lake Andrea Beach.
- Violation of park curfew hours and inappropriate late night activities is a problem, especially in Carol Beach and Lake Michigan Parks.
- Walking trails should be extended throughout the community, especially in residential areas.
- Watch the spending and budget for growth and changes.
- Water fountains/stations are needed in parks and around Lake Andrea path.

\*The open ended answers were summarized. The responses are listed alphabetically and not in priority order.

### **Comments Related to RecPlex Facilities**

Positive Comments:

- Appreciates special needs programs and adult day care.
- Day care services are wonderful.
- Drop-in day care offers a very good service.
- Great facility, love using the facility.
- Great foundation in the Village.
- Keep up the good work.
- Members enjoy using the facility.
- RecPlex and Lake Andrea are great places.
- RecPlex is doing a great job.
- RecPlex is a first class facility.
- The facility is awesome.
- The volunteers are great and very organized.

### Experiences, Recommendations and Comments:

- Adult day care facilities are wonderful for special needs persons.
- Brochures and other printed information should be provided about the RecPlex and Prairie Springs Park.
- Charging of tournament fees even with family membership to RecPlex is a problem.
- Children's inappropriate behaviors/language in the locker rooms needs to be monitored.
- Clarify that the RecPlex facility is not being funded by the Village property tax payers.
- Class sign up on line and refund problems for dropped classes.
- Classes such as group fitness should be free if a membership is purchased.
- Concerns regarding summer camp program planning, events and staff.
- Consider adding facilities and teams for competitive travel soccer.
- Front desk staff needs to be more focused and less staff conversation.
- Healthy cooking classes and snack bar healthy foods is desired.
- Hockey Programs should be expanded to include AAA, bantam, peewee and squirt levels
- Increase spacing between stationary weight machines.
- Inside walking track is blocked with gawkers, walkers and users stretching on track.
- Items stolen from the locker room.
- Limitations on field house to members/users due to outside organized sporting events.
- Lockers in women's locker room are getting old and are difficult to open and shut.
- Locker rooms need updating.
- More focus is needed on customer service.
- Notices are sent out late for cancelled organized sports.
- Offer greater business/corporate discounts for employees to benefit.
- Organize baseball teams fairly.

- Parking fees at the RecPlex parking lot is causing RecPlex users to park on adjacent street.
- Parking is inadequate and difficult to maneuver because of gating at the RecPlex.
- Parking needs to be expanded at RecPlex.
- Pool temperature is cold.
- RecPlex facility is overcrowded during open gym.
- RecPlex Members should not have to pay for parking in the lot.
- RecPlex Membership and facility costs inhibit resident users during tough economic times.
- RecPlex Memberships should be more affordable and offered for: one (1) to six (6) months, summer memberships, senior citizens and college memberships.
- RecPlex Memberships are cancelled due to high class program fees. Free classes should be offered.
- RecPlex revised/reduced membership pricing structure should be considered.
- Requests for information, telephone calls, and personal trainer requests were unanswered.
- Routine maintenance is needed on all fitness equipment.
- Senior citizen programs and discounts should be provided.
- Silver Sneakers program should be added to the RecPlex.
- Sound system is needed for aqua classes.
- Swimming lesson problems need to be addressed.
- Towel service should be provided.
- Ventilation is inadequate in the aquatic center, reduce pool chemicals.
- Waiting times for fitness equipment is sometimes a problem.

\*The open ended answers were summarized. The responses are listed alphabetically and not in priority order.

# **APPENDIX C**

# POTENTIAL FUNDING SOURCES FOR PARK AND OPEN SPACE FACILITIES

# POTENTIAL FUNDING SOURCES FOR PARK AND OPEN SPACE FACILITIES

Program	Purpose	Funding Details	Deadline	Notes	Administrative Agency	Contact		
Wisconsin Stewardship Programs								
Aids for the Acquisition and Development of Local Parks (ADLP)	To acquire or develop public, nature-based outdoor recreation areas and facilities	\$4 million avail. Per yr. 50% local match per project	May 1	<ul> <li>A comprehensive outdoor recreation plan is required.</li> <li>Priority for land acquisition</li> <li>Projects must comply with ADA</li> </ul>	WDNR	Jim Ritchie, Southeast Region 414-263- 8610		
Urban Greenspace Program (UGS)	To acquire land to provide natural space within or near urban areas, or to protect scenic or ecological features	\$1.6 million avail. Per yr. 50% local match per project	May 1	<ul> <li>A comprehensive outdoor recreation plan is required</li> <li>Projects must comply with ADA</li> </ul>	WDNR	Jim Ritchie, Southeast Region 414-263- 8610		
Acquisition of Development Rights	To acquire development rights for nature- based outdoor recreation areas and facilities	\$800,000 avail. Per yr. 50% local match per project	May 1	<ul> <li>Funds available to acquire development rights in areas where restrictions on residential, industrial or commercial developments are in place.</li> <li>May include enhancements of outdoor recreation.</li> </ul>	WDNR	Jim Ritchie, Southeast Region 414-263- 8610		
Urban Rivers Grant Program (URGP)	To acquire lands, or rights in lands, adjacent to urban rivers for the purpose of preserving or restoring them for economic revitalization or nature-based outdoor recreation activities.	\$1.6 million avail. Per yr. 50% local match per project	May 1	. A comprehensive outdoor recreation plan is required . Projects must comply with ADA	WDNR	Jim Ritchie, Southeast Region 414-263- 8610		
Federal Program			1					
Land and Water Conservation Fund (LAWCON)	To acquire or develop public, nature-based outdoor recreation areas and facilities	50% local match per project	May 1	. A comprehensive outdoor recreation plan is required	WDNR with TEA-21 Funds	Jim Ritchie, Southeast Region 414-263- 8610		

Program	Purpose	Funding Details	Deadline	Notes	Administrative Agency	Contact
Recreational Trails Act	To provide funds for maintenance, development, rehabilitation and acquisition of land for motorized, non-motorized and diversified trails.	50% local match per project	May 1	. Funds may only be used on trails which have been identified in, or which further a specific goal of a local, county, or state trail plan. . Funds may be used on trails that are referenced in a statewide comprehensive outdoor recreation plan	WDNR with TEA-21 Funds	Jim Richie, Southeast Region 414-263- 8610
	-Modal Improvement					1
Transportation Enhancements Program	Providing facilities for pedestrians and bicyclists. This program provides funding for rehabilitating and operating historic transportation buildings and structures, restoring railway depots, as well as streetscaping "Main Streets" and landscaping near transportation facilities.	. Funded through TEA-21 . 20% required match	February	<ul> <li>Not a grant program. 80% of funds are reimbursed if all federal guidelines are met.</li> <li>Project must relate to surface transportation.</li> <li>Construction projects must be over \$100,000.</li> <li>Non-construction projects must be over \$25,000.</li> </ul>	WisDOT	Mary Frigge, District 262-548- 8763
Surface Discretionary Grant Program (STP-D)	Provides flexible funds, which can be spent on a wide variety of projects, including roadway projects through the Federal-aid highway system, bridges, transit facilities, and bicycle and pedestrian facilities.	. Funded through TEA-21 . 20% required match	February	<ul> <li>Any project that fosters alternatives to single-occupancy vehicle trips.</li> <li>Facilities for pedestrians and bicyclists.</li> <li>System-wide bicycle planning.</li> <li>Construction projects must be over \$100,000.</li> <li>Non-construction projects must be over \$25,000.</li> </ul>	WisDOT	Mary Frigge, District 262-548- 8763

Program	Purpose	Funding Details	Deadline	Notes	Administrative Agency	Contact			
Federal Transit Administration Grants									
Section 5309 (old Section 3 discretionary funds)	Transit capital projects; includes intermodal facilities such as bicycle racks on buses and bicycle parking at transit stations; most funds are to be directed toward transit itself.	. 20% local match per project	Early Spring	<ul> <li>Finding for this program is allocated on a discretionary basis.</li> <li>Congress/</li> <li>Administration can pick the projects; however, the authorization bill contains a list of specific criteria.</li> </ul>	WisDOT Bureau of Transit	Ron Morse 608-266- 1650			
Congestion Mitigation and Air Quality (CMAQ) Improvement Program	Funds projects that will reduce vehicle trips and miles; reduce emissions due to traffic congestion, or reduce the per mile rate of vehicle emissions.	. Funded through TEA-21 . 20% local match	In April of odd numbered years	. Only available to Milwaukee, Kenosha, Racine, Ozaukee, Waukesha, Washington, Sheboygan, Kewaunee, Manitowoc, Walworth, and Door Counties.	USDOT	Mary Frigge, District 262-548- 8763			
	hway Safety Funds		1	1	1				
Community Programs Empowerment Program Enforcement Program	For bicycle and pedestrian safety education and training projects, including helmet promotion and purchases, sponsorship of rodeos, classes and development of brochures.	. 20% local match per project	October- December	. Engineering and maintenance work not eligible for funding.	WisDOT Bureau of Transportation Safety	Thomas Loeffler Southeast District 414-266- 1097			
Highway Safety Program (Section 403)	Available for bicycle/pedestrian education. May also be used to develop safety classes for bicycle/pedestrian offenders.	. 20%- %50 local match per project	February	. For communities that can document bicycle crashes related to motor vehicle violations. . Funds new enforcement programs up to \$1000.	WisDOT Bureau of Transportation Safety	Thomas Loeffler Southeast District 414-266- 1097			
Research Projects	Funds the research needed to substantiate unique local needs for additional safety funding.	. 20% local match per project	February	. A study of transit needs on public lands to assess the feasibility of alternative transportation modes (Section 3039)	WisDOT Bureau of Transportation Safety	Thomas Loeffler Southeast District 414-266- 1097			

Program	Purpose	Funding Details	Deadline	Notes	Administrative Agency	Contact
Other Programs	5					
Wisconsin Main Street Community Program	Comprehensive downtown revitalization program, which includes streetscape improvements.		No Date	. General downtown program . May benefit trail enhancements through streetscaping	National Main Street Center	Wisconsin Dept. of Commerc e, Bureau of Downtown Developm ent 608- 266-7531
Surface Transportation -Environment Cooperative Research Program	Evaluate transportation control measures. Improve understanding of transportation demand factors. Develop performance indicators that will facilitate the analysis of transportation alternatives.	. 20% local match per project	No Date	<ul> <li>\$ available for the development of national bicycle safety education curriculum.</li> <li>\$ available for grants to a national not-for- profit organization engaged in promoting bicycle and pedestrian safety.</li> <li>\$ available for a study of the safety issues attendant to the transportation of school children to and from school land school-related activities by various transportation modes</li> </ul>	FHWA	U.S. Dept. of Transport ation 202- 366-4000
Urban Forestry Grants	Assistance for tree maintenance, planting and public awareness.	\$1,000 to \$25,000 grants awarded with a 50% local match.	October 1	. Funding is prioritized for communities needing to develop an urban forestry plan, needing worker training, and needing to conduct a street tree inventory.	WDNR Urban Forestry	Tracey Teodecki 608-267- 3775
Home Depot Community Improvement (Environment al) Grants	Assistance for forestry and ecology projects, clean-up beautification projects, recycling programs.		No Date		Home Depot Community Affairs	Local Home Depot Store Manager

# APPENDIX D

**Enlarged Conceptual Plans** 

(Figures 8.1 to 8.15)

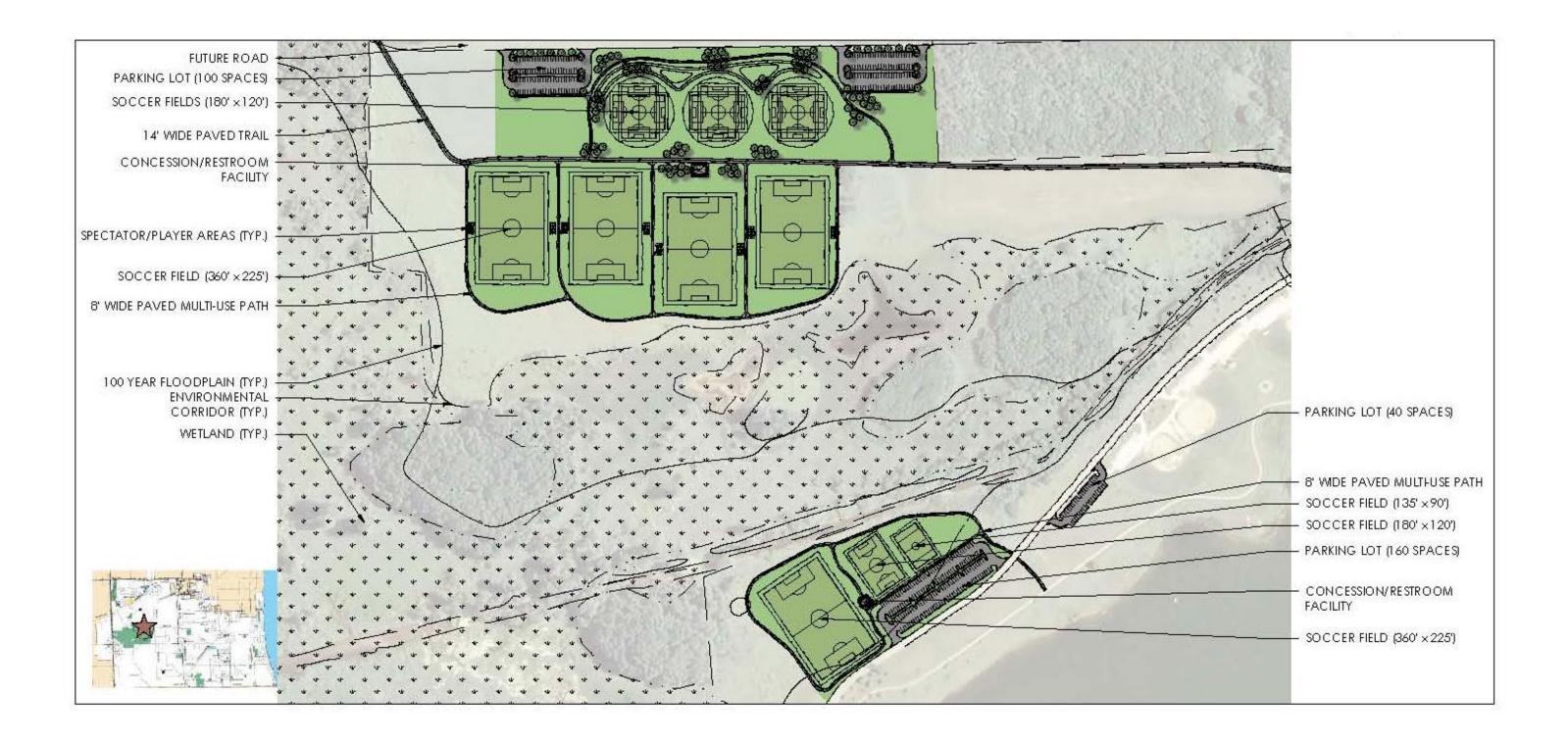


Figure 8.2 Ingram Park Conceptual



2

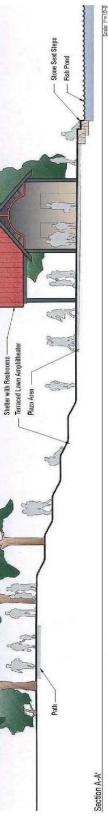


Figure 8.3 Concept Plan for Pleasant Prairie Park



### Figure 8.4 Conceptual Plan for Village Green Park

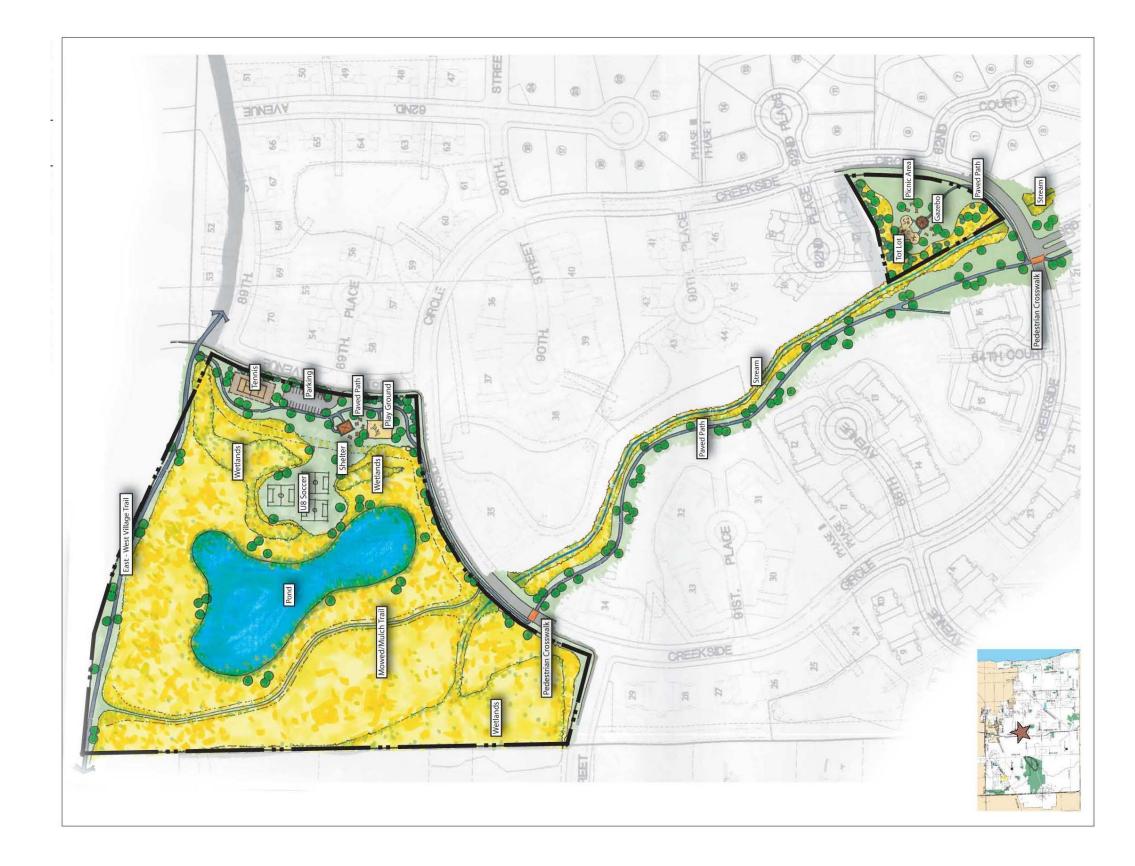


### Figure 8.5 Conceptual Plan for Becker Park



Figure 8.6 Conceptual Plan for Carol Beach Park





### Figure 8.8 Conceptual Plan for Lake Michigan Park

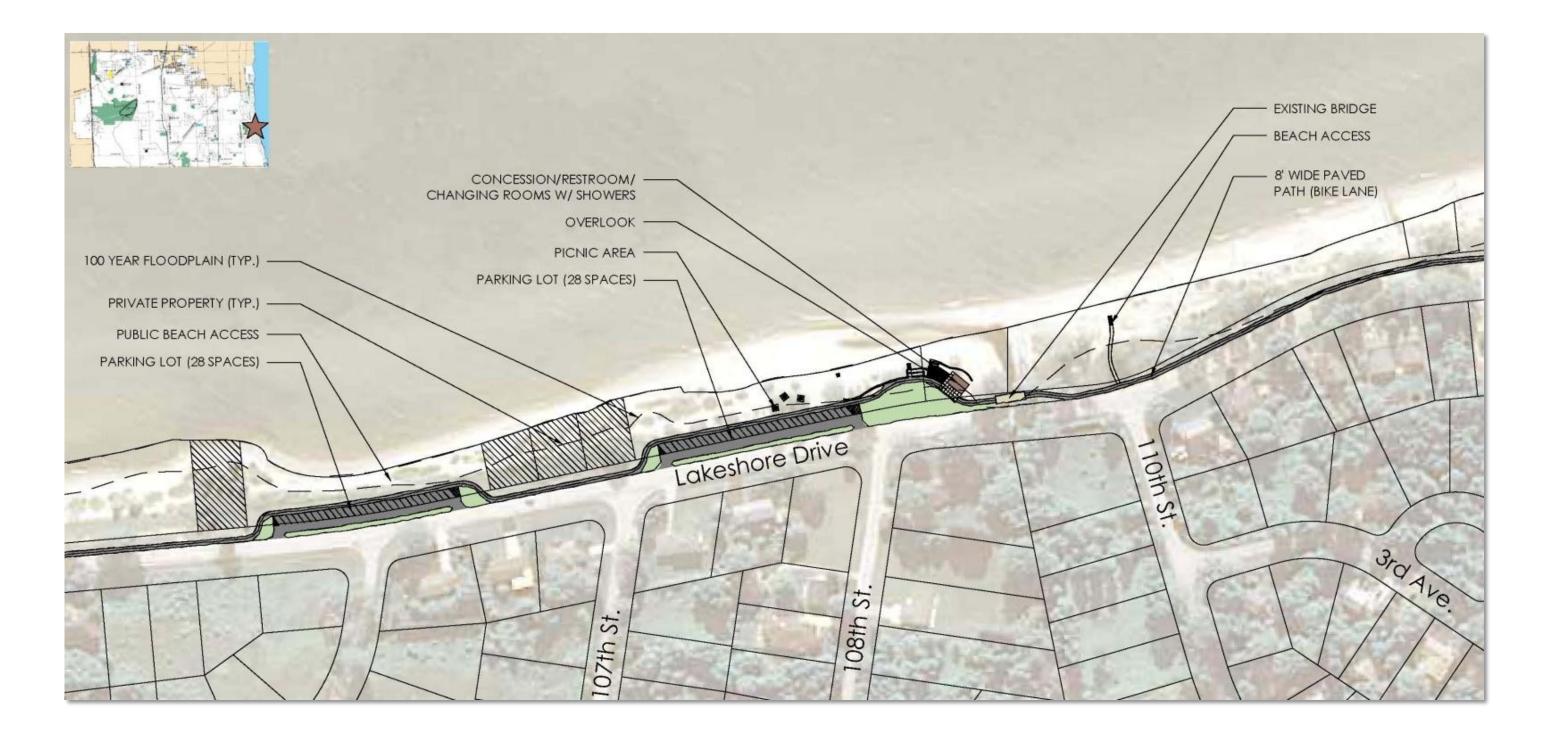








Figure 8.11 Conceptual Plan for Carol Beach Unit W Neighborhood Park



Figure 8.12 Conceptual Plan for Prairie Lane Neighborhood School/Community Park

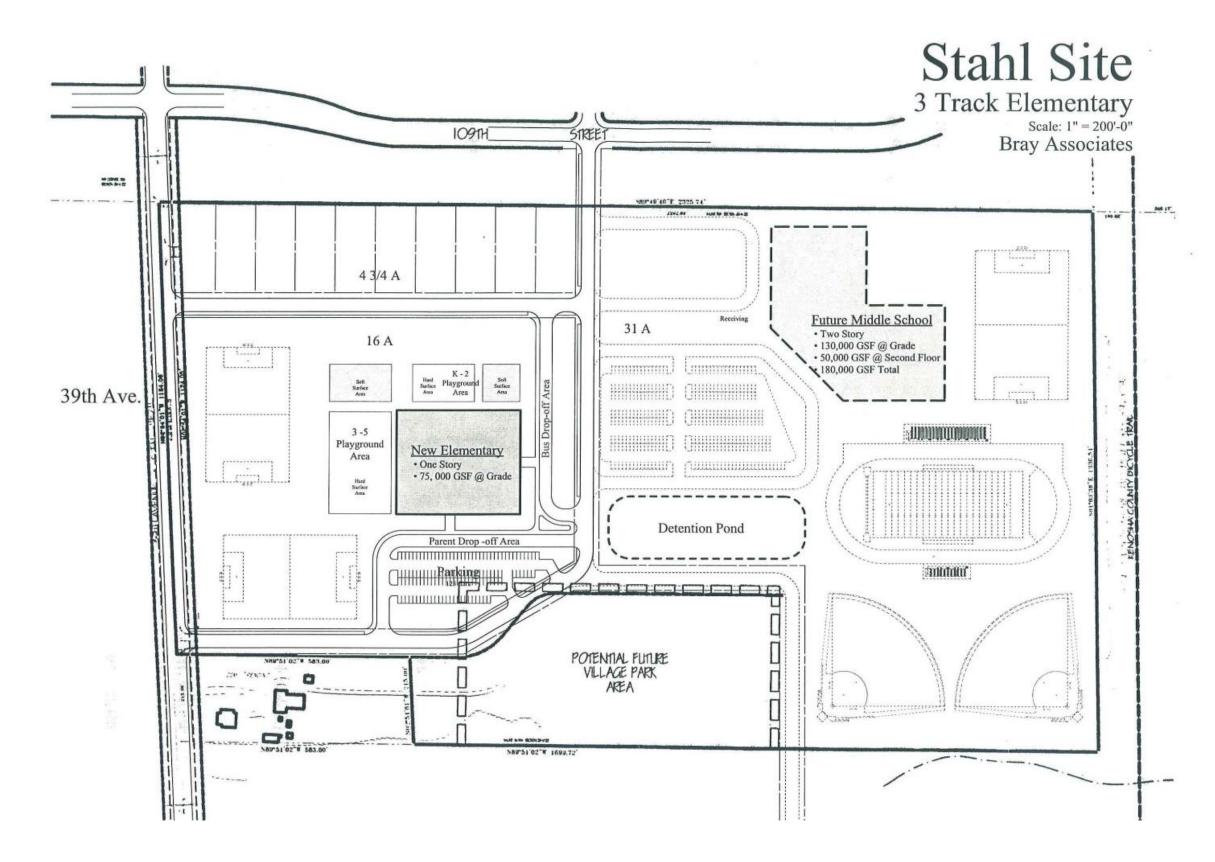


Figure 8.13 Conceptual Plan for Highpoint Neighborhood School/Community Park

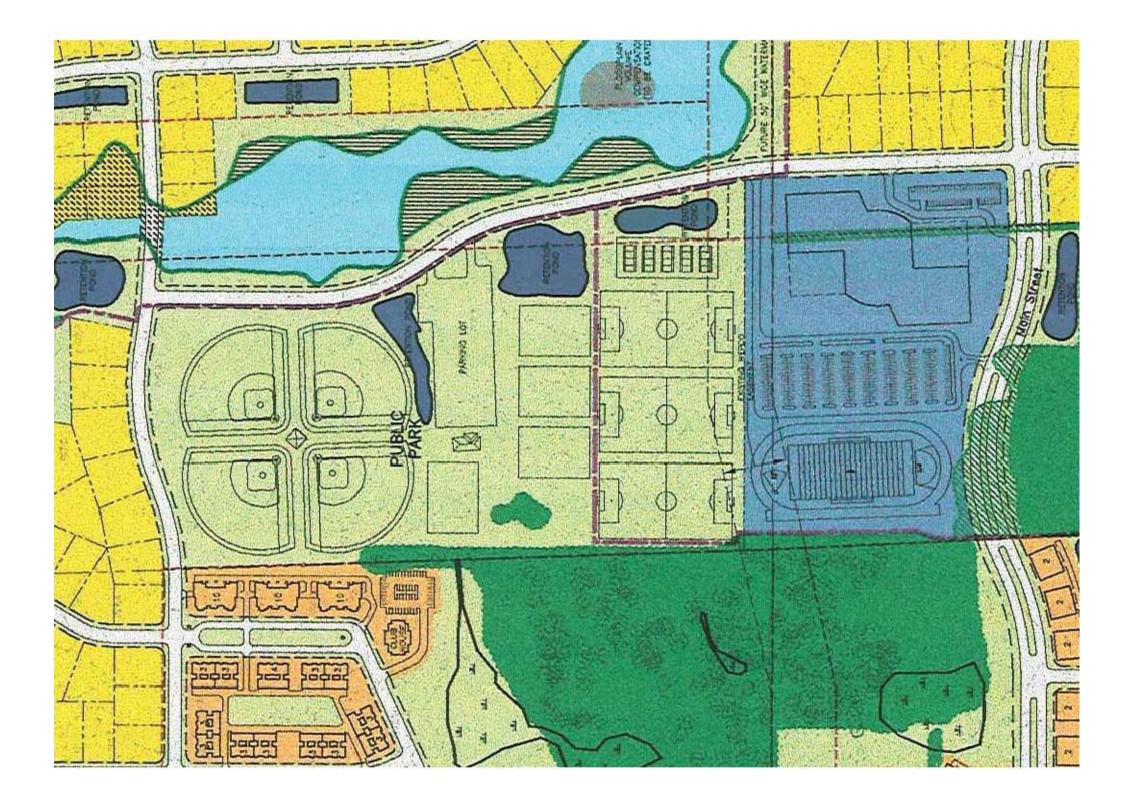
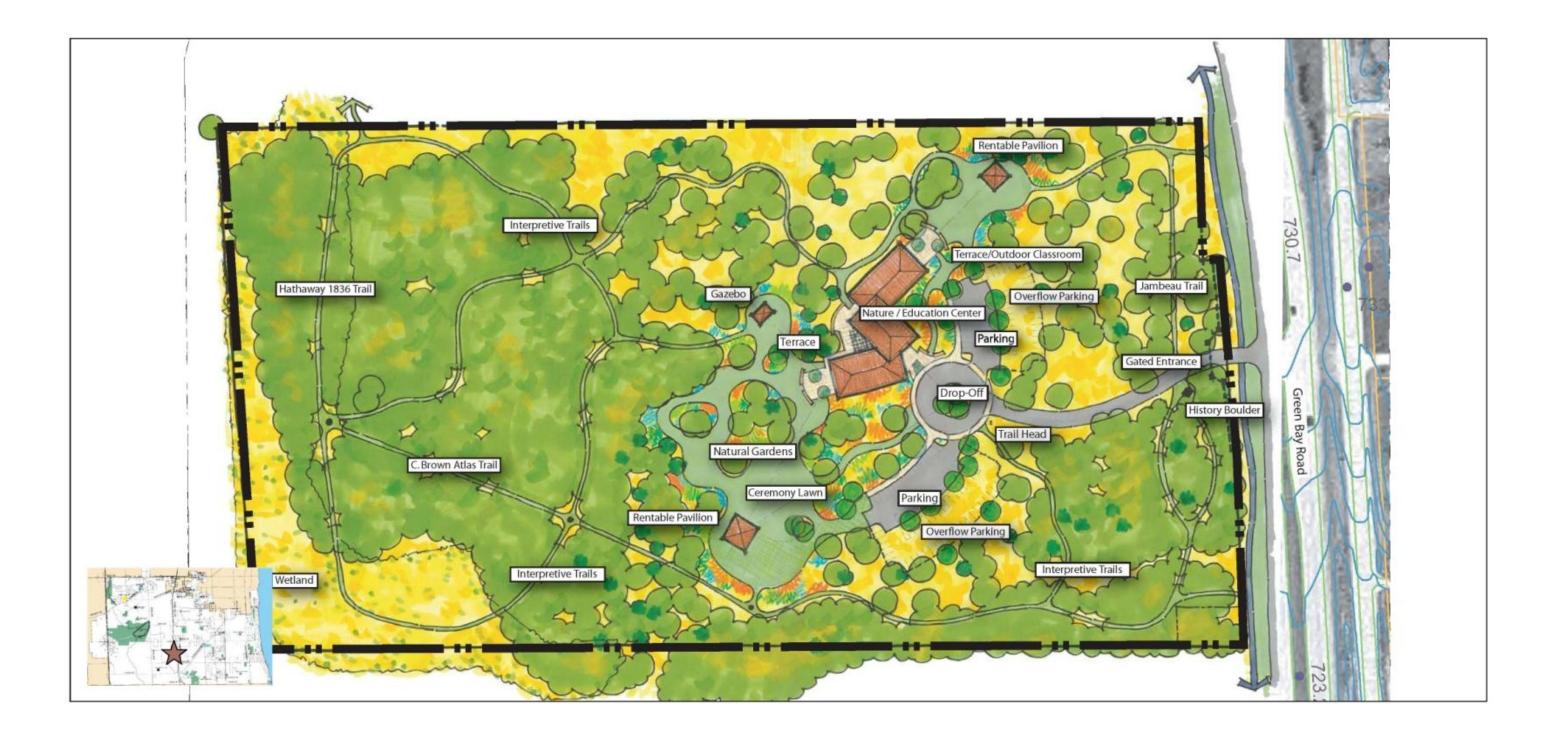


Figure 8.14 Conceptual Plan for Prairie Farms Neighborhood High School/Regional Park



### Figure 8.15 Conceptual Plan for Mompers Woods



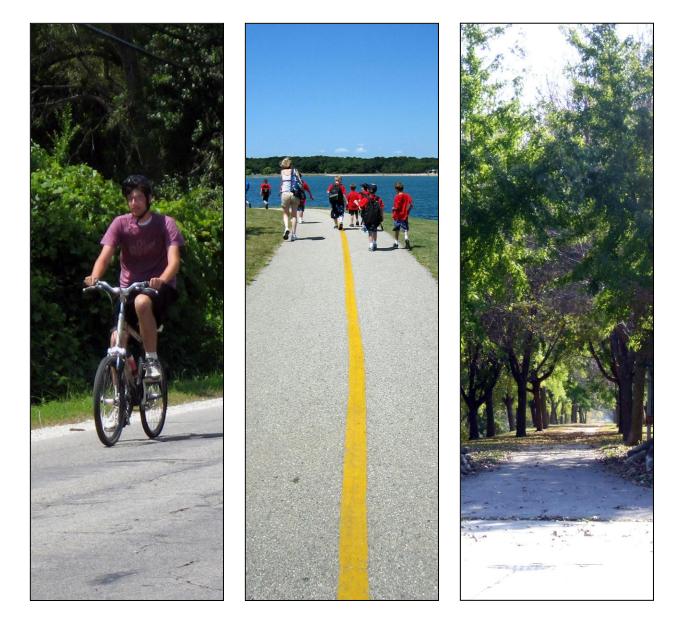
# **APPENDIX E**

# Village of Pleasant Prairie

# 2010 Bicycle and Pedestrian Trails Plan

# Village of Pleasant Prairie 2010 Bicycle and Pedestrian Trails Plan

Adopted by Ord. #10-27 on April 19, 2010



# Acknowledgements

#### Village of Pleasant Prairie Village Board

John Steinbrink, Village President Monica M. Yuhas, Trustee #1 Steve Kumorkiewicz, Trustee #2 Clyde R. Allen, Trustee #3 Michael Serpe, Trustee #4

#### **Project Manager**

John Steinbrink, Jr., Director of Public Works

#### **Steering Committee**

John Steinbrink, Jr., Village of Pleasant Prairie Director of Public Works Jean Werbie-Harris, Village of Pleasant Prairie Community Development Director Mike Spence, Village Engineer of Pleasant Prairie Brian Wagner, Village of Pleasant Prairie Chief of Police Glenn Christiansen, Village of Pleasant Prairie Parks Commission Troy Holm, Village of Pleasant Prairie Parks Commission Gregory Scheppler, Village of Pleasant Prairie Recreation Commission John Roscioli, Village of Pleasant Prairie Recreation Commission Patrick Finnemore, Kenosha Unified School District #1 Kathy Van Duyn, RecPlex Fitness Instructor

#### Prepared by



The Bicycle Federation of Wisconsin is a statewide nonprofit bicycle education and advocacy organization. The Bicycle Federation of Wisconsin's mission is to make Wisconsin a better place to bicycle. Bicycling is a viable, healthy, and environmentally sustainable means of transportation, recreation, and sport. The Bicycle Federation of Wisconsin provides bicyclists of all ages with information on recreational rides, safety tips, and commuting skills while educating decision makers about the importance of bicycling to our communities. Learn more at <a href="http://www.bfw.org">http://www.bfw.org</a>.

# **Table of Contents**

Executive Summary	iii
1. Introduction	1
1.1 Purpose	2
1.2 Scope	3
1.3 Summary of Public Input	4
2. The Importance of Bicycling and Walking	6
2.1 Social and Health Benefits	6
2.2 Environmental and Transportation Benefits	7
2.3 Economic Opportunities	8
3. Existing Conditions	9
3.1 Regional Context	9
3.2 Population and Demographics	10
3.3 Land Use	11
3.4 Parks and Open Space	12
3.5 Bicycle and Pedestrian Facilities	14
3.6 Existing Plans and Policies	15
4. Education, Encouragement, and Enforcement	18
4.1 Education	18
4.2 Encouragement	20
4.3 Enforcement	
5. Goals, Objectives and Policies	23
5.1 Goals	23
5.2 Objectives	23
5.3 Policies	24
6. Design Standards	26
6.1 Federal and State Design Guidance	26
6.2 On-Street Facilities	27
6.3 Off-Street Facilities	41
6.4 Support Facilities	44
7. Recommended Bike and Pedestrian Network	48
7.1 Proposed Network	48
7.2 Facility Recommendations and Conceptual Costs	49
7.3 Funding Opportunities	
7.3 Funding Opportunities	53
8. Conclusion	57
Appendix A: Public Meeting	58
Appendix B: Public Survey Results	59
Appendix C: Rules for Bicycling on the Road	70
Appendix D: Wisconsin State Bicycle Laws	73







## **Executive Summary**

Bicycling and walking are inexpensive, non-polluting forms of transportation and recreation that are accessible to nearly everyone. Children, many elderly and those without access to motor vehicles can rely on cycling and walking as an autonomous form of transportation. Other residents may rely on cycling for transportation due to its low cost, sustainability, or simply the joy of being on a bike. In addition to transportation, residents of all ages and abilities commonly participate in cycling, walking and running activities as a form of recreation and exercise.

For cycling and walking to be viable forms of transportation and recreation, there must be facilities that allow residents to safely and comfortably participate in these activities and reach their destinations. The Village of Pleasant Prairie recognizes the importance of providing these options for its citizens. Focused on bicycling and walking, the 2010 Bicycle and Pedestrian Trails Plan demonstrates the Village's commitment to providing facilities for walking and cycling. Based on current development as well as future development outlined in the 2035 Village of Pleasant Prairie Comprehensive Plan, this plan proposes a ten-fold increase in facilities designated for bicycle and pedestrian use. The plan also details encouragement, education, enforcement and evaluation programs and recommendations to boost bicycle ridership and walking within the Village.

The primary goal of this plan is to:

Establish and maintain a safe, comprehensive and integrated bicycle and pedestrian trail network that encourages the use of bicycles and walking for commuting, recreation and other trips.

A series of eight specific objectives and 27 policies are recommended in Chapter 6 to achieve this goal. Primary among these recommendations is the development of a robust and well-connected system of on-street bicycle lanes and off-street shareduse paths that tie together current and future Village neighborhoods, recreation areas, commercial areas and employment centers. The development of this network will allow residents to quickly and safely travel around the Village on bike or foot, no matter what their destination or the reason for their trip.

By proactively planning for bicycle and pedestrian facilities and programs and policies that encourage cycling and walking, Pleasant Prairie is demonstrating a commitment to offering Village residents and visitors accessible, environmentally friendly and inexpensive transportation and recreation options.



## 1. Introduction

Communities across the nation are recognizing that facilities for bicycling and walking are an important part of their infrastructure. These facilities, including shared-use paths, sidewalks and on-street bicycle lanes, offer transportation choices that are available to all ages and socioeconomic groups. Bicycle and pedestrian networks increase travel and recreational opportunities for residents. At the same time, a robust bicycle and pedestrian network can boost the local economy. Urban bicycle greenways have a positive impact on home values. Studies have shown that people walking or cycling to local businesses spend more than those who drive. Bicycle tourism can also draw increased traffic to local businesses.

Bicycling and walking are important modes of transportation. Bicycling in particular is a convenient and efficient form of transportation, and for some people, bicycling is their main mode of transportation. Like the automobile, but unlike public transit, a bicycle provides its user with autonomy and flexibility regarding travel schedules and destinations. Bicycling is also a form of transportation that is accessible to many people who cannot drive: the young, some elderly and those who do not to own a motor vehicle. A complete transportation system serves motorists, cyclists and pedestrians in a safe and efficient manner.



### 1.1 Purpose

Prior to incorporation as a village, Pleasant Prairie developed in a way that was not conducive to bicycling or walking: development was dispersed throughout the area and was linked by narrow, sometimes busy streets with no sidewalks or paved shoulders. Since incorporation the Village has grown rapidly. Residents and staff have worked to ensure that this growth and development better serves all forms of transportation while also being environmentally sensitive. This *Bicycle and Pedestrian Trails Plan* will complement that development by designating a system of bicycle and pedestrian facilities that connect all parts of the Village as well as surrounding communities. This plan will guide the development of shared-use paths, demarcate on-street facilities, provide design guidelines and policies for facilities and highlight funding opportunities for the Village to pursue.

The Plan will build on past bicycle and pedestrian planning, including that in the Village's 2006 Park and Open Space Plan and the Southeast Wisconsin Regional Planning Commission's (SEWRPC) Regional Transportation Plan for Southeastern Wisconsin: 2035, A Regional Land Use Plan for Southeastern Wisconsin: 2035, and Amendment to the Regional Bicycle and Pedestrian System Plan for Southeastern Wisconsin: 2020. The Plan should be updated every five years to reflect the needs of the community and progress that has been made in implementing new facilities.



### 1.2 Scope

*The Village of Pleasant Prairie Bicycle and Pedestrian Trails Plan* focuses on bicycling for transportation. For bicycle transportation, trip origins, destinations and trip purpose are of utmost importance (e.g. commuting to work or school, shopping, attending a social event, etc.), and the bicycle is simply the means to the end. Conversely, recreational bicycling trips are made expressly for the enjoyment of bicycling, and the destination, if there is one at all, is of minor importance. Most trips, and most facilities, serve both functions, but the bicycle facility network must be complete in order to serve the needs of transportation bicyclists while also serving recreational bicyclists and pedestrians.

Bicycle projects must be primarily for transportation purposes to be eligible for funding under most Federal aid programs, although the Recreational Trails Program is an exception. Federal guidelines generally consider any bicycle path other than a closed loop as being principally for transportation and eligible for federal funding.

The *Plan* is a comprehensive approach to bicycle transportation planning that encompasses the "four E's:"

- Engineering: facility creation and improvement
- Education: increasing cyclist and motorist awareness of the rules of the road
- Encouragement: programs to increase cycling
- Enforcement: applying the rules of the road to all users: motorists, bicyclists, and pedestrians

When combined with facility improvements, education and encouragement can dramatically increase the levels of bicycling and walking. Educating bicyclists and motorists on how to share the road safely is important. Education of elected officials, planners, engineers and others involved in land use development will help ensure that bicyclists' needs are included when planning new neighborhoods and roadways. Educating law enforcement officers about the illegal behaviors that are the most common factors in crashes can allow them to better target these behaviors.

The plan identifies existing facilities and recommends new programs, policies and facility projects including off-street paths, on-street facilities, signed routes and support facilities, such as bicycle parking. Implementation of the plan will encourage the use of this practical, non-polluting, affordable mode of transportation. The bicycle and pedestrian facility recommendations are necessary for safety, mobility and access to destinations such as schools, employment centers, commercial areas, public and institutional land uses and recreational areas. Recommendations are prioritized to augment the existing bicycle and pedestrian transportation network in the Village and its connections to other municipalities.



## **1.3 Summary of Public Input**

An effective planning effort requires the participation of the public. Public input during the planning process was solicited via a survey administered online by the Village of Pleasant Prairie and through a public information meeting. Both the survey and the meeting provided the public with opportunities to voice their concerns over bicycle and pedestrian issues as well as offer opinions on the direction of the planning effort and what they would like to see as the end product.

#### **Public Informational Meeting**

An informational meeting for the public was held on Thursday, May 28, 2009, from 5:00pm – 7:00pm at the IcePlex in Pleasant Prairie. Notice of the meeting was provided through the Village newsletter that was sent to each Pleasant Prairie household, as well as on the Pleasant Prairie official webpage. A total of 34 people attended the meeting including members of the Bicycle and Pedestrian Trails Plan Steering Committee and the general public. After a brief introduction to the planning process and a description of a map of proposed bicycle and pedestrian trails drawn from the *2006 Park and Open Space Plan*, the meeting was opened up for public comment. Comments were wide-ranging, but specific themes emerged repeatedly:

- **Connections:** The plan should identify a network of on- and off-street routes that connect throughout Pleasant Prairie as well as into the City of Kenosha, the State of Illinois and other recreational areas.
- **Signage:** Provide a signage system that clearly indicates where users are within the trail system, how to get to popular destinations and times and distances to locations.
- **Secure parking:** Provide secure bicycle parking facilities at employment centers, retail destinations, parks and other locations.
- **Maintenance:** Ensure that the bicycle and pedestrian system, both on- and off-street, is well maintained so as to be safe and efficient for all users.
- **Funding:** The plan should identify funding sources for bicycle and pedestrian trails and maintenance.
- **Trail amenities:** Provide off-street bicycle and pedestrian path and trail amenities including trash and recycling receptacles, drinking fountains, restrooms, picnic tables, signage and lighting.
- **Multimodal options:** The plan should identify and tap into opportunities to expand multimodal travel options, particularly rail and bus.

Attendees at the public meeting as well as written comments that were received at the meeting are listed in Appendix A.





Pleasant Prairie hosts numerous triathlons throughout the year drawing thousands of athletes to the Village.

#### **Bicycle and Pedestrian Trails Plan Steering Committee**

A Steering Committee was established at the beginning of the planning process to oversee development of the plan and to provide input from different areas of the community. The Steering Committee included representatives of the Parks Commission, the Kenosha Unified School District, the Recreation Commission, the Village Public Works, Engineering, Police, and Community Development Departments, and local recreation facilities. The Steering Committee met monthly during the planning process to review progress and provide input on the plan.

#### **Survey of Public Opinion**

A public survey was administered online to Village residents and others interested in bicycling and walking in the Village. Notice of the survey was placed on the Village website as well as in the Village newsletter that was mailed to each household in the Village. A total of 82 people participated in the survey, with the majority of those people residing within the Village. Results of the survey helped guide the planning process and the full survey results are presented in Appendix B.

## 2. The Importance of Bicycling and Walking

Bicycling and walking are both important forms of transportation and recreation that provide numerous benefits to individuals and communities as a whole. Bicycling and walking are good forms of exercise and are nonpolluting forms of transportation that are accessible to all. By encouraging bicycling and walking participation, particularly for transportation, Pleasant Prairie can provide social, health and environmental benefits to its residents.

### 2.1 Social and Health Benefits

Bicycling offers low cost mobility. For those who do not use or have access to an automobile, such as school-age children, bicycling and walking are particularly important. While bicycling and walking may not replace all trips by motor vehicle, they can be a practical mode for many trips and part of multi-modal trips as well (such as a trip to a park-and-ride carpool facility or transit stop).

Increased bicycling levels along with increased quality and quantity of bicycle facilities can benefit the community by providing those unable to drive or without access to a car with more independence; reducing the need for parents to chauffeur their children to school, social and recreational activities; allowing households to meet their transportation needs with fewer cars; and increasing recreational opportunities and, by extension, improving public health.

A total of 76% of respondents to the survey conducted for this plan reported that they regularly walk or jog for exercise while 65% reported bicycling regularly for exercise. Clearly walking and bicycling are important forms of exercise for local residents. Improving bicycle facilities for transportation purposes benefits those who bicycle for recreation and fitness as well. Recreational bicycle rides can begin at home and be combined with other, often utilitarian, trip purposes. When linked with a larger bikeway system, off-street paths can provide important transportation linkages, and a complete bikeway network benefits everyone, regardless of how they use the road.



A sign near the RecPlex reminds motorists of pedestrian rights.

## 2.2 Environmental and Transportation Benefits

Bicycling is a convenient and efficient form of transportation, and for some, their primary mode of transportation. Bicycling is a popular mode of transportation because a bicycle provides its user with autonomy and flexibility regarding travel schedules and destinations, including multiple destinations. Bicycling is the most energy efficient form of transportation and is often faster than driving for short trips. Bicycling is an important element of a transportation system. Multi-modal trips allow commuters to use their bicycles to reach a bus stop or to ride to their final destination from a convenient parking area. Bicycling levels are much higher during the warmer months, and the development of inexpensive, more versatile bicycles and clothing have increased both the appeal and the practice of bicycling in wetter and colder weather.

While travel within southeastern Wisconsin is predominantly by personal motor vehicle, walking and bicycle travel represent the next largest percentage of internal weekday travel by resident households of the region.

Although the Kenosha Transit bus system provides limited service to the LakeView Corporate Park and small portions of the north side of Pleasant Prairie, most of the residential areas in Pleasant Prairie are not served by transit. However, for the areas that are served, Kenosha Transit's buses have bicycle racks on them which encourage multimodal trips by users.

Increasing bicycle opportunities improves the efficiency of the transportation system and increases environmental benefits. It improves neighborhood livability by reducing motor vehicle traffic and its associated pollution and congestion, reducing the need for motor vehicle parking and reducing motor vehicle crashes, injuries and property damage.

When compared to a motor vehicle, bicyclists take up very little roadway space. In most urban traffic conditions, bicyclists do not significantly limit traffic flow. Providing adequate roadway width for all users, including bicyclists, will increase roadway capacity, reduce congestion and decrease trip times for everyone.



Kenosha Transit buses accommodate up to two bikes on front racks. *Credit: Kenosha Transit* 

### **2.3 Economic Opportunities**

Improving the bicycling environment can provide non-transportation related benefits as well. The community benefits from bicycle riders who purchase food and other needs locally. The tourism industry benefits as more bicyclists are attracted from outside the community. Most importantly, the quality of life of the community is enhanced by the presence of bicyclists and pedestrians when social interactions occur spontaneously and when people feel safer being outdoors.

Bicycle facilities have been shown to have a positive effect on nearby property values<sup>1</sup> and an increase in business reported by owners of businesses near bicycle facilities.<sup>2</sup> A study by North Carolina's Department of Transportation of bicycle facilities in the Outer Banks reveals an annual economic impact from the facilities that is six times greater than the one-time capital costs.<sup>3</sup> A Wisconsin study showed 39% of responding businesses indicated increased business as a result of users of the Fox River Trail. The same study showed that a bicycle facility had positive effects on real estate values and therefore property tax revenues. Lots adjacent to the Mountain Bay Trail in Brown County, Wisconsin, sold faster and for an average of 9% more than similar property not located next to the trail. The study also suggests that by providing workers an alternative to driving to work, the trail became an inexpensive alternative to increasing road capacity.<sup>4</sup> The conclusion that trail facilities generate increased revenue through higher property values is corroborated by the Consumer's Survey on Smart Choices for Home Buyers. In that survey, trails ranked the second most important amenity out of a list of 18 choices.<sup>5</sup>



Buffered bicycle lanes add additional separation between travel lanes and the bicycle lane which increases cyclists' comfort level.

<sup>&</sup>lt;sup>1</sup> National Association of Realtors and National Association of Builders, *Consumer's Survey on Smart Choices for Home Buyers*, April 2002.

<sup>&</sup>lt;sup>2</sup> Runge, Cole. Fox River Trail Study, Prepared for the Brown County Planning Commission, December 2002.

<sup>&</sup>lt;sup>3</sup> Lawrie, Judson, John Guenther, Thomas Cook, and Mary Paul Meletiou. *The Economic Impact of Investments in Bicycle Facilities: A Case Study of the North Carolina Outer Banks*, summary report, April 2004.

<sup>&</sup>lt;sup>4</sup> Runge, Cole. *Fox River Trail Study*, Prepared for the Brown County (WI) Planning Commission, December 2001.

<sup>&</sup>lt;sup>5</sup> National Association of Realtors and National Association of Home Builders, *Consumer's Survey on Smart Choices for Home Buyers*, April 2002

## 3. Existing Conditions

This chapter focuses on the existing conditions in the Village of Pleasant Prairie. The chapter provides an overview of the region, demographic information, the parks system, bicycle and pedestrian facilities and plans and policies currently in effect that impact bicycle and pedestrian planning and facilities. It should be noted that much of the demographic information is drawn from the 2000 Census, and the 2010 Census will provide a more accurate picture of current conditions in Pleasant Prairie when the data is released in 2011.

## 3.1 Regional Context

The Village of Pleasant Prairie is a rapidly growing community in Kenosha County in the far southeastern corner of Wisconsin. The Village is bordered by Illinois on the south, Lake Michigan on the east, the unincorporated Town of Bristol on the west and the City of Kenosha on the north. The Village is approximately 40 miles south of Milwaukee and 70 miles north of downtown Chicago.

The Village was incorporated in 1989 from the former Town of Pleasant Prairie. Because it incorporated from a Town with no central core, development is largely dispersed throughout the Village. Since incorporation Pleasant Prairie has grown rapidly: the population increased 63% while the number of housing units in the Village grew by 76%. Employment within the Village has increased rapidly as well, particularly with the industrial development of LakeView Corporate Park and commercial sites along 75<sup>th</sup> Street (State Highway 50) and near the I-94 corridor.

This rapid growth created a need for better bicycle and pedestrian planning and facilities. Many streets that were relatively quiet and safe for bicycling or walking 20 years ago are now busy and uncomfortable for cyclists and pedestrians. Additionally, while bicycle and pedestrian use may be safe and easy within many of the new office parks, industrial sites and residential developments, there are few good links or connections between these developments. Adequate links to popular destinations including Lake Michigan, schools, the RecPlex and IcePlex, and employment centers are also lacking.



It is important to provide bicycle parking at destinations throughout the Village.

## **3.2 Population and Demographics**

Demographic characteristics help determine the amount of bicycle and pedestrian trails that the Village now requires and will need in the future. As part of the transportation network, it is important that trail facilities link not only current housing, employment, and recreation centers, but future ones as well.

Since incorporation, the Village of Pleasant Prairie has experienced a significant population increase. From 1990 to 2009, the population increased from 12,037 to 19,570. This increase of 63% is a far greater growth rate than Kenosha County (27%) and the State (16%). Additionally, the Village's work-day population is significantly higher than this number due to the large number of people who commute to the Village for employment or other opportunities. Pleasant Prairie's population is projected to continue growing rapidly over the next 20 years, although at a slightly lower rate than it has over the last 20 years. The Village's projected population in 2030 is 28,911, a 48% increase over the 2009 estimated population. This growth rate is again significantly higher than the rate for Kenosha County and the State.

The average age of residents of Pleasant Prairie slightly higher than their counterparts in Kenosha County or Wisconsin as a whole: in 2000, the residents of Pleasant Prairie had a median age of 37.0 years, while Kenosha County had a median of 34.8 years and the State of Wisconsin had a median of 36.0 years. In 2000, 34% of the Village's population was under the age of 25, while 11% of the population was 65 or older.

In 2000, the average household size in Pleasant Prairie was 2.73 persons per household, a decrease since the 1990 Census count of 2.83 persons. This average household size was slightly higher than that of the County and State.

Although the size of the average household is decreasing, the statistical trends indicate that Pleasant Prairie has a growing population with many families. Providing a bicycle and pedestrian network that is comfortable and easy to use by users of all ages and skill levels will increase the quality of life of Village residents.



### 3.3 Land Use

Current and future land use is important for bicycle and pedestrian planning as the location of housing, employment centers, retail areas and recreational areas dictates where people will begin and end many of their trips. A well connected bicycle and pedestrian network should connect all of these areas so that residents can safely and conveniently travel from one location to another.

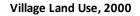
#### **Current and Future Land Use**

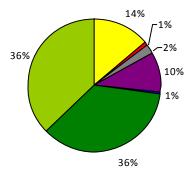
As mentioned in section 3.1, Pleasant Prairie incorporated in 1989. With this step, the new village gained the authority to zone land for specific uses including residential, industrial, commercial and other uses. This authority allows the Village to steer its growth and development in a desirable manner. The *Village of Pleasant Prairie, Wisconsin 2035 Comprehensive Plan* provides a detailed description of existing land uses in the Village, as well as the planned growth over the next 25 years.

Reflecting its recent incorporation, the majority of Pleasant Prairie is comprised of agricultural land, natural open space, and natural resource areas such as woodlands, wetlands, and surface water. These land uses cover approximately 76% of Village land. The remaining land is more developed and is comprised primarily of residential land uses, although commercial, industrial, governmental, transportation, and recreational land uses also cover significant areas of the Village. The *2035 Comprehensive Plan* envisions significant growth in residential, mixed-use, and commercial areas, with additional growth in industrial, governmental and transportation land uses. This growth will occur primarily on land that is currently agricultural and in non-environmentally sensitive open space areas.

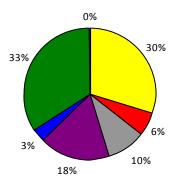
#### Land Use Impacts on Walking and Cycling

Development in Pleasant Prairie is relatively dispersed today, although much of it is clustered at the northern end of the Village, close to Kenosha. Because development is dispersed, it is often difficult for residents to rely on walking or cycling as transportation, as destinations may be too far away or they may not feel safe using the existing streets for walking or cycling. As the Village continues to develop, it should become easier for residents to walk or cycle to destinations as urban street sections with bicycle facilities, sidewalks, and multi-use paths are installed in areas not currently developed. These connections will make it easier for residents to reach destinations and will allow residents to more reliably use walking and cycling as transportation and recreation.





Village Land Use, 2035





Commercial & Mixed Use



- Transportation, Communication & Utilities
- Governmental & Institutional
- Recreational, Natural Resource & Open Space Areas
- Agricultural

## 3.4 Parks and Open Space

The 2006 Village of Pleasant Prairie Park and Open Space Plan inventoried existing parks, open space and recreation facilities within the Village. These spaces offer residents the opportunity to participate in organized recreational activities, exercise in areas away from motor vehicles or simply enjoy nature. Residents should be able to safely and easily access these spaces on bicycle or foot.

#### **Regional Park**

Three large community parks exist in Pleasant Prairie. The largest of these parks is Prairie Springs Park. This 930-acre park is located in the western portion of the Village, north of 104th Street and west of 88th Avenue. The park offers numerous sporting and recreation activities in addition to playgrounds, softball fields, volleyball pits, and picnic areas. The park also contains some of the Village's only paved bicycle and pedestrian trails. Although Prairie Springs Park contains pedestrian and bicycle trails and other recreational opportunities, many residents noted that it is difficult to access on foot or bicycle.

#### **Community Parks**

Anderson Park is a large existing park just north of the Village in the City of Kenosha. The 95-acre park offers City and Village residents amenities including two baseball diamonds, 20 soccer fields, a swimming pool with water slides and picnicking and other opportunities. Village Green Park and Ingram Park are planned for future development in the Village. All of these parks provide or will provide numerous benefits and amenities to Pleasant Prairie residents and should be readily accessible by bicycle or on foot.

#### **Neighborhood and School Parks**

The Park and Open Space Plan identified six existing neighborhood parks distributed throughout the Village: Carol Beach Park, Becker Park, Woodlawn Park, Pleasant Prairie Park, Rolling Meadows Park and Lake Michigan Park. These parks range in size from 0.14 acres to 22 acres and offer residents recreational opportunities and open space near their homes. Park space also exists at three Kenosha Unified School District elementary school sites in the Village, with two additional sites planned for future schools. These smaller park spaces provide recreation and open space that is dispersed throughout Pleasant Prairie.



Prairie Springs Park is a large regional park located in the western portion of the Village. The park contains a popular shareduse path around Lake Andrea.



Carol Beach Park is one of six neighborhood parks dispersed throughout the Village.



The Chiwaukee Prairie Nature Preserve is a popular destination for cyclists.

#### **Open Space and Natural Areas**

Numerous other open spaces and recreation areas exist throughout the Village. These areas include recreation areas such as two private driving ranges and a golf course, a private marina and a significant amount of preserved open space and natural areas.

The open spaces and natural areas throughout the Village are very popular with local residents as well as visitors to the area. These lands preserve critical plant and animal habitats and environmental corridors. They also allow residents and others to experience outdoor environments that are more natural and less controlled than what they experience in formal park space. Bicycling and walking can be an ideal match for these areas as they are quite, nonpolluting forms of transportation and recreation that require minimal impact in the way of trails and other facilities.

The largest of these areas is the Chiwaukee Prairie State Natural Area which is home to one of the largest prairie complexes in the State and the largest coastal wetland in southeastern Wisconsin. The southern portion of the area is managed by The Nature Conservancy, and the northern parcels are managed by the Wisconsin Department of Natural Resources, including the Kenosha Dunes, which contains open and stabilized sand dunes. Chiwaukee Prairie is recognized as a National Natural Landmark and a State Natural Area.

Other large open spaces and natural areas include the Carol Beach Natural Area Open Space, the Des Plaines River Natural Area Open Space and Momper's Woods. These areas cover over 250 acres throughout the Village and combine with approximately 200 acres of privately held land to form a significant natural presence within the Village. These areas are popular destinations for cyclists and offer opportunities for hiking and other recreational activities.



The Des Plaines River Open Space includes over 750 acres of preserved environmentally sensitive land.

## **3.5 Bicycle and Pedestrian Facilities**

Currently, the only major bicycle trail in Pleasant Prairie is the Kenosha County Bike Trail. This north-south trail runs from the Illinois border through the Village and the City of Kenosha and north to the Kenosha/Racine County line. The trail is built in a former railway corridor and lies at approximately 30<sup>th</sup> Avenue. The gravel trail is unpaved and is not maintained during the winter months. Although a popular facility, residents voiced concerns over maintenance issues with the trail and problems with crushed gravel from the trail spreading onto connecting roadways and creating hazards.

Prairie Springs Park contains an eight foot wide paved trail that forms a loop around Lake Andrea and other portions of the park. While this trail is a popular recreation facility, it is not connected with any larger network of bicycle or pedestrian facilities and is not useful as a transportation facility.

There are approximately five miles of bike lanes in Pleasant Prairie, primarily in the far northern section of the Village. Residential streets in Pleasant Prairie with low traffic volumes are safe and recommended for bicycling with no additional facilities. Arterial streets that have higher traffic volumes and speeds are in need of better facilities which may include bike lanes, wide curb lanes or paved shoulders to accommodate bicycles. In these cases, critical corridors connecting neighborhoods and destinations are recommended for either bicycle routes or lanes. Some arterial streets are wide enough to accommodate bike lanes without any additional pavement. In areas where there is not enough roadway width to add a full bike lane, bicycle routes may be recommended. Bicycle parking at key destinations is also needed.

Most streets in Pleasant Prairie developed prior to 1989 have a rural cross-section with no curb and gutter or sidewalk. On low-traffic minor residential streets the roadway may be able to safely serve as a pedestrian facility. However, pedestrian accommodations are needed on arterial and connector streets and intersections. A total of 44% of survey respondents reported that they do not feel Pleasant Prairie is pedestrian friendly because of personal safety concerns, and 61% thought the lack of sidewalks made Pleasant Prairie unfriendly for pedestrians. At the same time, 62% of respondents cited automobile traffic as a reason for Pleasant Prairie's pedestrian unfriendliness, a reason that is closely linked with the lack of sidewalks.

A map at the back of this plan depicts the existing as well as proposed bicycle and pedestrian facilities.



Many residential streets with very low traffic volumes are safe and recommended for bicycling in their current condition.

## **3.6 Existing Plans and Policies**

Numerous plans exist at the federal, state, regional and local levels that impact bicycling and walking in Pleasant Prairie. This section provides a brief summary of the most relevant of those plans. Additionally, Section 6.1 details federal and state guidance on the development of bicycle and pedestrian facilities.

#### **Federal Plans and Policies**

Congress firmly established the principle that the safe accommodation of bicycling and walking is the responsibility of state and local transportation agencies. This responsibility extends to the planning, design, operation, maintenance and management of the transportation system in federal transportation law, including the *Intermodal Surface Transportation Efficiency Act (ISTEA)*, and its reauthorizations, the *Transportation Equity Act for the 21st Century (TEA-21)* and the *Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU).* 

http://www.americabikes.org/resources\_policy\_biyclefriendly.asp

The Federal Highway Administration Program guidance on the federal transportation bills states that "In the planning, design, and operation of transportation facilities bicyclists and pedestrians should be included as a matter of routine and the decision not to accommodate them should be the exception rather than the rule. There must be exceptional circumstances for denying bicycle and pedestrian access either by prohibition or by designing highways that are incompatible with safe, convenient walking and bicycling." http://www.fhwa.dot.gov/environment/bikeped/Design.htm

#### **Wisconsin Plans and Policies**

The Wisconsin Bicycle Transportation Plan 2020 (WisDOT September 1998) is intended "to establish bicycling as a viable, convenient, and safe transportation choice throughout Wisconsin." The role of the state plan is "ensuring an interconnected transportation system across government boundaries and highway jurisdictions that can work safely for bicyclists...." The two primary goals of the state plan are doubling the number of bicycle trips by 2010 and reducing crashes involving bicyclists and motor vehicles by 10% or more by 2010. http://www.dot.state.wi.us/projects/state/bike2020.htm

The *Wisconsin Pedestrian Policy Plan 2020* outlines statewide and local measures to promote pedestrian comfort and safety. The plan lays out a policy framework to better integrate pedestrians into the transportation network by 2020 and describes WisDOT's role in meeting pedestrian needs. http://www.dot.wisconsin.gov/projects/state/ped2020.htm

#### **Regional Plans and Policies**

The *Regional [I-94] Freeway System Reconstruction Plan for SE WI* (SEWRPC) includes plans to reconstruct the I-94 interchange at County Highway C in Pleasant Prairie. The interchange reconstruction includes a redesign of portions of East Frontage Road and West Frontage Road that will separate the frontage roads from the highway interchange ramps. This should be safer for bicycles and still accommodate motor vehicle traffic. A separate paved shared-use path and wide



shoulders on the West Frontage Road are included as part of this reconstruction. Construction was completed in November 2009. <u>http://www.sewrpc.org/freewaystudy</u>

The Regional Transportation System Plan for Southeastern Wisconsin: 2035 (SEWRPC Planning Report No. 49) includes SEWRPC's vision for transportation in the region: "A multimodal system with high quality public transit, bicycle and pedestrian, and arterial street and highway elements which add to the quality of life of Region residents and support and promote expansion of the Region's economy, by providing for convenient, efficient, and safe travel by each mode...." http://www.sewrpc.org/regionalplans/regionaltransysplan.shtm

The Amendment to the Regional Bicycle and Pedestrian Facilities System Plan for Southeastern Wisconsin: 2020 (SEWRPC) "seeks to remove existing impediments to bicycle travel related to the lack of bicycle paths, the lack of safe accommodation on streets and highways, and the lack of support facilities such as bicycle parking and storage lockers. The plan recommends that improvements such as extra-wide outside travel lanes or paved shoulders be considered to be provided whenever an arterial street or highway is constructed or reconstructed to better accommodate shared roadway use by bicycles and motor vehicles." http://www.sewrpc.org/transportation/amendmentbikeped.asp

SEWRPC has identified several roads in and around the Village for expansion and/or extension. These projects will have significant impacts on bicyclists' ability to move safely and conveniently around the region. If the goals of the state and SEWRPC's own plans regarding bicyclists are to be realized, it is imperative that the bicyclists be accommodated as an integral part of every project.

While SEWRPC has proposed many projects in and around Pleasant Prairie in the future, the following projects are specifically identified in the 2009 – 2012 Transportation Improvement Program (TIP):

- Project 510 (2009): Resurfacing of State Highway 50 (75<sup>th</sup> Street) from I-94 to State Highway 31
- Project 513 (2009): Reconstruct State Highway 165 (104<sup>th</sup> Street) intersections with County Highway ML and County Highway EZ
- Project 516 (2009): Install flashing lights and gates at the 113<sup>th</sup> Street atgrade crossing of the Canadian Pacific railroad tracks
- Project 523 (2010)Preliminary engineering for reconstruction with additional lanes of County Highway C from County Highway U to West Frontage Road

It is critical that these projects properly accommodate pedestrians and bicycles, particularly since many of these are high-volume roads that are often difficult for cyclists and pedestrians to negotiate.

http://www.sewrpc.org/publications/tip/TIP\_2009-2012.pdf





Local Eagle Scouts constructed boardwalks through environmentally sensitive areas to allow bicycle and pedestrian access.

#### **Local Policies and Plans**

The Village of Pleasant Prairie 2035 Comprehensive Plan will guide development in the Village for the next 25 years. The plan includes numerous chapters that will impact bicycling and pedestrian facilities in the Village, including the Transportation; Utilities and Community Facilities; Agricultural, Natural, and Cultural Resources; and Land Use chapters. The Comprehensive Plan is Pleasant Prairie's definitive guiding document regarding land use, transportation and other planning and the Transpiration chapter has the following goal:

Provide an accessible interconnected network of highways, streets, and bicycle and pedestrian trails within the Village in order to present a safe, affordable, and efficient transportation system that meets the needs of multiple users; and to assure the functionality of the transportation network within Pleasant Prairie and the surrounding region while minimizing the impacts upon the transportation system.

The Village of Pleasant Prairie 2006 Parks and Open Space Plan serves as the planning document guiding park and open space development in the Village. The plan includes a number of policies and recommendations regarding bicycle facilities. The top priority to emerge from public participation in the planning process was the development of a multi-use trail system. Accordingly, the plan includes a map of existing and future bicycle trails and facilities. Additionally, a number of objectives and policies in the plan recommend or support the development of a robust bicycle and pedestrian trail network. No additional bicycle or pedestrian trails have been developed since the adoption of the plan.

It is important for the Village of Pleasant Prairie to work with surrounding jurisdictions to ensure that connections between the various jurisdictions are created concurrently with any new development or redevelopment. Additionally, Pleasant Prairie should encourage the other municipalities to begin creating bicycle plans of their own to incorporate into their long range transportation and land use plans.



Appropriate signage can draw attention to locations where shared-use paths intersect local streets.

# 4. Education, Encouragement, and Enforcement

There is a common perception that bicycling on streets and crossing streets as a pedestrian is dangerous, when in fact it is not. This concern keeps people from bicycling and walking more or at all. *Education, encouragement,* and *enforcement* can all be used to effectively counter the perception that bicycling or walking is unsafe. When including *engineering* (facilities discussed in Chapter 6), the "four Es" are popular and effective components to achieving the goals of increasing the number of trips by foot or bicycle and improving the safety and convenience of the bicycling and walking environment.

## 4.1 Education

Educating motorists and bicyclists to share the road will establish safer, more inviting streets for bicycling. Educating motorists and pedestrians about proper yielding and crossing procedures will establish a more encouraging and safe environment for pedestrians. Bike Rodeos, Bike Ed and Safe Routes to School initiatives are three examples of established bicycle education programs.

### Safe Routes to School

The Federal Safe Routes to School (SRTS) program addresses the decline in children walking and bicycling to school. In 1969, about half of all students walked or biked to school. Today, fewer than 15% of all school trips are made by walking or bicycling, while over half of all children arrive at school in private automobiles. This decline in walking and bicycling has had an adverse effect on traffic congestion and air quality around schools as well as pedestrian and bicycle safety. In addition, a growing body of evidence has shown that children who lead sedentary lifestyles are at risk for a variety of health problems such as obesity, diabetes and cardiovascular disease. Safety issues are a concern for parents who cite traffic danger as a reason why their children are unable to bicycle or walk to school. The SRTS program empowers communities to make walking and bicycling to school a safe and routine activity once again. The program funds a wide variety of programs and projects, from building safer street crossings to establishing programs that encourage children and their parents to walk and bicycle safely to school. A Safe Routes to School toolkit is available from WisDOT at:

http://www.dot.wisconsin.gov/localgov/aid/saferoutes-toolkit.htm

### **Bicycle and Pedestrian Education Efforts**

Bike Rodeos can be effective tools for teaching children safe bicycling basics, but only when those running the rodeos know what the most common kinds of child bicycle crashes are and the skills needed to avoid them. Teaching Safe Bicycling is a free course coordinated by WisDOT's Bureau of Transportation Safety that provides instructors with this critical knowledge. For more information contact the WisDOT Bicycle and Pedestrian Safety Program Manager at 608-267-3154 or view course information at:

http://www.dot.wisconsin.gov/safety/vehicle/bicycle/docs/tsb-brochure.pdf Bike Ed is a group of courses developed by the League of American Bicyclists (LAB) to suit the needs of any cyclist. LAB certifies, insures and equips League Cycling



Bicycles and walking should be safe and convenient ways for all students to travel to school.

Instructors (LCIs) to teach anything from basic skills to college level courses. LCIs are the experts in bicycle education and safety. Courses offered include: Road I, Road II, Commuting, Motorist Ed, Kids I and Kids II. LCI's can also offer modified versions of these courses, design bike rodeos and provide general safety consulting. Residents can take these classes on their own or community centers, senior centers, schools and employers can coordinate and host group classes.

#### Road I

Gives cyclists the confidence they need to ride safely and legally in traffic or on the trail. The course covers bicycle safety checks, fixing a flat, on-bike skills and crash avoidance techniques and includes a student manual. Recommended for adults and children above age 14, this fast-paced, nine-hour course prepares cyclists for a full understanding of vehicular cycling.

#### Road II

For more advanced students with an understanding of vehicular cycling principles, this 12 hour course includes fitness and physiology, training for longer rides, advanced mechanics, paceline skills, advanced traffic negotiation, foul weather riding and night riding. Student manuals are included with each class.

#### Commuting

For adult cyclists who wish to explore the possibility of commuting to work or school by bike. This three hour follow-up to Road I covers route selection, bicycle choice, dealing with cargo and clothing, bike parking, lighting, reflection and foul weather riding. Included with the class are handouts and student materials.

### **Motorist Education**

A 3-hour classroom session, this course can be easily added to a driver's education curriculum, such as diversion training for reckless drivers or a course designed for local bus drivers. Directed towards motorists in general, topics covered include roadway positioning of cyclists, traffic and hand signals, principles of right-of-way and left and right turn problems. Materials include Share the Road literature for bicyclists and motorists as well as other fact sheets.

#### Kids I

Designed for parents, instructors explain how to teach a child to ride a bike. Topics covered include how to perform a bicycle safety check, helmet fitting and bike sizing. The course includes the 10-minute "Kids Eye View" video and a brochure for parents.

#### Kids II

This 7-hour class for 5th and 6th graders covers the same topics as Road I, including on-bike skills as well as choosing safe routes for riding.

The Bicycle Federation of Wisconsin has trained dozens of people in Wisconsin to teach the League of American Bicyclist courses and the Bike Fed can connect those interested in taking Bike Ed with the closest LCI. For more information on Bike Ed in Wisconsin, visit the Bike Fed's website: http://www.bfw.org



Participants gather in the rain for a Road I course teaching safe bicycling skills.

To find the nearest LCI, visit the League of American Bicyclists website: <u>http://www.bikeleague.org/cogs/resources/findit</u>

### 4.2 Encouragement

Publicizing bicycling and walking is both education and encouragement. By producing and distributing bicycle and walking education material, the Village can provide bicyclists, pedestrians and potential bicyclists with the information they need to bicycle safely and comfortably. WisDOT provides a range of safety materials for free to anyone requesting them by their publication number.

Bicycle and pedestrian related safety materials provided by WisDOT include:

- Wisconsin Bicycle Laws card (HS 221)
- Bicycle Safety-What Every Parent Should Know (HS 239)
- From A to Z by Bike (HS 214, for ages 11-adult)
- Bicycle Safety: A 'Wheely' Good Idea (HS 213, handbook for ages 8-11)
- Bicycles & Traffic-Get Over Your Fear (brochure HS 238)
- Two-Wheeled Survival (brochure HS 227)
- Sharing The Road: Survival of the Smallest (brochure HS 228)
- Street Smarts (updated brochure HS 207)
- Share the Road with Bicycles (bumper sticker HS 237)
- I Stop for Pedestrians (bumper sticker HS 233)

Request materials from WisDOT by publication number using the form found at: <u>http://www.dot.wisconsin.gov/forms/docs/dt1265.doc</u>

Partnering with other agencies and organizations will help deliver bicycle information more effectively. For example, bicycle education should be integrated into school curricula and park programs so that many more children learn to bicycle more safely and frequently. Partnering with media outlets and the private sector will further increase the reach of education campaigns. The Village could also make use of the website www.Streetshare.org to promote bicycling and walking, and to educate citizens about bicycling and walking in the community. http://www.StreetShare.org.

#### **Bike to Work Week**

Bike to Work Week (BTWW) is a promotional campaign that has succeeded in increasing the numbers and safety of individuals who bike to work, shop, school or wherever they need to go in the communities where it has taken place. A recent BTWW campaign in Sheboygan County resulted in a 7% gain in bicycling mode share. The Bicycle Federation of Wisconsin produces a toolkit for concerned citizens to encourage bicycling in their community through Bike to Work promotions: http://www.bfw.org/

#### **Bicycle Map**

Producing and distributing a Village map for bicyclists can go a long way towards encouraging and educating citizens. A total of 92% of respondents to the survey

indicated that a bicycle map of the area would positively (54% "strongly," 38% "moderately") affect their decision to bicycle more. Such a map could educate citizens about the best routes for bicycling and help teach them to safety share the road with motor vehicle traffic by using safety tips and illustrations on the reverse of the map itself.

In creating a bicycle map it is important to gather information from the public regarding the map content. From previous bicycle mapping projects completed by the Bike Fed, map users have expressed that it is important to include the following items:

- All bicycle facilities, including signed routes, bike lanes, and bike trails.
- Public amenities, such as restrooms, parks, and emergency services.
- Private amenities, such as bike shops.
- A map scale that is appropriate for users to easily determine travel distance, with as many roads as feasible labeled.
- A digital version of the map should be available on-line.

Often some of the funds for a bicycle map can be procured from advertising fees from local businesses wanting representation on the map. It might also be possible to partner with the local tourism board or chamber of commerce for support. A bicycle map is more likely to be an effective educational strategy if it is available for free.

#### **Other Public Education Opportunities**

In addition to a bicycle map and education programs described above, there are other ways to get the word out that bicycling is a viable means of transportation and recreation. The Village could work with the Kenosha Area Chamber of Commerce, the Kenosha Area Convention and Visitors Bureau and the Wisconsin Department of Tourism to publicize bicycling. Television and/or radio Public Service Announcements about safe bicycling and motorists safely sharing the road with bicycles could be produced and aired, particularly on Chanel 25 WLIP. Advertising in newspapers, on billboards and on buses can gain bicycling exposure.

Bicycle facility openings and other events also provide opportunities for education and outreach. "Earned media," e.g. a press release in conjunction with a ribbon cutting ceremony, is a great way to get publicity and to generate interest in expanding the bikeway network.



Quiet rural roads offer excellent cycling opportunities just outside the Village. A map showing bicycle routes within the Village as well as links to areas outside the Village would be appreciated by cyclists.

### 4.3 Enforcement

For enforcement to be effective, law enforcement officers need to know which illegal behaviors are the most common factors in crashes. Wisconsin's Pedestrian and Bicycle Law Enforcement training course, available through the Wisconsin Department of Transportation (WisDOT) Bureau of Transportation Safety, provides education for law enforcement officers. The course qualifies towards the training hours required of most law enforcement agencies. For more information, contact the WisDOT Bicycle and Pedestrian Safety Program Manager at 608-267-3154.

The rules for riding bicycles on the road (and rules for motorists sharing the road safely with bicycles) are available online: http://www.dot.state.wi.us/safety/vehicle/bicycle/rules.htm.

WisDOT also distributes free printed safety materials such as a Summary of Wisconsin Bicycle Laws (HS226), and Bicycle (HS221) and Pedestrian (HS244) Law Cards that fit in a wallet. These materials may be requested using form DT1265: <u>http://www.dot.wisconsin.gov/forms/docs/dt1265.doc</u>

WisDOT's Division of Motor Vehicles Motorist Handbook includes nearly 10 pages of information on bicycling safely and on motorists sharing the road safely with bicyclists.

In addition to training police in law enforcement for bicycle safety, training drivers of commercial vehicles to model behavior can bolster enforcement by police officers. The Cities of Madison and Milwaukee educate all drivers of city vehicles about the regulations that require drivers to yield to pedestrians in crosswalks and to give all vehicles (including cyclists) three feet of clearance when passing. By training Village staff and partnering with Kenosha County or City agencies that operate in Pleasant Prairie, the Village can increase compliance with traffic laws on its streets.

# 5. Goals, Objectives and Policies

In order to produce an implementable plan, it is necessary to have an overarching set of goals that are supported by specific objectives and policies. These goals, objectives and policies help guide the overall recommendations of the plan and emerged from the planning process including public input.

## 5.1 Goals

Goals are broad statements that express general public priorities. Goals are formulated based on the identification of key issues, opportunities and problems that impact bicycling and pedestrian issues in the Village.

 Establish and maintain a safe, comprehensive and integrated bicycle and pedestrian trail network that encourages the use of bicycles and walking for commuting, recreation and other trips.

## 5.2 Objectives

Objectives are more specific than goals and are usually attainable through strategic planning and implementation activities. Implementation of an objective contributes to the fulfillment of a goal.

- 1. Provide a network of facilities that meets the transportation and recreation needs of users and links neighborhoods, schools, parks, employment centers and other key destinations.
- 2. Provide robust support facilities including bicycle parking, route signage and lighting that encourage commuter and recreational usage.
- 3. Provide a safe and easy to use bicycle and pedestrian network.
- 4. Maintain bicycle and pedestrian trails and support facilities to a level that provides safe, comfortable and convenient usage for users.
- 5. Increase enforcement of traffic violations by street users so as to reduce the number and severity of motor vehicle, bicycle and pedestrian crashes.
- 6. Provide safe bicycling and walking education to local children, teens and adults.
- 7. Conduct encouragement activities such as Bike to Work Week throughout the year to increase participation in bicycle commuting and recreation.
- 8. Ensure that relevant Village policies encourage and support bicycling and walking.



## **5.3** Policies

Policies are rules and courses of action used to ensure plan implementation. Policies often accomplish a number of objectives.

#### Facilities, Operations, and Maintenance

- 1. In the planning, design and operation of transportation facilities, bicyclists and pedestrians should be included as a part of the public participation process.
- 2. New and reconstructed arterial streets shall include bike lanes, wide curb lanes or paved shoulders, whenever possible.
- 3. New and reconstructed collector streets shall include bike lanes, although bike routes may be substituted in specific instances after consideration of traffic speeds and volumes, route continuity, nearby bicycle facilities and adjoining land uses, whenever possible.
- 4. As bicycles are legal users of the road, off-street bicycle facilities (paths or trails) should be provided as a *supplement* to on-street facilities.
- 5. Sidewalks should be included in all new development or redevelopment projects and new and reconstructed streets that have an urban cross section, when appropriate.
- 6. Include pedestrian actuation signals and count-down timers at all signalized street crossings with pedestrian facilities, including shared-use paths, at either side of the street.
- 7. Provide bicycle signal detectors per State standards at all new signalized intersections with bike lanes and, if feasible, when modifying existing signalized intersections with bike lanes.
- 8. Meet or exceed standard design guidelines on all new bicycle and pedestrian facilities; bring existing facilities that do not meet those standards up to standard as funding allows. The primary resources for these design guidelines are the Wisconsin Bicycle Facility Design Handbook, the WisDOT Facilities Development Manual (FDM), the Manual of Uniform Traffic Control Devices (MUTCD) and the AASHTO Greenbook.
- 9. Maintain Village bicycle and pedestrian facilities to a safe and comfortable level. Maintenance should include frequent sweeping to remove hazards, immediate plowing after snowfall and prompt attention to potholes and other hazards as funding allows.
- 10. Develop guidelines for routine and long-term maintenance of off-street bicycle and pedestrian paths.
- 11. Replace sewer or other drainage grates that are not safe for bicycle operation as funding allows.

### **Support Facilities**

- 12. Provide short-term bicycle parking (racks) at destination locations throughout the Village including retail and recreation areas, parks, schools, and employment centers.
- 13. Promote bicycle parking in all new commercial, industrial and multi-unit housing developments.
- 14. Include support facilities along bicycle and pedestrian paths. Such facilities may include trailhead parking lots, route map displays, rest areas/benches, drinking water, bike racks, restrooms and lighting where deemed necessary for safety.



Wide paved shoulders provide space for pedestrians and cyclists on rural roads.



Providing or requiring short-term bicycle parking at destination locations can encourage bicycle use for transportation.



Directional signage, like this in the Boston area, can aid cyclists in finding destinations.

- 15. Consistently sign on-street and off-street bicycle and pedestrian facilities to provide users with directional and distance information; on-street signage should comply with the Wisconsin Bicycle Facility Design Handbook and the Manual of Uniform Traffic Control Devices.
- 16. Work with adjoining communities to ensure that the bicycle and pedestrian network connects to facilities in those communities.
- 17. Work with Kenosha County and WisDOT to include and implement bicycle facilities on all projects within the Village that fall under County or State jurisdiction.
- 18. Conduct regular bicycle and pedestrian counts around the Village. Require bicycle and pedestrian counts in all manual traffic counts conducted for specific projects.

#### **Education and Encouragement**

- 19. Include at least one piece of bicycle or pedestrian education at least quarterly in the Village newsletter.
- 20. Provide bicycle and pedestrian safety and education materials on the Village webpage.
- 21. Form a Safe Routes to School (SRTS) Committee to develop and implement a SRTS plan and apply for federal, State-administered funding.
- 22. Provide bicycle and pedestrian education to all students enrolled in public and private schools located in the Village through a Safe Routes to School program.
- 23. Provide Village police officers with educational materials on bike issues by offering Wisconsin Pedestrian & Bicycle Law Enforcement Training Course on a biennial basis.
- 24. Adopt Bike to Work Week as a Village-sponsored event, helping with promotion and encouragement of biking.

#### Enforcement

25. Request that the Village Police Department allocate more time to enforcement of traffic violations, particularly failure to stop/yield violations, speeding and safe passing distance violations.

#### Funding

- 26. Pursue funding for bicycle and pedestrian facilities and resources from federal, State and local sources.
- 27. Apply for federal funding for Safe Routes to School and other bicycle education programs targeted at students.

# 6. Design Standards

All bicycle and pedestrian facilities in the Village must meet State and federal design standards for such facilities. These standards help ensure that facilities are safe and meet guidance that has been vetted by professionals in the field. Additionally, meeting proper design standards may help lessen or remove the Village's liability should a crash or accident occur on one of the facilities.

The design standards presented on the following pages are intended as an informational guide and should not be used as engineering documents. All facilities should meet the standards presented in the federal and State guidance noted below, particularly the *Wisconsin Bicycle Facility Design Handbook* and the *Manual on Uniform Traffic Control Devices (MUTCD).* 

## 6.1 Federal and State Design Guidance

The *Guide for the Development of Bicycle Facilities* by the American Association of State Highway and Transportation Officials (AASHTO) is commonly accepted as the "best practices" for building bicycle facilities. http://www.sccrtc.org/bikes/AASHTO 1999 BikeBook.pdf

The Manual on Uniform Traffic Control Devices (MUTCD) by the United States Federal Highway Administration (FHWA) contains currently acceptable signage for use on bicycle facilities as well as experimental signs. http://mutcd.fhwa.dot.gov/

The Wisconsin Department of Transportation *Facilities Development Manual (FDM)* details bicycle facility design (Chapter 11, Section 45, Subject 10). The manual provides definitive guidance from the State on all facility design standards. http://roadwaystandards.dot.wi.gov/standards/fdm/index.htm

The Wisconsin Bicycle Facility Design Handbook expands on the FDM and meets or exceeds AASHTO guidelines. The Handbook uses information from the AASHTO Guide for the Development of Bicycle Facilities and is tailored to meet Wisconsin's needs and conditions.

http://www.dot.wisconsin.gov/projects/state/docs/bike-facility.pdf

Although intended for larger communities, the *Wisconsin Bicycle Planning Guidance: Guidelines for MPOs & Communities in Planning Bicycle Facilities* contains useful information about the importance of planning a complete bikeway network. <u>http://www.dot.state.wi.us/projects/bikes.htm</u>

In addition to this guidance, all bicycle and pedestrian facilities shall meet the requirements of the Americans with Disabilities Act (ADA) Accessibility Guidelines. http://www.access-board.gov/adaag/html/adaag.htm





The Wisconsin Bicycle Facility Handbook provides definitive guidance on bikeway design for all Wisconsin municipalities.

## 6.2 On-Street Facilities

Under Wisconsin law bicyclists are legal users of the roadway and are entitled to all of the rights and responsibilities of motor vehicles. All streets other than limited access highways are an important part of the bicycle transportation network regardless of if they are specifically designated for bicycle use. However, there are specific on-street facilities that can make streets safer, more comfortable and more convenient for bicycles to use. These facilities are detailed below.

#### **Bike Lanes**

A bicycle lane is a portion of the roadway designated for exclusive or preferential use by bicyclists. Bicycle lanes are always one-way facilities and are identified with pavement markings and signing. On two-way streets, a one-way bicycle lane should be provided on each side. Bicycle lanes are the preferred bicycle facility on higher volume urban and suburban roadways (i.e., collector and arterial streets) but are seldom justified on residential streets. Bike lanes provide a significant benefit to safe and efficient bicycle circulation by providing separate identifiable areas on the street for bikes and autos. Bicycle lanes can be used on streets with and without onstreet parking. For more information and frequently asked questions regarding bike lanes, see the Pedestrian and Bicycle Information Center: http://www.bicyclinginfo.org/engineering/facilities-bikelanes.cfm

#### **Bike Routes**

Signed bike routes are on-street routes intended to provide connectivity and continuity to the bikeway system. Bike routes are usually established along through routes not served by bike routes or as an alternative to bicycling on busy streets. Bike routes are designated by signs or permanent pavement markings and are shared by motorists. Bike routes should provide direct travel from one destination to another or connect discontinuous segments of shared use paths, bike lanes, or bike routes. Efforts should be made, if necessary, to adjust traffic control devices to give greater priority to bicyclists on the route as opposed to other parallel streets. For more information and frequently asked questions regarding bike routes, see the Pedestrian and Bicycle Information Center:

http://www.bicyclinginfo.org/engineering/facilities-roadways.cfm

#### Wide Shoulders

On rural highways smoothly paved shoulders can serve as a bicycle facility. Shoulders provide clearance between bicyclists and high-speed motor vehicle traffic and they reduce the "wind blast" effect of passing trucks. In addition to benefiting bicyclists, wide paved shoulders provide an area for vehicles to pull onto during emergencies and maintenance benefits due to elimination of rutting adjacent to the edge of travel lane, improved drainage, and lateral support for the roadway base. For more information and frequently asked questions regarding wide shoulders, see the Pedestrian and Bicycle Information Center:

http://www.bicyclinginfo.org/engineering/facilities-shoulders.cfm







#### Roundabouts

Although not specific bicycle facilities, roundabouts deserve a special mention due to their recent approval for construction on State Highway 165 in Pleasant Prairie and design issues that can be problematic for bicycles. In general, single-lane roundabouts are excellent facilities for bicycles due to the relatively low speed of traffic and the ability of bicyclists to "take the lane" as they proceed through the roundabout. Double-lane roundabouts much be approached with greater caution due to higher motor vehicle speeds and lane changing within the roundabout.

Bicycle lanes should end before the roundabout with signage indicating that bicycle traffic may use the full travel lane. Additionally, a shared-use path should be provided around the circumference of the roundabout with bicycle entrance and exit ramps from the street; this allows bicyclists who are not comfortable moving into the travel lane through the roundabout to use the path instead. Signage on the path should indicate that it is a one-way facility for bicycles and the path should provide adequate width for pedestrians and bicyclists. It should be noted that WisDOT will fund 100% of construction costs for roundabout shared-use paths on projects the State is responsible for as long as the local municipality assumes maintenance for the facility. For detailed information on design see the WisDOT Roundabout Guide, found in Chapters 11, Sections 25 and 26 of the Facilities Development Manual:

http://www.dot.state.wi.us/safety/motorist/roaddesign/roundabout-design.htm

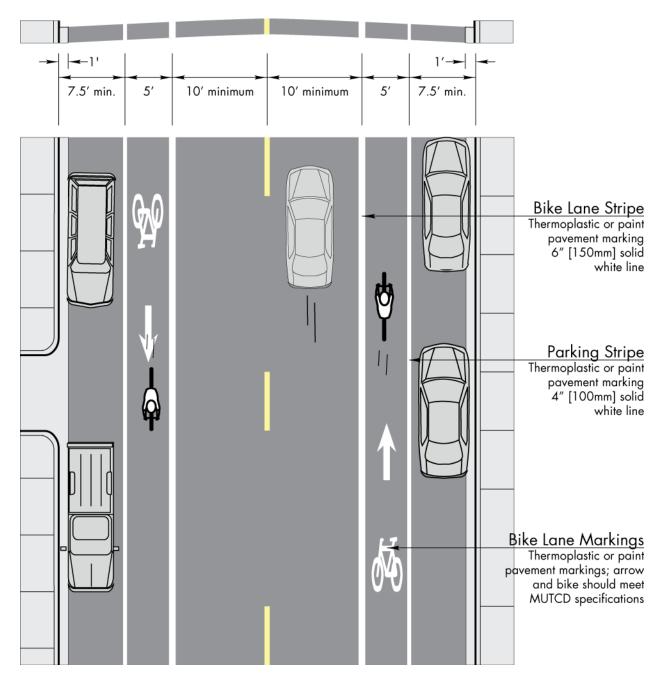


# **General Urban Street Cross Section with Bike Lanes**

This generalized cross section provides *minimum* dimensions for an urbanized street with bicycle lanes. It provides:

- One traffic lane in each direction
- One bicycle lane in each direction
- May include parking lanes on one or both sides of the street

Refer to the *Wisconsin Bicycle Facilities Design Handbook, Wisconsin Facilities Design Manual (FDM)* and 2009 Manual on Uniform Traffic Control Devices (MUTCD) for full guidance and engineering specifications. Diagram is for informational purposes only and is not to scale.

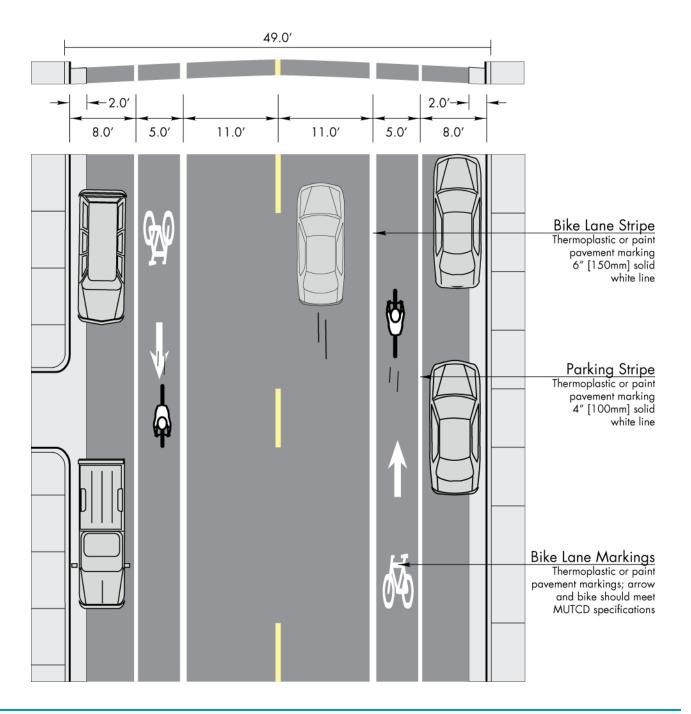


# **R-2 49' Cross Section**

This cross section provides dimensions for Pleasant Prairie's R-2 Residential Collector. It provides:

- One traffic lane in each direction
- One bicycle lane in each direction
- Parking lanes on both sides of the street

Refer to the *Wisconsin Bicycle Facilities Design Handbook* and *Wisconsin FDM* for full guidance and engineering specifications. Diagram is for informational purposes only and is not to scale.

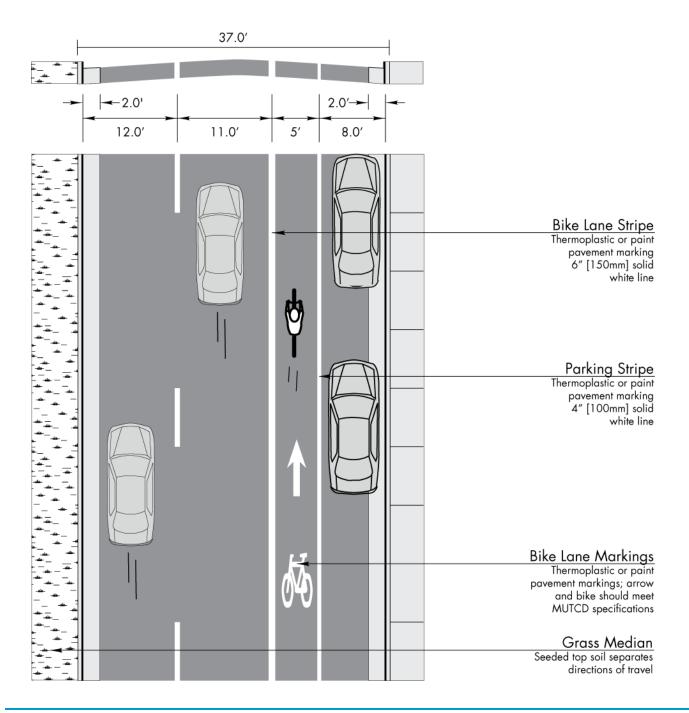


# R-5 37' Cross Section

This cross section provides dimensions for one side of Pleasant Prairie's divided R-5 Arterial. It provides:

- Two traffic lanes in each direction (divided by a median)
- One bicycle lane/wide curb lane in each direction
- Parking lanes on both sides of the street (divided by a median)

Refer to the *Wisconsin Bicycle Facilities Design Handbook* and *Wisconsin FDM* for full guidance and engineering specifications. Diagram is for informational purposes only and is not to scale.

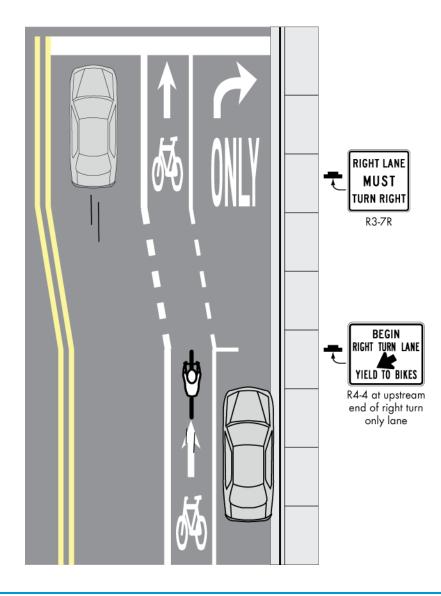


# **Generalized Intersection with Bike and Parking Lanes**

This is a generalized intersection approach for an unban street with one travel lane, one bike lane and one parking lane. Specific Pleasant Prairie intersection engineering documents are provided on pages 34 through 37.

- The parking lane should drop in advance of the intersection
- The right turn only lane should be added in place of the parking lane
- The travel and bicycle lanes may need to shift away from the curb to provide adequate width for the turn lane
- The bicycle lane should be dashed across the area where cars merge into the turn lane
- Signage should indicate that cars merging to the turn lane must yield to bikes

Refer to the *Wisconsin Bicycle Facilities Design Handbook, Wisconsin Facilities Design Manual (FDM)* and 2009 Manual on Uniform Traffic Control Devices (MUTCD) for full guidance and engineering specifications. Diagram is for informational purposes only and is not to scale.

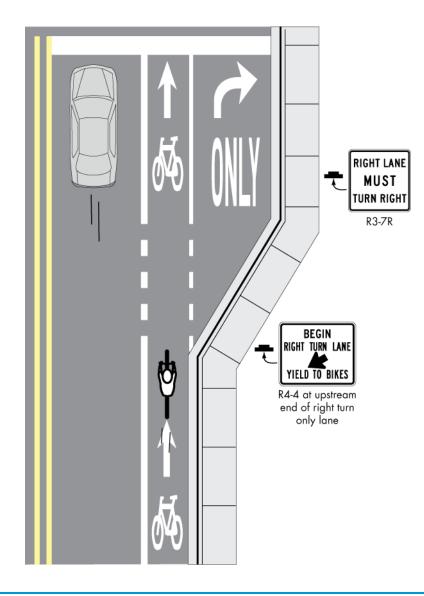


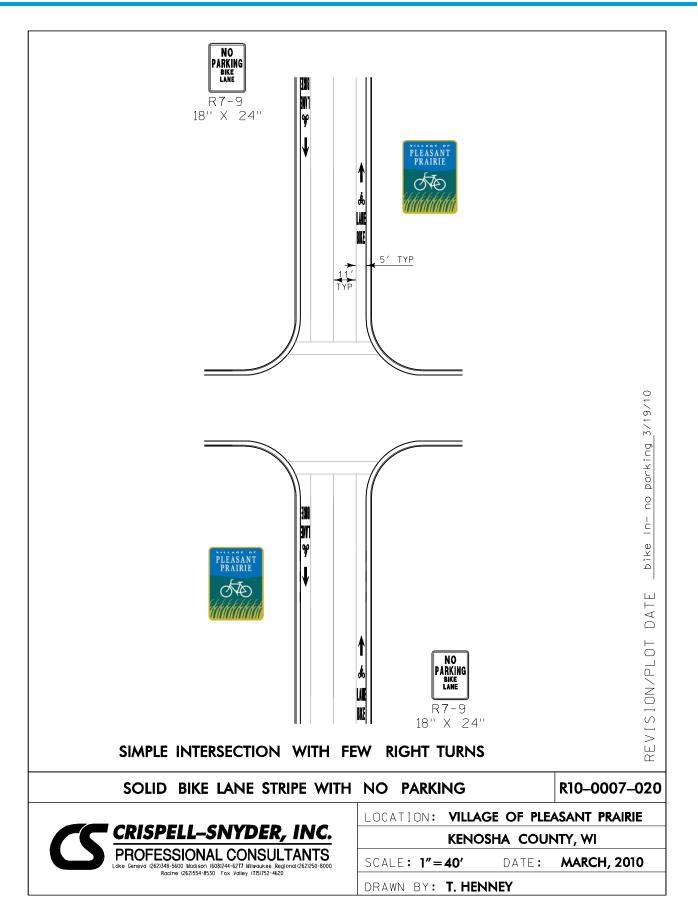
# **Generalized Intersection with Bike and Turn Lanes**

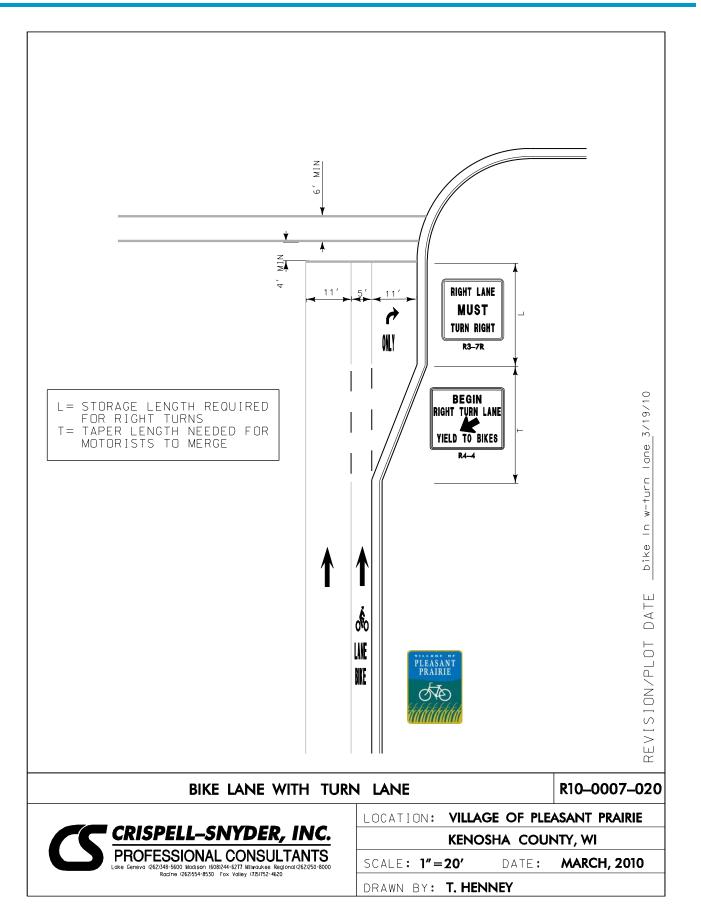
This is a generalized intersection approach for an unban street with one travel lane, one bike lane and an added turn lane.

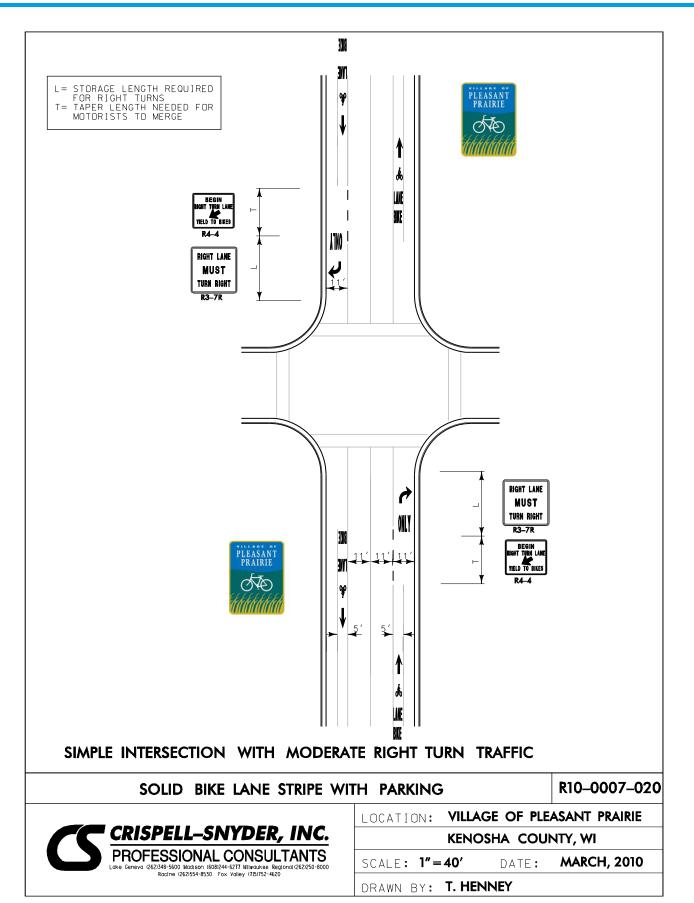
- The right turn only lane should be added to the right of the bike lane
- The bicycle lane should be dashed across the area where cars merge into the turn lane
- Signage should indicate that cars merging to the turn lane must yield to bikes

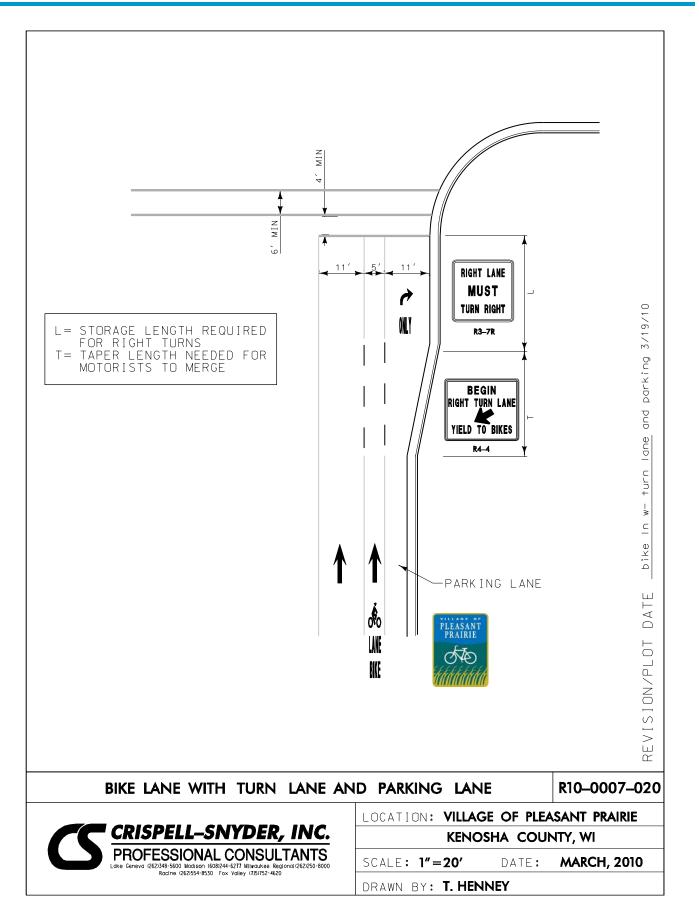
Refer to the *Wisconsin Bicycle Facilities Design Handbook, Wisconsin Facilities Design Manual (FDM)* and 2009 Manual on Uniform Traffic Control Devices (MUTCD) for full guidance and engineering specifications. Diagram is for informational purposes only and is not to scale.









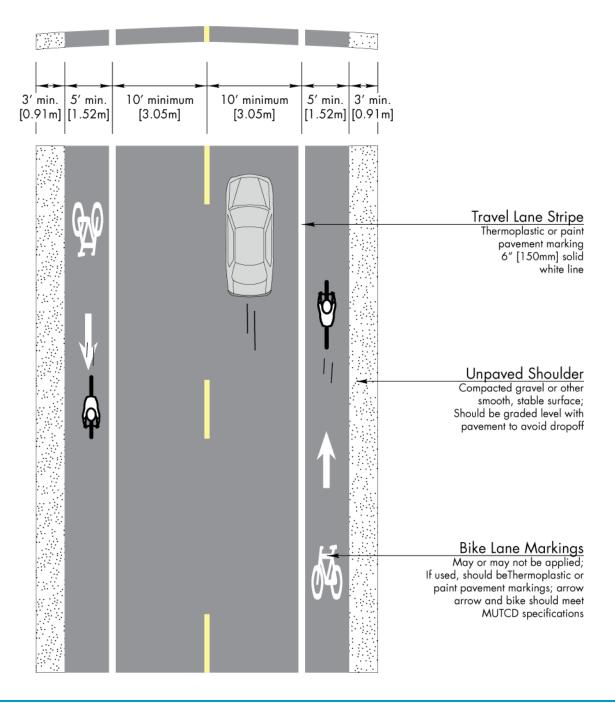


# **Rural Collector/Arterial Cross Section**

This cross section provides dimensions for a rural collector or arterial street with wide paved shoulders. It provides:

- One traffic lane in each direction
- One bicycle lane/paved shoulder in each direction
- May include parking lanes on one or both sides of the street

Refer to the *Wisconsin Bicycle Facilities Design Handbook* and *Wisconsin FDM* for full guidance and engineering specifications. Diagram is for informational purposes only and is not to scale.



# **Signed Bicycle Routes**

Signed bicycle routes are typically designated on low traffic streets and do not feature any special on-street marking for bicycles. Signs for bicycle routes should be placed before and after every turn, at every signalized intersection, every ¼ mile in urbanized areas and every ½ mile in rural areas.

Bicycle route signage should be combined with directional signage that allows users to easily determine the distance to specific destinations. Common destinations include parks, schools, cultural centers, retail areas, and other popular locations.

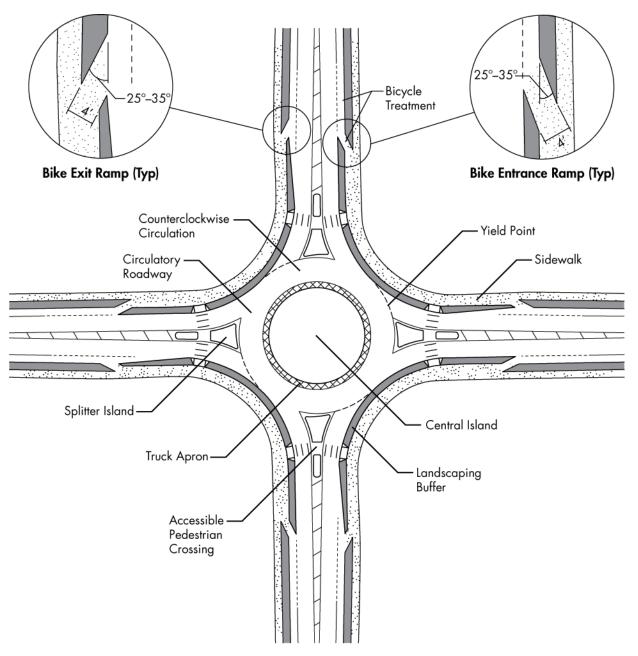
Refer to the *Wisconsin Bicycle Facilities Design Handbook*, the *MUTCD* and *Wisconsin FDM* for full guidance and engineering specifications.



# Roundabouts

Roundabouts are increasingly replacing intersections of collector and arterial streets. These facilities provide numerous benefits, but can be challenging for bicyclists to navigate. When approaching a single-lane roundabout, bicyclists should be given the option to "take the lane" and proceed through the roundabout as a motor vehicle would or to use a shared-use path around the exterior of the roundabout.

Refer to the *Wisconsin Department of Transportation Roundabout Guide* and *Wisconsin FDM* for full guidance and engineering specifications.



Source: Wisconsin Department of Transportation Roundabout Guide

# 6.3 Off-Street Facilities

Off-street facilities are a critical part of the bicycle and pedestrian network. For bicyclists, off-street facilities provide a supplement to on-street facilities and can provide more direct access to specific destinations than on-street facilities do. For pedestrians, off-street facilities can also provide more direct access to destinations. Both sets of users can benefit from facilities that are designed for their specific use without the use of motor vehicles. It should be stressed that as a part of the bicycle network, off-street facilities are a complement to, and not a replacement for, onstreet facilities.

# Shared-Use Paths

Shared-use paths are paved facilities located in a separate right of way, for the exclusive use of bicycles and pedestrians, with minimal cross flow by motor vehicles. Off -street bike paths are typically located within open space corridors along creeks, high voltage power line corridors and community/city-wide parks. They may also be located within greenways, which are landscaped areas created for the purpose of providing important bicycle and pedestrian linkages between uses. Shared-use paths offer important bicycle commuting opportunities, but on their own are not sufficient to fully support bicycle commuting because of limited connections to destinations. Shared-use paths provide an important recreational amenity for bicyclists, pedestrians, dog walkers, runners, roller blades and other non-motorized forms of travel.

Shared-use paths are paved facilities that should be wide enough to offer a wide range of users a comfortable and safe experience. They are also often located and designed to provide service vehicle access to utility corridors and emergency vehicle access to open space, including police and fire vehicles.



# **Recreational Trails**

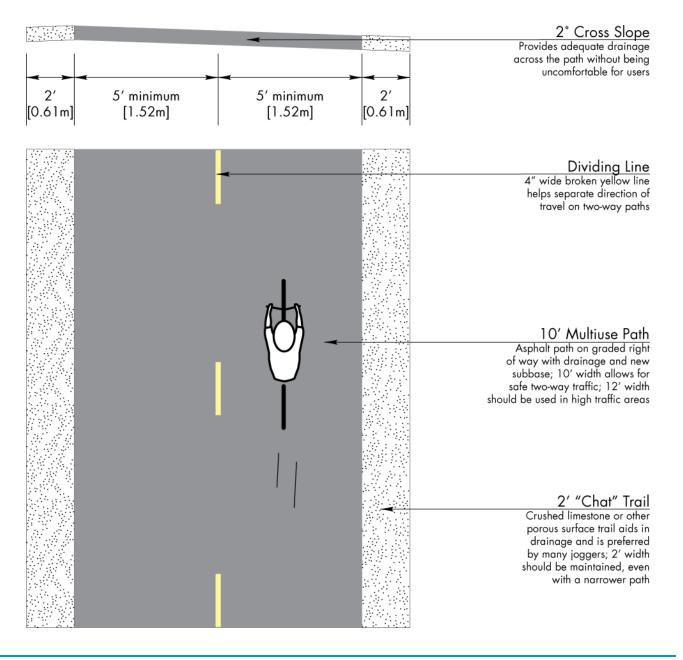
Recreational trails are typically non-paved, narrower trails designed specifically for pedestrian use. Such trails are appropriate in sensitive ecological areas or areas where high levels of bicycle and pedestrian traffic are not desired. Recreational trails are designed primarily for recreational and not transportation purposes, although some may use them for transportation. These trails should be provided as a supplement to the off-street shared-use path network and the on-street bicycle facility network.

# **Shared-use Paths**

Shared-use paths are off-street bicycle facilities that serve two-way bicycle and pedestrian traffic. Because shared-use paths are intended for pedestrian use, the design must conform to Americans with Disabilities (ADA) guidance.

Shared-use paths should be considered a supplement to, and not a replacement for, on-street bicycle facilities.

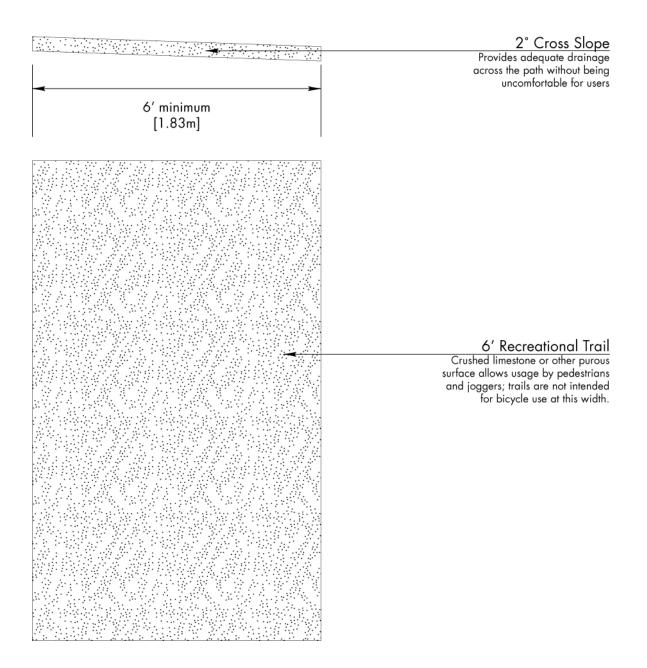
Refer to the *Wisconsin Bicycle Facilities Design Handbook* and *Wisconsin FDM* for full guidance and engineering specifications. Diagram is for informational purposes only and is not to scale.



# **Recreational Trails**

Recreational trails are dirt or gravel trails intended for pedestrian only use. These trails are appropriate in environmentally sensitive areas.

Refer to the *Wisconsin Bicycle Facilities Design Handbook* and *Wisconsin FDM* for full guidance and engineering specifications. Diagram is for informational purposes only and is not to scale.



# **6.4 Support Facilities**

With the provision of bicycle and pedestrian facilities it is critical that the Village or private sponsors offer support facilities and services to ensure that paths and roadways are safe and comfortable to use. These facilities and services include highlevel maintenance and the provision of signage throughout the bicycle and pedestrian network. Additionally, trailheads, lighting, and parking areas can be appropriate in specific locations to ease access to the network and increase safety.

# **General Maintenance**

Good maintenance is critical for the safe operation of bicycle and pedestrian facilities. Debris in bicycle lanes or on paths presents a hazard to users as bicyclists can easily fall or be pushed into traffic while pedestrians also risk falls.

On-street bicycle facilities should be maintained as part of regular street maintenance. However, due to the susceptibility of bicycles to damage and danger risks from debris or pavement damage, priority should be given to sweeping bicycle lanes and repairing potholes or other damage more frequently than standard travel lanes. Frequent sweeping of priority bicycle lanes and routes is particularly important because cars in adjoining lanes push rocks and other debris into the less traveled bicycle lanes.

Off-street bicycle and pedestrian facilities, including sidewalks, should also receive frequent regular maintenance. Off-street facilities should be swept as needed, with particular attention paid to areas adjoining gravel paths where gravel may spread onto the pavement and present a slip hazard. Sweeping should also be conducted more frequently during the fall and spring seasons when leaves may present a hazard on trails and paths. The appearance of potholes or cracks is less common on off-street facilities, but those that do appear should be promptly patched and sealed. Regular inspection of sidewalks should check for cracking or lifting due to pressure from tree roots. Vegetation adjoining sidewalks and paths must be pruned to maintain visibility along the facility.

Appendices A.3 and A.4 of the *Wisconsin Bicycle Facilities Design Manual* provide additional details on maintenance of on- and off-street facilities



Bicycle lanes and access should be maintained through street construction or projects that impede into the bike lane.



Shared-use paths should be plowed immediately after a snow fall.



Ample bicycle parking should be provided at popular destinations.

# **Snow Plowing**

For bicycle and pedestrian facilities to serve as true transportation alternatives they must be accessible and usable year round. Snow should promptly be cleared from all priority portions of the network. On-street facilities should be plowed when the street is cleared. It is critical that travel lanes are not plowed into the bike lane therefore forcing bicyclists into the regular travel lane, particularly on high-speed arterials. Sidewalks should be cleared of snow in a timely manner especially in key corridors and areas around schools.

Priority off-street paths should be plowed at the same time as major arterials and collectors. These priority routes should be those that parallel major roadways, serve as cross-Village links, or provide access to major destinations including employment centers, schools, and recreation facilities. Assigning snow clearance duties to the Public Works Department allows path plowing concurrent to the plowing of streets. Plowing of minor off-street paths can occur after major paths and streets have been cleared, but should be cleared in a timely manner after each snowfall.

# **Bicycle Parking**

Bicyclists must have a reasonable expectation that there will be a safe location to lock their bike at their destination for bicycling to serve as a practical form of transportation. The Village should ensure that adequate bicycle parking exists both at recreational sites such as parks, the RecPlex and trailheads, as well as commercial, industrial and employment sites. This can be accomplished by direct installation of racks by the Village, offering a cost sharing program for rack installation with interested businesses, and through changes in the local ordinance that require minimum amounts of bicycle parking at employment and commercial buildings, as is currently done for motor vehicles.

It is important that racks adequately support bicycles and are securely fastened to the ground. The City of Madison has produced a one-page handout that describes racks that do and do not meet the City's standards for bicycle parking. The document also describes minimum dimensions for the installation of bicycle racks. http://www.cityofmadison.com/trafficEngineering/documents/MadisonBikeParkin g.pdf

Bicycle racks should be prominently located so that cyclists can easily find them when arriving at destinations. Cyclists should not have to search around a building for a rack or lock their bike to objects such as street signs or railings because racks are not available. Studies have shown that patrons arriving by bike or on foot typically spend more money at local restaurants and shops than those arriving in motor vehicles. Additionally, employees who regularly commute by bike tend to be healthier and have lower medical costs than those who do not commute by bike. It may be useful to highlight these facts to business owners when requesting or requiring that they install bicycle parking.

# Signage and Markings

Adequate signage is essential throughout the bicycle and pedestrian network. This signage provides regulatory, warning and directional information to users on both on-road and off-road facilities.

Regulatory signage and marking should provide users with a clear indication of how they should behave. Common markings and signage indicates proper direction of travel, speed limits and establishing right of way for users.

Warning devices alert users to potential hazards. These devices often warn of hazardous conditions including steep grades, sharp curves or changes in path conditions. Signage can also alert users to approaching intersections or railroad crossings. When used on off-street facilities, warning signage should be the same as MUTCD-approved on-road signage, although it is often scaled down slightly in recognition of the lower speeds of path users compared to street users.

Users often consider informational signage the most important signage on a path or street. This signage commonly points users to popular destinations, orients users within the larger network and provides travel distances and approximate times to specific locations. Informational signage should generally follow MUTCD guidance, although there is more freedom to use custom colors or designs with this signage, particularly on off-street paths.

All signage and markings should comply with the *Wisconsin Bicycle Facilities Design Manual*, which details signage and marking requirements throughout the manual. Section 4.14 of the Manual presents guidance specific to off-street paths, although much of the signage detailed there is also relevant to on-street facilities.

# Lighting

Bicyclists in the State of Wisconsin are required to use a front headlight when traveling between dusk and dawn. Despite this requirement, fixed-source lighting should be considered to improve visibility and safety along paths and at intersections with streets. Lighting allows bicyclists to better see their surroundings and observe potential hazards, while also providing pedestrians who may not have a light the same advantages. Lighting should be strongly considered where night use of paths is expected; paths to and from the RecPlex are a good example in Pleasant Prairie. Regardless if lighting is provided along paths, lighting should always be provided at intersections between paths and streets or other paths. Additionally, lighting should be strongly considered through underpasses or tunnels, on overpasses and in areas where personal safety may be a concern, even if lighting is not provided on other portions of the path.

All path lighting should comply with the guidance provided in the Wisconsin Bicycle Facility Design Manual, section 4.13. Additionally, lighting should be provided at a scale that is appropriate for bicycles and pedestrians. Tall street lamps intended for lighting streets or parking lots are not appropriate for path or sidewalk lighting, although they may be used where paths intersect streets. **Trailheads** 



Pedestrian scale lighting should be closely spaced. These lights in Madison are solar powered.



Trailheads located at a number of locations throughout the Village can provide an important amenity to path and trail users. Trailheads should be located at heavily used path and street intersections. At a minimum, these locations should provide a small parking lot, bicycle racks, a large map of the bicycle and pedestrian network and benches for resting. Additional amenities can include drinking fountains, public restrooms and lighting. The trailheads should provide a safe and welcoming environment for path users whether they are beginning and ending their trip there, or using the area for a rest during their commute or workout. The Village should consider integrating trailheads and with existing and planned parks, this allows shared use of facilities with park visitors.



Trailheads can be provided at popular destinations such as parks where parking and other facilities can be shared.

# 7. Recommended Bike and Pedestrian Network

This section describes the recommended bicycle and pedestrian trail network in the Village. The network was developed through the public participation process, future development plans and plats, an assessment of local roadways and current transportation plans. The network was designed to provide coverage throughout the community, including access to schools, parks, and other popular destinations, and connectivity into surrounding municipalities.

# 7.1 Proposed Network

A map of the proposed bicycle and pedestrian network is presented on the following page. As shown in Table 1, the proposed network expands the bicycle and pedestrian facilities in the Village from approximately 10 miles to 98 miles. Slightly more than half of the total network is comprised of on-street bicycle lanes, while the remainder is primarily composed of paved shared-use paths. These facilities, combined with installation of sidewalks in new developments and along rebuilt roadways, will result in a robust network for pedestrians and bicyclists.

# Table 1: Existing and Planned Bicycle and Pedestrian Facilities (miles)

	Existing	Proposed	Total
Bicycle Lanes	2.29	50.98	53.27
Shared-use Path (Paved)	3.74	27.18	30.92
Shared-use Path (Gravel)	3.53	2.24	5.77
Recreational Trails	0.80	7.52	8.32
Total	10.36	87.92	98.28



lane, one bike lane and one parking lane in each direction.

The proposed facilities will need to be individually evaluated for feasibility and environmental constraints before implementation, particularly the shared-use paths and proposed future streets.

A full map of existing and proposed bicycle and pedestrian facilities is located at the back of this plan. Small maps on the following pages divide Pleasant Prairie into thirds and detail the proposed facilities within those areas.

# 7.2 Facility Recommendations and Conceptual Costs

This section provides more detailed information on the proposed bicycle and pedestrian facilities, including segment lengths and conceptual costs. Costs for bicycle projects vary widely across the state and the country and the figures provided should be treated only as very rough planning figures.

# **Cost Calculator**

A detailed project cost calculator is provided Pedestrian and Bicycle Information Center. This tool provides regional averages for construction, planning, administrative and other costs while also allowing users to specify known costs. http://www.bicyclinginfo.org/bikecost/

# **Bicycle Lanes**

The cost assumptions for bicycle lanes are for pavement markings and signage only. It is assumed that streets with proposed bicycle lanes either have adequate width to add lanes or that space for lanes will be added when the street is next reconstructed. *Construction costs are not included in the cost estimates for bicycle lanes*. Based on costs provided by WisDOT and from recent City of Milwaukee projects, an estimate of \$5,000/mile is used for painted bike lanes.

# **Shared-Use Paths**

Cost estimates vary widely for paved shared-use paths depending on the topography, soil and water features, drainage issues and other environmental constraints. Common figures for a 10' paved shared-use path vary from \$240,000/mile (Florida Department of Transportation) to over \$1.3 million/mile (Roseville, CA Bike Plan) for basic paths. Road crossings, environmental constraints, drainage issues, and path amenities can rapidly escalate these costs. For the purposes of this plan, an estimated cost of \$500,000/mile is used.

The proposed gravel multiuse paths are estimated to cost \$25,000/mile. Pricing for recreational trails is not provided as costs can vary widely given ground conditions.

# Budgeting

The Village should budget for engineering costs, including a contingency for cost overruns. Federal and State funding is awarded for a fixed amount, and will not cover cost overruns, so budgets should be made carefully. A common figure to budget for engineering and contingencies is 20% of the total project cost.

# Table 2: Proposed Bicycle Network Conceptual Costs

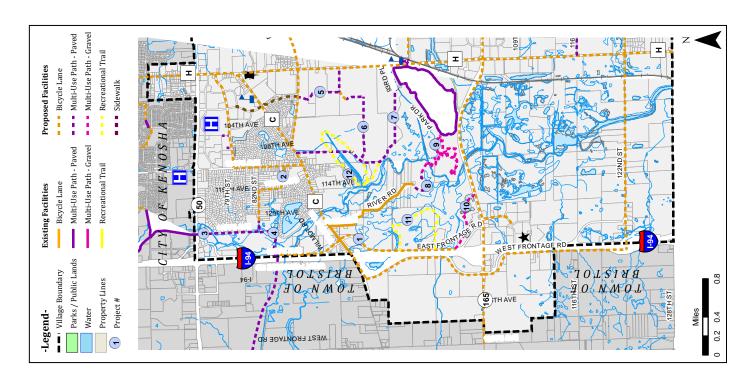
	Length	Cost per mile	Total
Bicycle Lanes	50.98	\$5,000	\$254,900
Shared-use Path (Paved)	27.18	\$500,000	\$13,590,000
Shared-use Path (Gravel)	2.24	\$25,000	\$56,000
Recreational Trails	7.52	N/A	N/A
Total	80.40		\$13,900,900



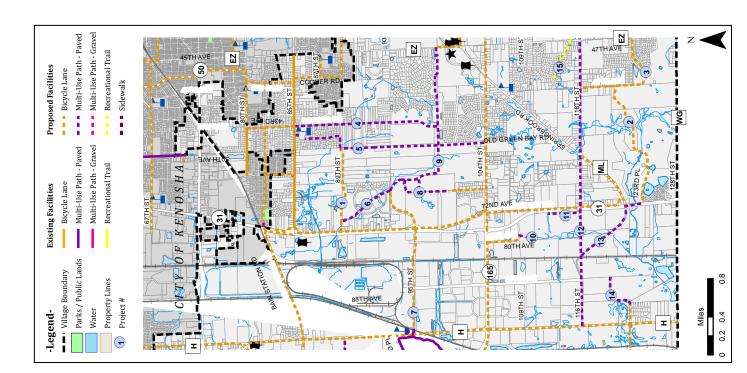
A divided shared-use path in Madison provides separate space for bicyclists and pedestrians.

# West Village Border – 88<sup>th</sup> Ave

;					
Name	#	From	То	Miles	Estimated Cost
Proposed Bike Lanes (On Street)	s (On St	reet)			
Prairie Ridge Blvd		104th Ave	88th Ave	1.03	\$5,163
82nd St		E of 111 Ave	108th Ave	0.33	\$1,652
Bain Station Rd		104th Ave	88th Ave	1.02	\$5,114
Cty Hwy C		West Frontage Rd	88th Ave	2.50	\$12,516
104th St		W Village Border	88th Ave	3.00	\$14,984
122nd St		West Frontage Rd	88th Ave	2.09	\$10,431
West Frontage Rd		South of Cty Hwy C	122nd St	3.02	\$15,109
East Frontage Rd		104th St	S of Cty Hwy C	1.22	\$6,105
114th Ave		104th St	116th Ave	0.36	\$1,775
River Rd		Cty Hwy C	End	1.11	\$5,567
108th Ave		82nd St	84th St	0.24	\$1,218
104th Ave		Prairie Ridge Blvd	Bain Station Rd	0.73	\$3,668
88th Ave		S Village Border	N Village Border	5.54	\$27,698
Proposed Bike Lanes (Future Streets)	s (Futur	e Streets)			
East Frontage Rd	1	South of Cty Hwy C	North of Cty Hwy C	1.15	\$5,735
	2	2 108th Ave	104th Ave	0.26	\$1,304
<b>Proposed Multiuse Paths - Paved</b>	aths - F	aved			
Path 3	3	3 N Village Border	84th St	1.00	\$498,382
Path 4	4	4 W Village Border	84th St	0.33	\$163,584
Path 5	5	Bain Station Rd	Path 6	0.48	\$240,655
Path 6	9	Path 7	88th Ave	1.24	\$621,644
Path 7	7	7 Cty Hwy C	Lake Andrea Path	1.84	\$918,920
Path 8	8	8 River Rd	Jerome Creek	0.50	\$248,863
Proposed Multiuse Paths - Gravel	aths - G	iravel			
Path 9	6	9 Jerome Creek	Lake Andrea Path	1.57	\$39,182
Path 10	10	10 East Frontage Rd	104th St	0.67	\$16,869
Proposed Recreational Trails - Gravel	nal Trail	s - Gravel			
Trail 11	11	11 East Frontage Rd	East Frontage Rd	1.22	No estimate



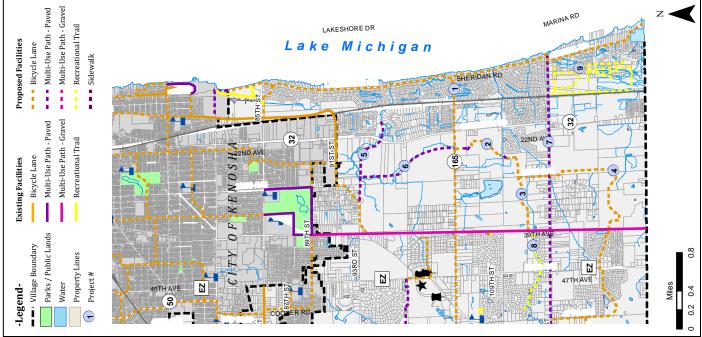
Name	From	10	Miles	Estimated Cost
Proposed Bike Lanes (Un Street)	On Street)			
Bain Station Rd	88th Ave	Green Bay Rd	1.23	\$6,127
80th St	60th Ave	Village Border	0.55	\$2,730
82nd St	60th Ave	Cooper Rd	0:50	\$2,479
85th St	Green Bay Rd	39th Ave	1.89	
86th PI	43rd Ave	39th Ave	0.25	
104th St	88th Ave	39th Ave	2.99	57
122nd St	88th Ave	End	0.22	\$1,096
123rd PI	Green Bay Rd	End	0.45	
122nd St	43rd Ave	39th Ave	0.19	296\$
123rd St	End	122nd St	0.15	
Green Bay Rd	Bain Station Rd	85th St	02.0	\$1,519
Old Green Bay Rd	Bain Station Rd	Springbrook Rd	3.81	\$19,075
Cooper Rd	80th St	85th St	0:50	\$2,515
Springbrook Rd	Green Bay Rd	39th Ave	2.64	\$13,213
39th Ave	Path 8	100th St	0.26	\$1,290
43rd Ave	Village Border	85th St	0.25	\$1,251
43rd Ave	85th St	86th PI	0.22	\$1,089
47th Ave	116th St	123rd St (Future)	0.65	\$3,257
72nd Ave	104th St	108th St	0.35	
64th Ave	104th St	100th St	0.29	\$1,444
Proposed Bike Lanes (Future Streets)	(Future Streets)			
89th St (Future)	1 Green Bay Rd	89th St (Existing)	0.90	\$4,523
123rd PI Extension	2 123rd PI (Existing)	47th Ave	1.01	\$5,049
123rd St Extension	3 47th Ave	123rd St (Existing)	0.20	\$1,003
64th Ave Extension	64th Ave		0.11	\$558
<b>Proposed Multiuse Paths</b>	ths - Paved			
Cooper Road Path	4 85th St	Cooper Rd (S Section	1.54	\$768,462
Path 5	5 #1	104th St	1.54	\$768,877
Path 6	6 89th St (Future)	93rd St	0.64	
95th St Path	7 88th Ave	Old Green Bay Rd	1.06	
Path 8	8 64th Ave Extension	Path 9	0.72	\$361,901
Path 9	9 Old Green Bay Rd	39th Ave	2.47	\$1,237,366
72nd Ave Extension	10 72nd Ave	Park	0.17	\$85,915
Green Bay Rd Path	11 116th St	Park	0.35	\$172,609
116th St Path	12 88th Ave	39th Ave	2.98	\$1,492,488
Springbrook Rd Path	13 116th St	Green Bay Rd	0.63	\$313,406
122nd St Extension	14 122nd St		0.46	\$230,977
54th Ave Path	Park	89th St	0.37	\$186,785
89th St Path	54th Ave	End	0.12	\$60,220
Green Bay Rd Path	Springbrook Rd	123rd PI	0.19	
Proposed Recreational Trails - Gravel	l Trails - Gravel			
Trail 15	15 113th St	39th Ave	0.46	No estimate



**Existing and Proposed Facilities:** 

**East Section** 

Village of Pleasant Prairie



39 <sup>th</sup> Ave	ц Г	39 <sup>th</sup> Ave – Lake Michigan	gan		
Name	#	From	To	Miles	Estimated Cost
Proposed Bike Lanes (On		Street)			
Springbrook Rd		39th Ave	91st St	1.38	\$6,922
89th St		Village Border	Village Border	0.17	\$832
89th St		22nd Ave	17th Ave	0.25	\$1,248
85th Ave		7th Ave	3rd Ave	0.22	\$1,121
3rd Ave		85th Ave	86th PI	90'0	602\$
86th PI		3rd Ave	1st Ave	01.0	\$516
100th St		39th Ave	32nd Ave	0.43	\$2,143
32nd Ave		39th Ave	Park	0.06	\$310
104th St		39th Ave	8th Ave	1.80	066'8\$
22nd Ave		166th St	End	0.25	\$1,259
122nd St		39th Ave	32nd Ave	25.0	\$1,837
32nd Ave		122nd St	Kenosha County Path	90'0	\$321
Marina Rd		116th St	End	0.87	\$4,363
Lakeshore Dr		116th St	86th PI	2.98	\$14,876
Proposed Bike Lanes (Future Streets)	s (Futui	re Streets)			
104th St Extension	Ì	I 104th St (Existing)	2nd Ave	0:30	\$1,490
		2		0.45	\$2,271
		3 Kenosha County Path		08.0	\$4,000
	7	4 Kenosha County Path	166th St	1.51	\$7,529
Marina Rd Extension		Marina Rd	Lake Michigan	0.24	\$1,188
22nd Ave Extension		22nd Ave	#3	0.09	\$452
Proposed Multiuse F	Paths - I	- Paved			
Path 5	4	5 Springbrook Rd	Park	0.95	\$476,582
Path 6	6	6 104th St	Path 5	1.10	\$548,105
116th St Path		7 39th Ave	Lakeshore Dr	2.28	\$1,139,666
		32nd Ave	Kenosha County Path	0.05	\$22,997
Proposed Recreational Trails - Gravel	nal Trai	ls - Gravel			
Trail 8	~	3 39th Ave	Kenosha County Path	0.45	No estimate
Trail 9	0,	9 S of 116th St	E of RR Corridor	3.70	No estimate

# 7.3 Funding Opportunities

Many different funding sources are available for accommodating bicycles and pedestrians through on-street or off-street facilities. In order to be eligible for funding under most federal aid programs, bicycle projects must be primarily for transportation purposes. In general, federal guidelines consider any bicycle path or trail other than a closed loop trail as being principally for transportation and eligible for federal funding. State funding for the construction of on-street and off-street bicycle facilities is available through programs administered by the Wisconsin Department of Transportation (WisDOT), and includes funds provided directly by the state and "pass-through" funds provided by the Federal government as part of the Federal-aid Highway, Transit, and Highway Safety Programs.

# **Transportation-Based Funding Sources**

The following is a summary of potential transportation-based funding sources for accommodating bicycles from WisDOT:

# Local Transportation Enhancement (TE) Program Bicycle and Pedestrian Facilities Program (BPFP)

The Transportation Enhancement Program funds projects that increase sharedmodal transportation alternatives and enhance communities and the environment. Federal funds administered through this program provide up to 80% of costs for a wide variety of projects such as bicycle or pedestrian facilities, landscaping, streetscaping and the preservation of historic transportation structures. Examples of bicycle projects include shared-use trails, paved shoulders, bike lanes, bicycle route signage, bicycle parking, overpasses/underpasses/bridges and sidewalks.

The Bicycle and Pedestrian Facilities Program (BPFP) was merged with the TE program beginning with the 2008 application cycle. As the name implies, BPFP funds bicycle and bicycle/pedestrian facilities. Per state statute, BPFP cannot fund exclusive pedestrian projects or streetscaping projects even if they include some bicycle and pedestrian elements. Because the TE program also extensively funds bicycle and pedestrian facilities, the two programs share the same application, review and selection process.

- Central Office Program Manager: 608-264-8723
- SE Region Program Manager: 262-548-8789

# Congestion Mitigation and Air Quality Program (CMAQ)

The primary purpose of the Congestion Mitigation and Air Quality (CMAQ) Improvement Program is to fund projects and programs that reduce travel and/or emissions in areas that have failed to meet air quality standards for ozone, carbon monoxide (CO), and small particulate matter. Bicycle and pedestrian projects are eligible for CMAQ if they reduce the number of vehicle trips and miles traveled. Approved projects are reimbursable at 80% of the cost, and a local match of 20% is required. Almost all bicycle projects eligible for Transportation Enhancements are likely to be eligible, but a higher burden of proof that the project will reduce air pollution will be required for CMAQ funding. CMAQ is not a statewide program; only bicycle projects in Milwaukee, Kenosha, Racine, Ozaukee, Waukesha, Washington, Sheboygan, Kewaunee, Manitowoc, and Door Counties are eligible.



Shared-use paths should contain traffic control and directional signs where they intersect streets.

# Hazard Safety Improvement Program

Bicycle and pedestrian projects are eligible for this program that focuses on projects intended for locations that have a documented history of previous crashes.

• Contact WisDOT SE Region Traffic Safety Engineer, 262-548-5958, for more details before contacting the statewide coordinator at 608-266-3341.

# Surface Transportation Program - Urban

Metropolitan areas receive an allocation of funds annually. These funds can be used on a variety improvement projects including bicycle and pedestrian projects. Most of the Metropolitan Planning Organizations (MPOs) that administer this program have been using these funds to integrate bicycle and pedestrian projects as larger street reconstruction projects are taken on. SEWRPC is the MPO for Southeast Wisconsin.

Contact SEWRPC, 262-547-6722

# **Recreation-Based Funding Sources**

The following information for potential recreation-based funding sources was culled from the Wisconsin Department of Transportation website.

Funding for the Recreational Trails Program (RTP) is provided through federal gas excise taxes paid on fuel used by off-highway vehicles. Towns, villages, cities, counties, tribal governing bodies, school districts, state agencies, federal agencies and incorporated organizations are eligible to receive reimbursement for development and maintenance of recreational trails and trail-related facilities for both motorized and non-motorized recreational trail uses. Eligible sponsors may be reimbursed for up to 50% of the total project costs.

Eligible projects include:

- Maintenance and restoration of existing trails
- Development and rehabilitation of trailside and trailhead facilities and trail linkages
- Construction of new trails (with certain restrictions on Federal lands)
- Acquisition of easement or property for trails

Wisconsin Department of Natural Resources (DNR) regional staff review and rank eligible projects. Projects are then ranked in a statewide priority listing. The highest ranking projects will be funded to the extent that funds are available.

Following you will find general program information for programs that provide up to 50% funding assistance to acquire land or conservation easements and develop facilities for outdoor recreation purposes – the Stewardship Local Assistance Grant Programs, the Federal Land & Water Conservation Fund Program, and the Federal Recreation Trails Program. Any project application submitted will be considered for each of the following programs that it is eligible for.

Under the Knowles-Nelson Stewardship Local Assistance Grant Program, the following programs provide 50% funding assistance to acquire land and easements and develop trails, facilities, etc. for nature-based outdoor recreation purposes.

# Aids for the Acquisition and Development of Local Parks (ADLP)



ADLP helps to buy land or easements and develop or renovate local park and recreation area facilities (e.g. trails, fishing access, and park support facilities). Applicants compete for funds on a regional basis.

# Urban Green Space Grants (UGS)

UGS helps to buy land or easements in urban or urbanizing areas to preserve the scenic and ecological values of natural open spaces for outdoor recreation, including non-commercial gardening. Applicants compete for funds on a statewide basis.

# Acquisition of Development Rights Grants (ADR)

ADR helps to buy development rights (easements) for the protection of natural, agricultural, or forestry values, that would enhance outdoor recreation. Applicants compete for funds on a statewide basis.

# Land and Water Conservation Fund (LWCF)

LWCF provides 50% funding assistance for the acquisition and development of public outdoor recreation areas and facilities. Similar to the Stewardship ADLP program above except that active outdoor recreation facilities are eligible for grant assistance and school districts may be eligible project sponsors. Applicants compete for funds on a statewide basis.

# **Recreational Trails Act (RTA)**

RTA provides 50% funding assistance for the development and maintenance of recreational trails and trail related facilities for both motorized and non-motorized recreational trail uses. Applicants compete for funds on a statewide basis.

These programs are administered by the Wisconsin Department of Natural Resources. The Stewardship Advisory Council, with representatives from local units of government and nonprofit conservation organizations (NCOs), advises the department on matters relating to the Stewardship program. Similarly the State Trails Council advises the department on matters relating to the Recreational Trails Program. The National Park Service plays the major role in working with the Department on the Land & Water Conservation Fund Program and the Department of Transportation plays a role with the Recreational Trails Program. Key components of the programs are cooperation and partnership between the Wisconsin Department of Natural Resources, the federal government, local units of government and NCOs. The programs recognize the important role each partner plays in meeting the conservation and recreation needs of Wisconsin residents and is designed to assist groups in meeting those needs. The application deadline for all of the programs is May 1 each year and complete applications should be submitted to the regional Community Services Specialist (CSS).

# **Pedestrian Specific Funding**

# Traffic Signing and Marking Enhancement Grants Program

The Traffic Signing and Marking Enhancement Grants Program provides funds to local units of government for the installation of traffic signing and roadway



marking enhancements, with the goal of improving visibility to assist elderly drivers and pedestrians. The program distributed approximately \$3.8 million in state funding in 2005 and 2006. Funding for the TSMEGP was eliminated in the 2007-2009 State Biennial Budget, but continues by Wisconsin State Statute 85.027 through June 30, 2009.

# Other potential funding sources

# **Impact Fees and Dedications**

Many of the paths and trails proposed in this plan are on land that has yet to be developed. Whenever possible, as new development is proposed, impact fees, easements, or dedications should be required to provide paths through the development and connecting to the larger network.

# Additional federal, state and county programs

In addition to the funds administered by the state, funding for public bicycle and pedestrian projects can come from federal highway traffic safety programs, federal traffic safety (section 402) funds, the County (Kenosha County Department of Public Works), impact fees required of new development or redevelopment, public/private partnerships or wholly from the private sector. Additionally, TIF funding and Block Grants can be leveraged as local matching funding for state and federal grants.

Properly designated crosswalks and warning signs can alert motorists to the potential presence of pedestrians.

# 8. Conclusion

Bicycling and walking are inexpensive, non-polluting forms of transportation and recreation that are accessible to nearly everyone. The Village of Pleasant Prairie is proactively planning for these activities to ensure that residents and visitors can safely and efficiently travel throughout the village on foot or bike.

The 2010 Bicycle and Pedestrian Trails Plan calls for a ten-fold increase in the amount of bicycle lanes, shared-use paths and recreational trails in the Pleasant Prairie. When fully implemented, these facilities will create a robust network throughout the Village and will allow users to safely bicycle and walk to specific destinations or simply for exercise or personal enjoyment.

The *2010 Bicycle and Pedestrian Trails Plan* will serve as the guiding document for bicycle and pedestrian facilities as well as encouragement, education and enforcement programs in the years to come. By actively planning for bicycling and walking, the Village demonstrates the importance of these activities. Providing the facilities proposed in this plan will offer low-cost and environmentally friendly transportation and recreation options that are accessible to all residents.

# **Appendix A: Public Meeting**

# Attendees at May 28, 2009, Public Meeting

First	Last	<u>City/Village</u>
Clyde R	Allen	
Karen	Armstrong	Pleasant Prairie
Stan	Armstrong	Pleasant Prairie
Jim	Bandura	Pleasant Prairie
Dennis	Bell	Pleasant Prairie
Brian & Carol	Boehm	Pleasant Prairie
Dale	Brandes	
Mildred M.	Carlson	Kenosha
Sally	Fennema-Janser	1
Cliff	Folkers	Pleasant Prairie
Kate & Tom	Fuller	
Jim	Hart	Pleasant Prairie
Carolyn	Henry	
Peggy	Herrick	Pleasant Prairie
Troy	Holm	Pleasant Prairie
Richard & Carol	Hunt	Pleasant Prairie
Kim	Jensen	Pleasant Prairie
Carla	Knickrehm	Pleasant Prairie
Karen	Kostizke	
Jeff & Dorene	Martin	
Don	Moldenhaner	Kenosha
Mike	Pollocoff	Pleasant Prairie
John	Roscioli	Pleasant Prairie
Greg	Scheppler	Pleasant Prairie
Mike	Schmitz	
Mike	Spence	Pleasant Prairie
John	Steinbrink, Jr.	Pleasant Prairie
Dave	Steck	Pleasant Prairie
Brian	Wagner	Pleasant Prairie
Jean & Jessie	Werbie	Pleasant Prairie

# Public Comments from May 28, 2009, Public Meeting

The following written comments were submitted at the meeting:

- I am interested in putting in bike friendly trails going east-west from Kenosha.
- Please recommend that all major and minor roads, when widened, be widened to the max. Wilmat Road (Hwy C) – many children in the area by Pleasant Prairie School, eventually this road is going to be widened, please create a separate lane like on University Ave in Madison. Cars drive on this road way fast now and will probably be faster once widened.
- A good map that can be carried and passed out would be ideal similar to Racine map.

# **Appendix B: Public Survey Results**

An online survey was available for public input from mid-May 2009 through the end of September 2009 to gauge pubic opinion on bicycle and pedestrian facilities and usage. The survey was advertised in the Village of Pleasant Prairie *The Village* newsletter that is mailed to all Pleasant Prairie households. Additionally, a link to the survey was posted on the official Village website. Because the survey was available online, it was open to anyone who wanted to participate, regardless of if they were a Pleasant Prairie or not.

The survey had a total of 82 participants. The results of the survey are printed below, as are verbatim responses to open-ended questions, although personal information has been removed. Although there were 82 total participants, the number of responses to each question may not sum to 82 as some participants skipped some questions and some questions allowed participants to enter more than one response if appropriate.

# Question 1: How important do you think it is to include bicycle issues in the Village's transportation planning process?

Very Important	Somewhat Important	Somewhat Unimportant	Very Unimportant	No Opinion	Did Not Answer
58	16	0	8	0	0
70.73%	19.51%	0.00%	9.76%	0.00%	0.00%

# **Question 2: Where do you live?**

Village of Pleasant Prairie	Kenosha County, Outside VPP	Outside of Kenosha County	Other	Did Not Answer
64	17	1	0	0
78.05%	20.73%	1.22%	0.00%	0.00%

# Question 3: If you are employed, how far do you live from your primary job?

Less than 1 mile	1-2 miles	2-5 miles	5-10 miles	10+ Miles	l am not employed	Did Not Answer
6	5	11	11	32	16	1
7.32%	6.10%	13.41%	13.41%	39.02%	19.51%	1.22%

# Question 4: Do you walk or jog regularly?

Yes	No	Did Not Answer
74	8	0
90.24%	9.76%	0.00%

# If yes, do you primarily walk/jog (check all that apply)

For Exercise	Personal Enjoyment	Means of Transportation	Year-Round	Did Not Answer
63	53	9	29	2
76.83%	64.63%	10.98%	35.37%	2.44%

# Question 5: Do you have access to a bicycle?

Yes	No	Did Not Answer
80	2	0
97.56%	2.44%	0.00%

# If yes, do you (check all that apply)

Ride Regularly	Personal enjoyment	Exercise	Means of Transportation	Ride Year- Round	Did Not Answer
39	63	53	23	10	2
47.56%	76.83%	64.63%	28.05%	12.20%	2.44%

# Question 6: What's the longest distance you would consider riding a bicycle?

0-1 Mile	1-5 Miles	5-10 Miles	10+ Miles	Did Not Answer
1	9	16	53	1
1.25%	11.25%	20.00%	66.25%	1.25%

# Question 7: If you have school-age children in your household, do they regularly walk or bike to school?

Yes	No	Not Applicable	Did Not Answer
4	35	39	0
5.13%	44.87%	50.00%	0.00%

# If you answered no, why not?

- 1. Not allowed; have to ride bus or be driven
- 2. KUSD schools are too far away
- 3. Rural
- 4. Prairie Lane Elementary discourages it because there are no sidewalks

- 5. School won't allow it
- 6. They attend Prairie Lane; it is not allowed as there is not access
- 7. Teen
- 8. I don't want them riding their bikes on Sheridan Rd
- 9. No safe route to school
- 10. No sidewalks, cars drive too fast down Cooper Rd
- 11. Too far away, unsafe roads to ride
- 12. Speeders on 7th Ave. Potholes on 91st St. Dangerous cross at Sheridan/91ST
- 13. Because it is too far and roads don't have sidewalks!
- 14. not safe
- 15. No sidewalks, dangerous road to walk 85th Street
- 16. 7 miles and 25 miles from home; no safe route either
- 17. Too far away, busy roads
- 18. We drive them
- 19. Crossing Springbrook Road
- 20. Too far
- 21. Roadway to school is dangerous even though bike route (91 St into 7 Ave)

<b>Question 8:</b>	Do you feel	that Pleasant	Prairie is	pedestrian	friendly?
--------------------	-------------	---------------	------------	------------	-----------

Yes	No	Did Not Answer
7	70	3
8.75%	87.50%	3.75%

# If no, why?

Personal Safety Concerns	Bad Weather	Automobile Traffic	Bad Driver Behaviors	Destinations are too far away	No Sidewalks	Unsafe Intersections
35	4	50	24	25	49	27
43.75%	5.00%	62.50%	30.00%	31.25%	61.25%	33.75%

# Other Responses (12):

- 1. No shoulder on the road
- 2. Rural Community
- 3. No shoulder to walk on
- 4. Roadways unsafe
- 5. Need sidewalk Carol Beach
- 6. No shoulders to walk on
- 7. No police enforcement
- 8. Poor roads, no bike lanes
- 9. No accommodations
- 10. Narrow roads no bike lanes
- 11. RecPlex is only destination
- 12. Other walking paths to lake

# Question 9: Do you feel that Pleasant Prairie is bicycle-friendly?

Yes	No	Did Not Answer
-----	----	-------------------

11	69	0
13.75%	86.25%	0.00%

### If no, why?

Personal Safety Concerns	Bad Weather	Automobile Traffic	Bad Driver Behaviors	Destinations are too far away	No Bike Lanes	Unsafe Intersections
38	4	46	26	11	60	26
47.50%	5.00%	57.50%	32.50%	13.75%	75.00%	32.50%

# Other Responses (11);

- 1. Rural Community
- 2. Poor road conditions 116th St.
- 3. Road condition 93rd, 116th
- 4. Rough roads
- 5. No police enforcement
- 6. Some areas are good
- 7. Little or no shoulder
- 8. Only on bike trail
- 9. It depends on which part of Pleasant Prairie you are talking about. For instance 85th Street between Cooper Road and 39th Avenue is great because it provides a large paved shoulder on each side. This is great to encourage non-motorized travel. Then there is the stretch of 7th Avenue between 75th Street and 91st Street, with its narrow bike lane to be shared by both north and southbound travelers. While I do like striped bike lanes on roadways as a way to bring bicycles on the road into the mind of car drivers, this stretch is not only unsafe, it perpetuates a myth that it is best to ride a bicycle against traffic. I will save space by not listing the many reasons why riding a bicycle against traffic is not safe since you are working with the Bike Fed they can educate the Village on the many reasons why bicycling against traffic is unsafe. This stretch of striped bike lane is also unsafe because it is not wide enough to accommodate two bicycles traveling in opposite directions, put two cars driving in opposite directions at the same time and you have a recipe for disaster.
- 10. No shoulders on Cooper, 85th, 92nd, etc
- 11. Narrow bike lanes

Parks/ Recreation Trails	Shopping Areas/ Supermarket	Schools	Downtown Kenosha	Work	Doctor's Offices/ Hospitals	Did Not Answer
79	43	20	46	13	16	1
96.34%	52.44%	24.39%	56.10%	15.85%	19.51%	1.22%

# Question 10: What places in Pleasant Prairie would you like to be able to reach by bike or foot?

# Other Responses (10):

- 1. Restaurants
- 2. RecPlex
- 3. Bike trails, RecPlex
- 4. Restaurant/café
- 5. Carol Beach area
- 6. North Point Marina
- 7. I-94 and west
- 8. Lake County, Des Plaines River trail
- 9. Lakefront
- 10. Lakefront

# Question 11: How would the factors below affect your decision to bicycle or walk?

# More on-street bicycle facilities (bike lanes, paved shoulders, wide travel

lanes, etc.)

No Affect	Moderately Affect	Strongly Affect
4	13	60
5.19%	16.88%	77.92%

# More sidewalks

No Affect	Moderately Affect	Strongly Affect
17	26	31
22.97%	35.14%	41.89%

# More greenway trails

No Affect	Moderately Affect	Strongly Affect
1	29	47
1.30%	37.66%	61.04%

# More bicycle parking

No Affect	Moderately Affect	Strongly Affect
18	39	20
23.38%	50.65%	25.97%

# Increased enforcement of laws applying to motorists and bicyclists

No Affect	Moderately Affect	Strongly Affect
12	35	30
15.58%	45.45%	38.96%

# Education programs for bicycle safety

No Affect	Moderately Affect	Strongly Affect
27	32	19
34.62%	41.03%	24.36%

# A map of bicycle facilities for planning routes

No Affect	Moderately Affect	Strongly Affect
6	29	41
7.89%	38.16%	53.95%

# Question 12: Where do you feel comfortable bicycling?

# Streets containing bike lanes

Yes	No
66	12
84.62%	15.38%

Streets signed as designated bike routes

Yes	No
51	27
65.38%	34.62%

# Low traffic neighborhood streets

Yes	No
78	1
98.73%	1.27%

# Main Village thoroughfares

Yes	No
14	63
18.18%	81.82%

# **Rural thoroughfares**

Yes	No
41	36
53.25%	46.75%

# **Greenway trails**

Yes	No
71	8
89.87%	10.13%

Question 13: Whenever possible, the Village will seek grants for development of bike routes and trails. Would you be willing to support this endeavor using tax dollars?

Yes	No	Did Not Answer
72	6	2
90.00%	7.50%	2.50%

# Question 14: Any additional concerns or comments?

- 1. Huge savings and pollution abatement if kids had a safe way to ride or walk to schools
- 2. The trails in the park areas should be routed to keep the hiker/biker in natural surroundings. They should not be near any houses or manmade structures where possible without a natural barrier between them
- 3. The Des Plaines River trail is great but we need to utilize biking/walking to replacing "driving" everywhere we need to go.
- 4. I feel this is an awesome idea. I feel that Cooper Rd. is especially a horrible place for walking or riding bikes and you see many adults and children walking and riding to school every morning down this road. I would love to see this issue addressed. I would most definitely vote yes to spend tax dollars on this endeavor!!
- 5. It would be terrific if Pleasant Prairie could connect with the Des Plaines River trail that ends just across the border in Illinois. Good luck.
- 6. It is unsafe to ride on Village Streets and I will never allow my children to ride on Village Streets either. Want to make an impact on the Village? Build a train station.
- 7. I would love to see a bike lane or trail on 116th Street. I use that street quite often to bike from Carol Beach out to the industrial park and it is very bumpy and very narrow without shoulders.
- 8. Thanks for fixing 93rd Street... however 116th Street is a major E/W street that crosses the bike path and the road is horrific to bike due to cracks and bumps especially going east from Old Green Bay Road.
- 9. Poorly maintained roads (especially along the triathlon route) make training more difficult.
- 10. Pleasant Prairie has a great chance to change the roads and make the bike path more reachable for families to travel to safely.
- 11. That would be great to have bike lanes, sidewalks and more trails.
- 12. Some street conditions are rough. Particularly 116th Street. 116th is a main road I would like to use to get to the current trails, however it is just too rough so I end up driving my car and unloading out bikes to avoid 116th Street
- 13. I regularly bike the triathlon route and 93rd Street is in extremely poor condition for bikes between Cooper and Green Bay Rd. It is dangerous the road is in such poor condition.
- 14. 116th Street and especially 93rd Street are bad roads for biking. Need road improvement and bike lanes! Thanks!
- 15. Streets containing bike lanes that I've seen have only one lane so all traffic going in both directions use this lane it does not seem safe and the lane is narrow. Also in Pleasant Prairie where I live, you have to use Sheridan to access any other roads/trails in the community. Sheridan is very busy, drivers travel very fast and intersections are difficult/unsafe for turning. It is tough turning onto our road (101st) in a car, let alone a bike. I believe adding bike lanes and making the roads safe for bikers and pedestrians would greatly enhance the appeal of living in Pleasant Prairie. Thanks for this opportunity.
- 16. I frequently ride 10-15 miles per day, 4-5 days per week and I frequently ride on the bike trail from 89th Street south to the IL border and further south. It is not in as good a condition as in IL. There are spots were ruts are deep and gravel is loose. I also do not feel it is safe to ride alone on the bike trail,

especially after dark. If you have more questions please feel free to contact me: [omitted].

- 17. The Village has always held an "edge" in exercise/fitness needs for individuals and families. This project would continue that tradition and support the "green" needs of the future.
- 18. Present bike trails need to be maintained, large gravel or stone should not be used especially at street crossings and access to street crossing should not be inclined.
- 19. I currently bike at times when traffic is low to minimize safety issues. I would like to bike to work, but traffic is worst during commuting hours. Please consider providing more bike trails such as the Kenosha Bike trail and Des Plaines River trail. These trails eliminate most of the encounters between bikes and cars, providing a much safer route for commuting.
- 20. Having a bike trail from Prairie Ridge subdivision to RecPlex center instead of hwy H
- 21. One of the most popular and scenic areas of our town is Lakeshore Drive. Many people walk, jog, bicycle and push baby strollers down this road. It is dangerous to do these activities on this road regardless of automobile speeds because there is just not enough room for both autos and pedestrians to safely use this road. Dedicated paths are needed along Lakeshore Drive and the surrounding Chiwaukee Prairie areas in order for the citizens to more fully utilize and to safely access these areas.
- 22. I live on Cooper Road, not far from Whittier School. I would love to see sidewalks or something that my child could use when she goes to middle school next year at Lance. Currently, I would not allow her to ride her bike or walk to school. Even though the speed limit is 30 all the way down Cooper, drivers constantly exceed the speed limit. I'd love to see the road widened, bike lanes and /or sidewalks put in, but that could also encourage drivers to go even faster. I think enforcement of the speed limits in the Village would help a great deal.
- 23. I do enjoy the Kenosha bike trail and the path from Southport Park through Kenosha. There are some great bike paths available. I would like to see more so that we are encouraged to bike and walk to destinations.
- 24. The current location of the winter snowmobile trails on Hwy 165 would be an excellent place for a paved bike trail with connections to Prairie Springs Park and to the gravel trail east of 39th Avenue.
- 25. A paved trail would be nice to have. My 5-year old child slipped on the gravel on the Kenosha County bike trail last fall and was injured. The dirt trail is not compatible with road bikes and makes bikes and riders very dirty. Lack of neighborhood sidewalks for safe walking is also a concern. We often take evening walks between Springbrook Meadows and Village Green subdivisions. The busier streets (like Cooper Rd) are not safe for strollers and small kids on bikes and scooters. Finally, a paved trail linking Lake Andrea to residential neighborhoods to the East of Green Bay Road would be used heavily. Currently, riding on Hwy 165 is the only option, clearly not safe for children. A trail from the Kenosha County Bike trail to the Lake Andrea trail would provide access to the lake for users of this widely used trail.
- 26. I'd focus less on public safety/law enforcement issues as it relates to cycling/walking. The number of cyclists I see pass in front of my house tells me that the problem isn't one of unsafe conditions or safety but, rather, the need to

better connect Village parks, transit areas, etc. with potential pathways. Don't focus on nanny-state politics, just look at how you can make efficient use of our tax dollars to make this a more navigable village.

- 27. As a Carol Beach resident I am strongly interested in sidewalks for walking alone and with my children. With the many bicycles we get in the area in addition to automobile traffic it has become very unsafe to walk with children or with strollers. Vehicles also drive VERY fast especially down Lakeshore Dr. They take the curves at high speeds and you cannot react once you see the car coming at a high speed. I have not taken my children for many walks in the neighborhood because of the vehicle and bike traffic. Sidewalk would be a fantastic addition for the residents here and would significantly improve the safety of the area. [Name and address omitted].
- 28. There is no police presence on Lakeshore Drive. It's a drag way for speeders from 90th Street to the south. Everyone is driving way too fast on this street: residents, school buses, UPS, Fed Ex, Pat's Sanitation all drive at dangerous speeds.
- 29. Pleasant Prairie needs to connect better to Kenosha especially with Main Street (39th) via a well marked bike lane. The bike path is not safe for female individuals biking alone.
- 30. We'd like to see more paved or fine gravel trails in natural settings, similar to the abandoned railroad trails in the state (Elroy-Sparta trail, Red Cedar trail etc) A trail in the northwest corner of Pleasant Prairie would be great!
- 31. Let's Do It!
- 32. Both "Main" and "Rural" thoroughfares should be marked No. Is it possible to have bike lanes marked on Hwy C from 50 to I94. Traffic is getting heavy and the new interchanges worry me. Also, I stay off of H during much of the day due to traffic from the industrial park, especially between 50 and 165. I really miss not going past John Steinbrink's house as much as I used to. Bain Station Road could also use shoulders, especially between H and C. Thanks for the opportunity to voice my opinion.
- 33. Let's get bike friendly!
- 34. It's obvious that the time for more bike/walking is upon us. For the environment and economy.
- 35. Good Idea. Keep putting 3' paved shoulder on all new roads and resurface roads.
- 36. I commend you for taking the initiative to look into improving our community
- 37. Paint crossing lines on the roads where the bike trails cross. The openings are not clearly marked and hard to judge the distance in a car.
- 38. I would most like to see the 7th Avenue stretch corrected. What bothers me most about it is the way it is telling people that it is safe to ride a bicycle against traffic when this is wrong and very dangerous. I believe what the Village of Pleasant Prairie is doing by asking for input and more importantly working with the bicycle federation of Wisconsin is fantastic. I am very optimistic that these steps are going to go a long way to help the Village of Pleasant Prairie become a great place to live, work and visit.
- 39. The village needs a safe East-West bicycle corridor to connect RecPlex with the lakefront. 93rd St sees a lot of bicycle traffic, but is EXTREMELY unsafe (narrow, bumpy road, no shoulders, dangerous intersection at CR ML
- 40. We live in Prairie Village condos and would like to be able to get to Lake Andrea, but 93rd Street is just to narrow with most traffic seeming to go faster than the

posted speed limit. When I'm driving my car through there at 35-40 mph, I often have a car or truck running up behind me. Then too, to go east over to the bike trail on 93rd Street from 39th Ave, it'll rattle the fillings out of your teeth on a bike or car.

41. Carol Beach Lake Shore Drive has a bike trail that would that could use a little more widening. Its very appealing to everyone who has walked or biked along the lakeshore.

## **Appendix C: Rules for Bicycling on the Road**

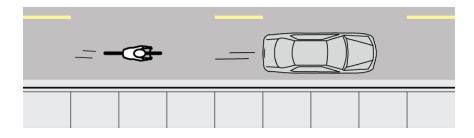
The following information is drawn from the Wisconsin Department of Transportation Safety and Consumer Protection webpage. <u>http://www.dot.state.wi.us/safety/vehicle/bicycle/rules.htm</u>

## **General rules**

- Bicycles are vehicles. They belong on the road.
- Ride at least three feet from the curb or parked vehicles.
- Ride in a straight line. Don't swerve in and out around parked vehicles.
- Always ride in the same direction as traffic.
- Sidewalk riding for bicyclists past the learning stage can be more dangerous than on the road, obeying traffic laws. It is also illegal in most communities.
- Obey all traffic laws.
- Be predictable! Let other users know where you intend to go and maintain an understood course.

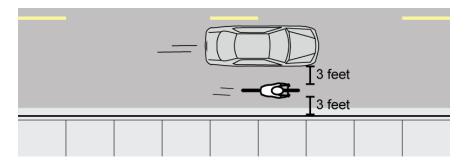
## **Narrow lanes**

- Ride in the center of the lane.
- Keep at least three feet between yourself and passing or parked traffic.



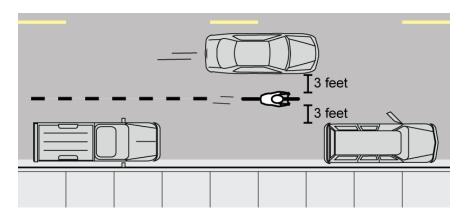
## Wide lanes

- Ride just to the right of the actual traffic line, not alongside the curb.
- Keep at least three feet between yourself and the curb or parked vehicles.
- Motorists should be passing you with at least 3 feet of clearance.

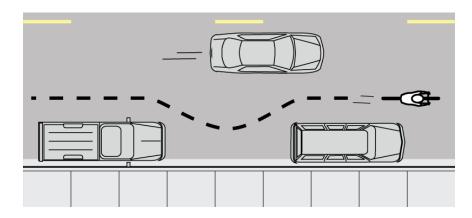


## Don't get doored!

Ride in a straight line three feet out from parked cars. You'll avoid car doors that open in front of you and you'll be more visible to other drivers.



• Don't pull into the space between parked cars. Ride just to the right of the actual traffic line, not alongside the curb.



## Take the lane

You will fare better with other road users if you function like a legal vehicle operator, which you are.

- Right turning motorists can be a problem, but taking the lane or more of the right portion of the wide curb lane can prevent this. Take an adult bicycling course to learn skills and develop confidence in traffic.
- Left turning motorists are the cause of most adult bicyclists' crashes. Motorists claim not to see the cyclist who is traveling in a straight path in the opposite direction.

Bicyclists, when making your own left turn look over your left shoulder for traffic, signal your left turn and change lanes smoothly, so you are to the left side or center of the through lane by the time you reach the intersection. If a left turn lane is present, make a lane change to center of that lane. Do not move to left of that lane as left-turning motorists may cut you off.

• Do not wait until you reach the crosswalk, then stop and try to ride from a stop across other traffic. If you need to cross as a pedestrian, leave the

travel lanes, then get into the crosswalk, walking or riding your bicycle like a pedestrian travels, not fast, and with pedestrian signals.

Lane positioning can be especially important in approaching a downhill intersection. Moving to the center makes you more visible to intersecting and left turning motorists in opposing lanes.

- Going downhill, your speed is likely to be closer to traffic speeds or posted speed limits. Hugging the curb when there are visual barriers increases your chances of being struck by a bigger vehicle, or of hitting a pedestrian or sidewalk riding bicyclist.
- Take the lane, be seen and see other traffic better if you are close to traffic speeds

## How to ride

Wear bright colors during the day and retro-reflective items at night along with headlight and taillight to increase your visibility to other road users.

 Wear a bicycle helmet on every ride to reduce your chance of head injury in event of a fall or crash. Most serious injuries from a fall or crash are to the head and most frequently, the forehead, so wear helmet level with the ground, just above the eyebrows.

Be aware of changing road surfaces, new construction or unusual barriers on the roadway, distracters for both you and other vehicle operators.

 Leaves can be slippery in the early morning and are a hazard even when slightly damp. Distractions such as dogs, wild animals and even humans can draw attention from the roadway and lead to a crash. Expect them.

## **Motorist reminders**

- Bicycles are vehicles. They belong on the road.
- Cyclists need room to get around potholes, sewer grates and other obstructions.
- Leave at least three feet when passing bicycles, more room at higher speeds.
- Change lanes to pass any bicycle traveling in a narrow lane.
- Train yourself to scan for fast moving (it's hard to tell speed) bicycles and motorcycles in the opposing lane to you when turning left, and scan sidewalks and crosswalks for pedestrians and bicyclists using the sidewalk and crosswalk as a pedestrian. Always scan to your right side sidewalk before you leave a stop light or stop sign. And to the left and right side sidewalks when on a one-way street.

## **Appendix D: Wisconsin State Bicycle Laws**

The information below is a summary of current Wisconsin State Laws relating to bicycling. The numbers in brackets refer to the specific state statute; for the complete statutes, consult the Wisconsin Department of Transportation Summary of Wisconsin Bicycle Laws.

http://www.dot.state.wi.us/safety/vehicle/bicycle/docs/bikelaws.pdf

## A. Vehicular Status

- The bicycle is defined as a vehicle. [340.01(5)]
- The operator of a vehicle is granted the same rights and subject to the same duties as the driver of any other vehicle. [346.02(4)(a)]

## **B.** Lane Positioning

- Always ride on the right, in the same direction as other traffic.
   [346.80(2)(a)]
- Ride as far to the right as is practicable (not as far right as possible).
   [346.80(2)(a)]
- Practicable generally means safe and reasonable. 346.80(2)(a) lists a few situations when it is not practicable to ride far to the right:
- When overtaking and passing another vehicle traveling in the same direction;
- When preparing for a left turn at an inter-section or driveway;
- When reasonably necessary to avoid unsafe conditions, including fixed or moving objects, parked or moving vehicles, pedestrians, animals, surface hazards or substandard width lanes [defined as a lane that is too narrow for a bicycle and a motor vehicle to travel safely side by side within the lane].

## C. One Way Streets

 Bicycles on a one-way street with 2 or more lanes of traffic may ride as near the left or right-hand edge or curb of the roadway as practicable (in the same direction as other traffic). [346.80(2)(b)]

## D. Use of Shoulders

• Bicycles may be ridden on the shoulder of a highway unless prohibited by local authorities. [386.04(1m)]

## E. Riding 2-Abreast

 Riding 2 abreast is permitted on any street as long as other traffic is not impeded. When riding 2 abreast on a 2 or more lane roadway, you both have to ride within a single lane. [346.80(3)(a)]

## F. Hand Signals

- Bicyclists are required to use the same hand signals as motorists [346.35].
- Hand signals are required within 50 feet of your turn. It is not required continuously if you need both hands to control the bicycle [346.34(1)(b)]

## G. Passing

- A motorist passing a bicyclist in the same lane is required to give the bicyclist at least 3 feet of clearance, and to maintain that clearance until safely past. [346.075]
- A bicyclist passing a stopped or moving vehicle is also required to give at least 3 feet of clearance when passing. [346.80(2)(c)]

## H. Use of Sidewalks

- State Statutes allow local units of government to permit vehicles on sidewalks through local ordinances. [346.94(1)]
- When bicycles are allowed to be operated on sidewalks, bicyclists must yield to pedestrians and give an audible warning when passing pedestrians traveling in the same direction. [346.804]
- At intersections and other sidewalk crossings (alleys, driveways), a bicyclist on the sidewalk has the same rights and duties as pedestrians. [346.23, 24, 25, 37, 38]

## I. Bicycling at Night

Bicycling at night requires at least a white front headlight and a red rear reflector. The white front light must be visible to others 500 feet away. The red rear reflector must be visible to others between 50 and 500 feet away. A red or amber steady or flashing rear light may be used in addition to the required reflector. These are required no matter where you ride--street, path or sidewalk. [347.489(1)]

### J. Duty to report accident

- The operator of a vehicle involved in an accident resulting in injury to or death of any person, or total damage to property owned by any one person of \$1,000 or more shall immediately give notice of such accident to the police. [346.70]
- "Injury" means injury to a person of a physical nature resulting in death or the need of first aid or attention by a physician or surgeon, whether or not first aid or medical or surgical treatment was actually received;
- "Total damage to property owned by one person" means the sum total cost of putting the property damaged in the condition it was before the accident, or the sum total cost of replacing such property.
- This section does not apply to accidents involving only vehicles propelled by human power.

Consider **Zoning Text Amendments (Ord. #13-17)** to amend Section 420-41 A and C of the Village Zoning Ordinance to clarify requirements of a home occupation permit.

### **Recommendation:**

On May 13, 2013 the Plan Commission held a public hearing and recommended that the Village Board approve the **Ordinance Amendment (Ord. #13-17)** as presented.

## VILLAGE STAFF REPORT OF MAY 20, 2013

Consider **Zoning Text Amendments (Ord. #13-17)** to amend Section 420-41 A and C of the Village Zoning Ordinance to clarify requirements of a home occupation permit.

On Mach 18, 2013 the Village Board adopted Resolution #13-08 to initiate amendments to the Village Zoning Ordinance to clarify that a home occupation is allowed in both the agricultural and residential districts for homeowners or occupants who reside full time at the home wherein they are requesting a home occupation permit pursuant to Article VII of the Chapter 420 of the Village Zoning Ordinance.

The following amendments (bolded and highlighted in yellow) are proposed to section 420-41 A and C of the Village Zoning Ordinance:

A. The home occupation shall be carried out by the property owner or occupant at their place of primary residence and said home occupation use be clearly incidental to the residential use of the principal or accessory building and parcel and shall not change the essential residential character of the dwelling and parcel.

## "Primary Residence" shall be defined as follows:

- (1) The place where the owner's or occupant's habitation is fixed, without any present intent to move, and to which, when absent, the person intends to return.
- (2) The primary residence of a person performing the home occupation is the main home that the person lives in for a majority of the year as defined by the Internal Revenue Service. A person shall not have more than one primary residence for the purposes of operating a home occupation.
- (3) When a person's spouse and/or family resides at one place and the other spouse's business is conducted at another place, the former place establishes the place of primary residence for the purpose of determining whether a home occupation may be conducted at the requested premises.
- (4) No person gains the right to conduct a home occupation at a residence while living there for temporary purposes only.
- C. No more than 25% of the principal building or the accessory building area shall be used to conduct the home occupation. Further, there shall be no more than two home occupations conducted from the same premises provided that the combined home occupation usage does not exceed 25% of either the principal or accessory buildings on the premises and all other home occupation requirements as set forth in this section are met.

On May 13, 2013 the Plan Commission held a public hearing and recommended that the Village Board approve the **Ordinance Amendment (Ord. #13-17)** as presented.

### ORD. NO. 13-17

#### ORDINANCE TO AMEND THE VILLAGE ZONING ORDINANCE (CHAPTER 420) TO CLARIFY THE HOME OCCUPATION REQUIREMENTS IN THE VILLAGE OF PLEASANT PRAIRIE, KENOSHA COUNTY, WISCONSIN

THE VILLAGE BOARD OF TRUSTEES OF THE VILLAGE OF PLEASANT PRAIRIE, KENOSHA COUNTY, WISCONSIN, DOES HEREBY ORDAIN THAT SECTION 420-41 A AND C ARE HEREBY AMENDED TO READ AS FOLLOWS:

A. The home occupation shall be carried out by the property owner or occupant at their place of primary residence and said home occupation use be clearly incidental to the residential use of the principal or accessory building and parcel and shall not change the essential residential character of the dwelling and parcel.

### "Primary Residence" shall be defined as follows:

- (1) The place where the owner's or occupant's habitation is fixed, without any present intent to move, and to which, when absent, the person intends to return.
- (2) The primary residence of a person performing the home occupation is the main home that the person lives in for a majority of the year as defined by the Internal Revenue Service. A person shall not have more than one primary residence for the purposes of operating a home occupation.
- (3) When a person's spouse and/or family resides at one place and the other spouse's business is conducted at another place, the former place establishes the place of primary residence for the purpose of determining whether a home occupation may be conducted at the requested premises.
- (4) No person gains the right to conduct a home occupation at a residence while living there for temporary purposes only.
- C. No more than 25% of the principal building or the accessory building area shall be used to conduct the home occupation. Further, there shall be no more than two home occupations conducted from the same premises provided that the combined home occupation usage does not exceed 25% of either the principal or accessory buildings on the premises and all other home occupation requirements as set forth in this section are met.

Adopted this 20<sup>th</sup> day of May, 2013.

VILLAGE OF PLEASANT PRAIRIE

ATTEST:

John P. Steinbrink Village President

Jane M. Romanowski Village Clerk

Posted: \_\_\_\_\_

### THESE ITEMS ARE RELATED AND WILL BE DISCUSSED AT THE SAME TIME HOWEVER SEPARATE ACTION IS REQUIRED.

Consider **Resolution #13-15 for a Floodplain Boundary Adjustment** for the request of Adam Artz, P.E. with Pinnacle Engineering Group, agent for Majestic Realty Co. owner of the vacant 91 acre property on the east side of 88<sup>th</sup> Avenue south of Bain Station Road to remove 98,173 cubic feet of floodplain and to create 99,144 cubic feet of floodplain to compensate for the floodplain being filled.

**<u>Recommendation</u>**: Village staff recommends that the Village Board approve the **Floodplain Boundary Adjustment (Res. #13-11)** subject to the comments and conditions of the Village Staff Report of May 20, 2013.

Consider **Zoning Text and Map Amendments (Ord. #13-18 and #13-19)** for the request of Adam Artz, P.E. with Pinnacle Engineering Group, agent for Majestic Realty Co. owner of the vacant 91 acre property generally located approximately <sup>1</sup>/<sub>4</sub> mile south of Bain Station Road on the east side of 88<sup>th</sup> Avenue for the proposed 1.2 million square foot warehouse and distribution facility to be known as Majestic Badger Logistic Center for a **Zoning Map Amendment** to add a PUD Planned Unit Development Overlay District on the property (the existing underlying zoning districts will remain); and a **Zoning Text Amendment** to create the specific PUD District zoning regulations for the proposed development of said property.

**<u>Recommendation</u>**: Village staff recommends that the Plan Commission send a favorable recommendation to the Village Board to approve the **Zoning Text and Map Amendments** (Ord. #13-18 and #13-19) as presented in the Village Staff Report of May 20, 2013.

Consider the request of John Burroughs, agent for Majestic Realty, Co. owner of the vacant 91 acre property generally located approximately ¼ mile south of Bain Station Road on the east side of 88<sup>th</sup> Avenue for approval of a **Certified Survey Map** for the proposed 1.2 million square foot warehouse and distribution facility to be known as Majestic Badger Logistic Center.

**<u>Recommendation</u>**: Plan Commission recommends that the Village Board approve the **Certified Survey Map** as presented in the Village Staff Report of May 20, 2013.

## VILLAGE STAFF REPORT OF MAY 20, 2013

Consider **Resolution #13-15 for a Floodplain Boundary Adjustment** for the request of Adam Artz, P.E. with Pinnacle Engineering Group, agent for Majestic Realty Co. owner of the vacant 91 acre property on the east side of 88<sup>th</sup> Avenue south of Bain Station Road to remove 98,173 cubic feet of floodplain and to create 99,144 cubic feet of floodplain to compensate for the floodplain being filled.

Consider **Zoning Text and Map Amendments (Ord. #13-18 and #13-19)** for the request of Adam Artz, P.E. with Pinnacle Engineering Group, agent for Majestic Realty Co. owner of the vacant 91 acre property generally located approximately ¼ mile south of Bain Station Road on the east side of 88<sup>th</sup> Avenue for the proposed 1.2 million square foot warehouse and distribution facility to be known as Majestic Badger Logistic Center for a **Zoning Map Amendment** to add a PUD Planned Unit Development Overlay District on the property (the existing underlying zoning districts will remain); and a **Zoning Text Amendment** to create the specific PUD District zoning regulations for the proposed development of said property.

Consider the request of John Burroughs, agent for Majestic Realty, Co. owner of the vacant 91 acre property generally located approximately <sup>1</sup>/<sub>4</sub> mile south of Bain Station Road on the east side of 88<sup>th</sup> Avenue for approval of a **Certified Survey Map** for the proposed 1.2 million square foot warehouse and distribution facility to be known as Majestic Badger Logistic Center.

## THESE ITEMS ARE RELATED AND WILL BE DISCUSSED AT THE SAME TIME HOWEVER SEPARATE ACTION IS REQUIRED.

The petitioner is requesting several approvals for the development of approximately 91 acres of land generally located approximately ¼ mile south of Bain Station Road on the east side of 88<sup>th</sup> Avenue for a 1.2 million square foot warehouse/distribution facility to be known as Majestic Badger Logistic Center, which would be generally located on the east side of 88<sup>th</sup> Avenue and south of Bain Station Road.

The Village had been working with the developer for several months and to date the following approvals have been obtained for this project:

- October 15, 2012 the Village of Pleasant Board of Trustees approved the following requests related to the development of the property generally located south of Bain Station Road on the east side of 88<sup>th</sup> Avenue (Tax Parcel Number 92-4-122-162-0301) in the Village of Pleasant Prairie.
  - Village Comprehensive Plan (Ord. #12-33) to amend the Village of Pleasant Prairie 2035 Comprehensive Land Use Plan Map 9.9 and to amend a portion of the Pleasant Farms Neighborhood Plan.
  - **Conceptual Plan** was conditionally approved by the Village Board on October 15, 2012 for the development of a proposed 1.2 million square foot warehouse/distribution facility building to be known as Majestic Center.
  - **Zoning Text and Map Amendments** (Ord. #12-34) to rezone a portion of the property that is zoned A-2, General Agricultural District into the M-2, General Manufacturing District. Portions of the property within the 100-year floodplain that are zoned FPO, Floodplain Overlay District and C-1, Lowland Resource Conservancy District will remain unchanged at this time. [*Note:* Upon the completion of detailed wetland field delineation and the 100-year

floodplain field survey (including floodplain boundary adjustments) additional amendments to the 2035 Comprehensive Land Use Plan and Zoning Map will be required.]

2. **Site and Operational Plans** were conditionally approved by the Plan Commission on May 13, 2013 for the development of the property with a 1.2 million square foot warehouse/distribution center to be known as Majestic Bager Logistic Center.

At this time the petitioners are requesting approval of **Floodplain Boundary Adjustment**, a **Certified Survey Map** and a **Zoning Text and Map Amendment** for the construction of a 1,180,480 square foot warehouse/distribution center on the 91 acre site generally located approximately ¼ mile south of Bain Station Road on the east side of 88<sup>th</sup> Avenue. Over 35% of the site will remain as open space which includes landscaped areas around the perimeter of the building/parking and other areas used for stormwater management facilities and the preservation of the 100-year floodplain (after amended) and wetlands.

At this time, there is no defined user for the building; however the owners are requesting Final Site and Operational Plans with anticipation that the building will commence construction later this summer. [NOTE: any tenant that proposes to use/occupy 50% or more of a speculative building will also require Site and Operation Plan approval from the Plan Commission. Also, depending on the use proposed, the tenants may also require a Conditional Use Permit/Site and Operational Plan approval from the Plan Commission].

The site has been designed so that the building can be easily divided into as many as four tenants. The building design is a "cross dock" facility with trailer storage as indicated on the plans. The design provides for "secured" truck courts in the event that future tenants require that feature. Majestic has coordinated closely with the Village to provide ongoing access to the Village's sanitary sewer line that runs through the site in a Village easement. Landscaping has been designed to provide screening where appropriate. The project also includes minor floodplain boundary adjustment to facilitate the development of the site.

<u>Wetlands</u>: The wetlands were field delineated on the site in 2008 by Wetland and Waterway Consulting as approved by the WI DNR on December 19, 2008. The wetland areas are shown on the CSM within Wetland Preservation and Protection, Access and Maintenance Easement. Legal descriptions of the WI DNR approved wetland delineations shall be shown on the CSM.

<u>Shorelands:</u> Jerome Creek is located adjacent to the property. This Creek has been determined to be a navigable waterway. The location of the Ordinary High Water Mark (OHWM) shall be field identified by the WI DNR and shown on the plans and the CSM with a legal description. The Plans shall also show the location of the 75 foot OHWM setback and the 300 foot shoreland jurisdictional area. Any work within 75 feet of the OHWM will require approval of a Stipulated Shoreland Permit and any work within 300 feet may require additional permits from the WI DNR.

<u>Floodplains</u>: The location of the 100-year floodplain associated with Jerome Creek shall be field delineated pursuant to the DFIRM Map panel 191D dated June 19, 2012 and the associated table for the Jerome Creek which identifies the 100-year floodplain elevations. As noted above, the Floodplain Boundary Adjustment was conditionally approved by the Plan Commission on April 8, 2013 (*approval from the DNR is still pending*). The floodplain boundary adjustment proposes to remove approximately 98,173 cubic feet of floodplain and to create approximately 99,144 cubic feet of floodplain to compensate for the floodplain being filled. The 100-year floodplain area after the amendment will be shown in a Floodplain Preservation and Protection, Access and Maintenance Easement on the CSM.

<u>Woodlands</u>: A tree survey has been completed along the north property lines and will be preserved. The woodland areas to be protected will be located within a Woodland Preservation and Protection, Access and Maintenance Easement on the CSM. The grading and berming along the north property line east of the cemetery will also be done in such a way as to protect the drip line of the trees.

<u>Site Access and Parking</u>: A Traffic Impact Analysis (TIA) has been prepared by the Developer's Consultant and reviewed by the Village and Kenosha County. A copy of the Consultant's recommendations indicates that both deceleration/acceleration lanes and by-pass lanes should be installed by the Majestic Developer. In addition, the Village Engineering Department is completing a Transportation Improvement Analysis (TIA) to address the Village's transportation system for the roadways and intersections located in proximity to the Majestic development site. The Majestic Development will be required to pay for their cost share of the intersection improvements at CTH H and 95<sup>th</sup> Street and at CTH H and Bain Station Road. The Village TIA for the area transportation system is anticipated to be completed this spring.

Employee, client, visitor vehicular and secondary truck access to the site will be from two (2) driveways on 88<sup>th</sup> Avenue. The northern entrance will be directly south of the cemetery. The southern driveway will be located north of the proposed retention facility approximately 900 feet north of the south property line.

All parking areas and maneuvering lanes, fire lanes including the truck court, shall be improved with concrete vertical curb and gutter. The plan includes 283 automobile parking spaces, 22 handicapped accessible parking spaces and 461 trailer spaces. The truck court faces west 88<sup>th</sup> Avenue and east toward the Pleasant Prairie Power Plant. The final number of required parking spaces will be determined based on the actual tenant(s). However, the number of parking spaces that have been provided in anticipation of the future tenants is indicated on the site plan.

<u>Public Sewer and Water:</u> The development shall be served by public sanitary sewer and water. Municipal water is located in 88<sup>th</sup> Avenue and public sanitary sewer is located within an easement on the site. The location of the parking areas, fire lanes and maneuvering lanes may be allowed over the existing sanitary sewer easement subject to certain conditions. Easement language has been added to the CSM that specifies access and maintenance responsibilities. It is important to note that the Village must be able to obtain easy access to and over the sewer main for maintenance purposes.

<u>Open Space, Stormwater Retention and Landscaping:</u> The M-2 District requires that at a minimum, 25% of the site must be open space. Within the amount of 100-year floodplain and proposed stormwater facilities more than 35% of the site will remain in open space. The retention basin edge shall be a minimum of 20 feet setback to the CTH H right-of-way (after right-of-way has been dedicated for the future widening of CTH H).

The truck dock areas adjacent to 88<sup>th</sup> Avenue will be extensively screened with a combination of berms and evergreens and other more dense-like landscaping and fencing. Landscaping and fencing shall be installed to screen the site from the adjacent highway/bike trail and existing residential and future residential development proposed to the west of 88<sup>th</sup> Avenue. All berms, fencing and landscape screening shall be installed within the adjusted property boundaries (after the dedication of the additional right-of-way on 88<sup>th</sup> Avenue). In addition, the owner will be required to install street trees adjacent to the roadway within the CTH H right-of-way per the Village requirements. A street tree plan, insurance certificate (with the Village also listed as an insured party) and financial security (cash on deposit) shall be provided to and approved by the Village for the installation of the public street trees. The Landscaping Contractor shall be pre-approved by the Village.

**Zoning Map and Text Amendment:** The current M-2, General Manufacturing, C-1, Lowland Resource Conservancy District and FPO, Floodplain Overlay District will remain unchanged on the property. The property will be rezoned to include a Planned Unit Development (PUD) Zoning District Overlay. A Zoning Text Amendment is proposed to create the specific PUD District zoning regulations for the development.

The following dimensional variation is included within the PUD Ordinance: Section 420-47 L of the Village Zoning Ordinance requires a minimum 20 foot parking/hard-surface setback. The maneuvering lane at the southeast corner of the building is proposed to have a zero (0) setback to the eastern property line. The property to the east is a 250 foot property owned by the Wisconsin Electric Power Company, which is then adjacent to the CP Railway.

In consideration of this dimensional variation, the Village will require that a gravel access roadway to the public sewer shall be installed by the Developer and the setback of the passenger car parking increased to 70 feet and the trailer parking increased to 90 feet from a portion of the north property that will be adjacent to future residential development.

**Certified Survey Map (CSM)**: A CSM will be required for the site development. The CSM must identify the dedication of additional right-of-way (dedicated public street area) along CTH H Avenue per Kenosha County's requirements of 60 feet from the centerline and the dedication of the following easements: 1) storm water management facilities/retention ponds easement; 2) relocation or expansion of the existing public sanitary sewer, access and maintenance easement; 3) landscape, berm and plantings, access and maintenance easements; 5) floodplain preservation and protection, access and maintenance easements; and 6) woodland preservation and protection, access and maintenance easements.

## **Recommendations:**

## Village staff recommends approval of the floodplain Boundary Adjustment and approval of Resolution #13-11 subject to the following conditions:

- 1. Prior to the execution of the required Community Concurrence Documents the following shall be submitted to the Village:
  - a. Written approval from the WI DNR.
  - b. The compensatory mitigation exhibit shall be revised to state the updated floodplain elevation for the north portion of the site (678.9). It is our understanding that text is wrong, however, the compensatory calculations reflect the revised elevation. (
  - c. Three (3) sets of final plans/calculations at full size, one reduced to 11" by 17" and a pdf of the plans/calculations including a pdf of the FEMA submittal. (*The revised plans and calculations will be used as Exhibit 1 of Resolution* #13-11)t
- 2. Since the final engineering plans for the development are still in process. The owner is responsible for any changes that may be required. It is the Village's understanding that the design engineer has reviewed the Village's Site and Operational Plan comments and feels that no major revisions will be needed to the floodplain adjustment. Approval of the floodplain adjustment plans is subject to final site engineering plan review and approval.
- 3. Upon receiving Village Board approval, the petitioner is responsible for transmitting all required applications to FEMA with a request for a C-LOMR-F.
- 4. The petitioner shall receive a C-LOMR-F prior to work commencing on the site. Upon receipt of the conditional C-LOMR-F and any required permits from the WI DNR,

copies of said approvals/permits shall be submitted to the Village with the required Erosion Control Permit Application (and Stipulated Shoreland permit if required.) and related plans and specification (paper copies and a pdf copy) for review and issuance of the required Village Permits.

[Note: Any grading within 75 feet of the Ordinary High Water Mark (OHWM) of the adjacent navigable waterway will required that a Stipulated Shoreland Permit from the Village be issued prior to work commencing (20 day comment period required)].

- 5. All required permits from the WI DNR shall be obtained and provided to the Village prior to the Village issuance of permits to commence work on site.
- 6. Upon completion of the floodplain boundary adjustment an as-built survey and calculations shall be submitted to verify the compliance with design plans. The asbuilt survey and calculations shall be reviewed by the Village and the WI DNR prior to being submitted to FEMA for review. Upon review of the documents by the Village, the petitioner shall submit and receive a final LOMR-F from FEMA.
- 7. Upon completion of the work, an as-built grading plan and supporting documentation certified and stamped by a Wisconsin registered and licensed profession engineer shall be submitted to the Village to verify compliance with interim design plans. The as-built grading plan and calculations shall be reviewed by the Village and the WI DNR prior to being submitted to FEMA for review and obtaining the required LOMR-F.
- 8. Once the final floodplain adjustment work is complete to meet the Village Ordinance requirements as outlined above in the proposed sequencing a final as-built grading plans and supporting documentation certified and stamped by a Wisconsin registered and licensed profession engineer shall be submitted to the Village to verify compliance with final design plans. A paper and pdf copy is required to be submitted. ([Note: Both the interim and final as-built information shall be reviewed and approved by the WI DNR and the Village]
- 9. Once a LOMR-F is issued and the Final as-built grading plans and supporting documentation is approved by the WI DNR and the Village, the petitioner shall submit an application to amend the Village Comprehensive Land Use Map and the Village Zoning Map and Text. All applicable applications and fees shall be submitted and paid by the petitioner. A Certificate of Compliance shall not be issued by the Village until the Comprehensive Land Use Plan Amendment, the Zoning Text Amendment and Zoning Map Amendment are approved by the Village.

## Village staff recommends approval of the Zoning Map and Zoning Text Amendments (Ord. #13-18 and #13-19) as presented.

## <u>Village staff recommends approval of the Certified Survey Map subject to</u> <u>compliance with the above comments and the following conditions:</u>

- 1. Several changes are required to the CSM see *attached*.
- 2. Any outstanding taxes, special assessments or invoices shall be paid prior to recording the CSM.
- 3. The CSM shall be finalized, executed and recorded at the Kenosha County Register of Deeds Office and confirmation of recording submitted to the Village prior to issuance of any building permits.

#### VILLAGE BOARD RESOLUTION #13-11 VILLAGE OF PLEASANT PRAIRIE RESOLUTION AND COMMUNITY CONCURRENCE TO AMEND THE 100-YEAR FLOODPLAIN BOUNDARY

**WHEREAS,** Adam Artz, P.E. with Pinnacle Engineering Group, agent is requesting to amend the 100-year floodplain as delineated and shown on Flood Insurance Rate Map (FIRM) and Flood Insurance Study (FIS) for the Village of Pleasant Prairie (Effective Date of June 19, 2012) for a part of U.S. Public Land Survey Section 16, Township 1 North, Range 22 East, Village of Pleasant Prairie, Kenosha County, Wisconsin on the property owned by Majestic Realty Co., generally located south of Bain Station Road on the east side of 88<sup>th</sup> Avenue (CTH H) and further identified as Tax Parcel Number 92-4-122-162-0301 for the proposed industrial development of the property; and

**WHEREAS**, the Floodplain Boundary Adjustment will remove *98,173* cubic feet of floodplain and create *99,144* cubic feet of floodplain to compensate for the floodplain being filled as shown on **Exhibit 1**; and

**WHEREAS,** the Village shall not permit amendments to the floodplain boundary that are inconsistent with the purposes of Section 420-131 of the Village Zoning Ordinance, or in conflict with the applicable rules of the Wisconsin Department of Natural Resources (DNR) and Federal Emergency Management Agency (FEMA); and

**WHEREAS**, an amendment to the floodplain boundaries shall not be permitted where the change will increase the regional flood stage elevation unless the applicant has made appropriate legal arrangements with the Village, any other affected governmental units, and any other property owners affected by the flood stage increase; and

**WHEREAS**, petitions for the floodplain amendments shall provide adjusted water surface profiles and adjusted floodplain limits to reflect the increased flood elevation; and

**WHEREAS**, any area removed from the floodplain shall be contiguous to land lying outside the floodplain; and

**WHEREAS**, whenever any volume of flood storage capacity is removed from the floodplain, as defined by the ground surface and the regional flood elevation, an equal volume of flood storage capacity shall be created within the newly determined floodplain boundary, in the vicinity of the removal, to compensate for the lost flood storage capacity. Excavation below the ordinary high-water mark shall not be considered as providing any equal volume of storage capacity for compensation purposes. Any such area of compensating flood storage capacity shall drain freely to the receiving stream; and

**WHEREAS**, removal of land from the floodplain shall not be permitted unless the land has been filled to an elevation at least two (2) feet above the elevation of the floodplain; and

**WHEREAS,** a public hearing was held by the Village Plan Commission of the Village of Pleasant Prairie, Kenosha County, Wisconsin, at the Village Municipal Building, 9915 39th Avenue on the 8<sup>th</sup> of February, 2013, at 6:00 P.M. of said day, for the purpose of determining the application of the floodplain boundary map amendment; and

**WHEREAS,** the Pleasant Prairie Village Board has been provided with sufficient evidence that the petition for a floodplain boundary map adjustment and amendment is consistent with the requirements of the Village Zoning Ordinance.

**NOW, THEREFORE,** the Village Board does hereby resolve to amend the 100-year recurrence interval floodplain boundary as delineated and shown on the Flood Insurance Rate Map (FIRM) and Flood Insurance Study (FIS) for the Village of Pleasant Prairie (Effective Date of June 19, 2012) for a part of U.S. Public Land Survey Section 16, Township 1 North, Range 22 East, Village of Pleasant Prairie, Kenosha County, Wisconsin, subject to compliance with the terms and conditions hereinafter stated:

Page 2 Resolution #13-11

- 1. Upon receiving Village Board approval, the petitioner is responsible for transmitting all required applications to FEMA with a request for a C-LOMR-F.
- 2. The petitioner shall receive a C-LOMR-F prior to work commencing on the site. Upon receipt of the conditional C-LOMR-F and any required permits from the WI DNR, copies of said approvals/permits shall be submitted to the Village with the required Erosion Control Permit Application (and Stipulated Shoreland permit if required.) and related plans and specification (paper copies and a pdf copy) for review and issuance of the required Village Permits.

[Note: Any grading within 75 feet of the Ordinary High Water Mark (OHWM) of the adjacent navigable waterway will required that a Stipulated Shoreland Permit from the Village be issued prior to work commencing (20 day comment period required)].

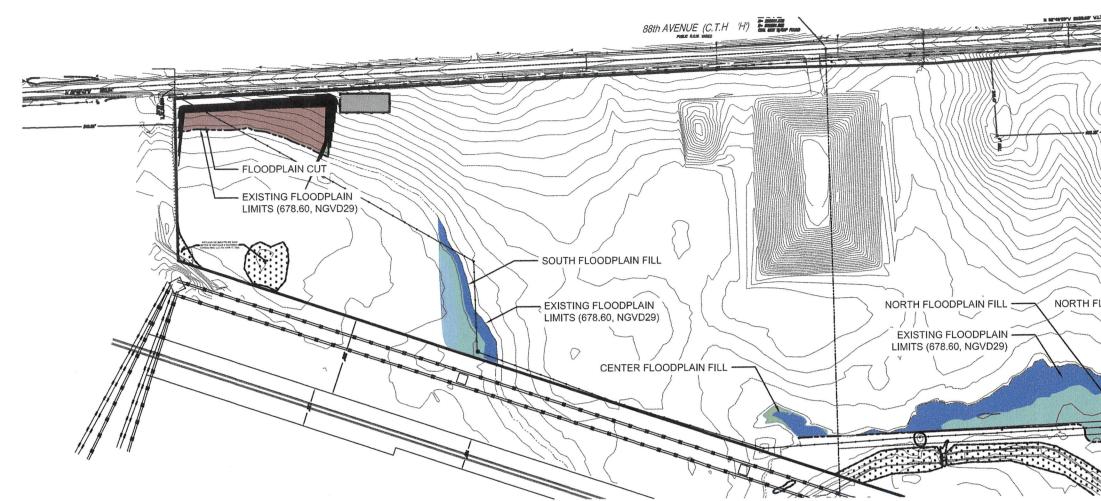
- 3. All required permits from the WI DNR shall be obtained and provided to the Village prior to the Village issuance of permits to commence work on site.
- 4. Upon completion of the floodplain boundary adjustment an as-built survey and calculations shall be submitted to verify the compliance with design plans. The as-built survey and calculations shall be reviewed by the Village and the WI DNR prior to being submitted to FEMA for review. Upon review of the documents by the Village, the petitioner shall submit and receive a final LOMR-F from FEMA.
- 5. Upon completion of the work, an as-built grading plan and supporting documentation certified and stamped by a Wisconsin registered and licensed profession engineer shall be submitted to the Village to verify compliance with interim design plans. The as-built grading plan and calculations shall be reviewed by the Village and the WI DNR prior to being submitted to FEMA for review and obtaining the required LOMR-F.
- 6. Once the final floodplain adjustment work is complete to meet the Village Ordinance requirements as outlined above in the proposed sequencing a final as-built grading plans and supporting documentation certified and stamped by a Wisconsin registered and licensed profession engineer shall be submitted to the Village to verify compliance with final design plans. A paper and pdf copy is required to be submitted. ([Note: Both the interim and final as-built information shall be reviewed and approved by the WI DNR and the Village]
- 7. Once a LOMR-F is issued and the Final as-built grading plans and supporting documentation is approved by the WI DNR and the Village, the petitioner shall submit an application to amend the Village Comprehensive Land Use Map and the Village Zoning Map and Text. All applicable applications and fees shall be submitted and paid by the petitioner. A Certificate of Compliance shall not be issued by the Village until the Comprehensive Land Use Plan Amendment, the Zoning Text Amendment and Zoning Map Amendment are approved by the Village.

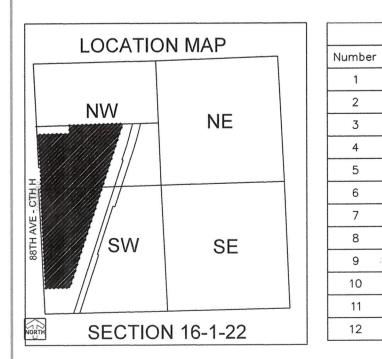
## **RESOLUTION** by action of the Village Board of the Village of Pleasant Prairie adopted this 20<sup>th</sup> day of May 2013.

Attest:

John P. Steinbrink Village President

Jane M. Romanowski Village Clerk





Elevations Table				
Number	Color	Minimum	Maximum	
1		-100.00	-2.50	
2		-2.50	-2.00	
3		-2.00	-1.50	
4		-1.50	-1.00	
5		-1.00	-0.50	
6		-0.50	0.00	
7		0.00	0.50	
8		0.50	1.00	
9 :		1.00	1.50	
10		1.50	2.00	
11		2.00	2.50	
12		2.50	100.00	

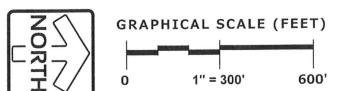
## Cut/Fill Summary

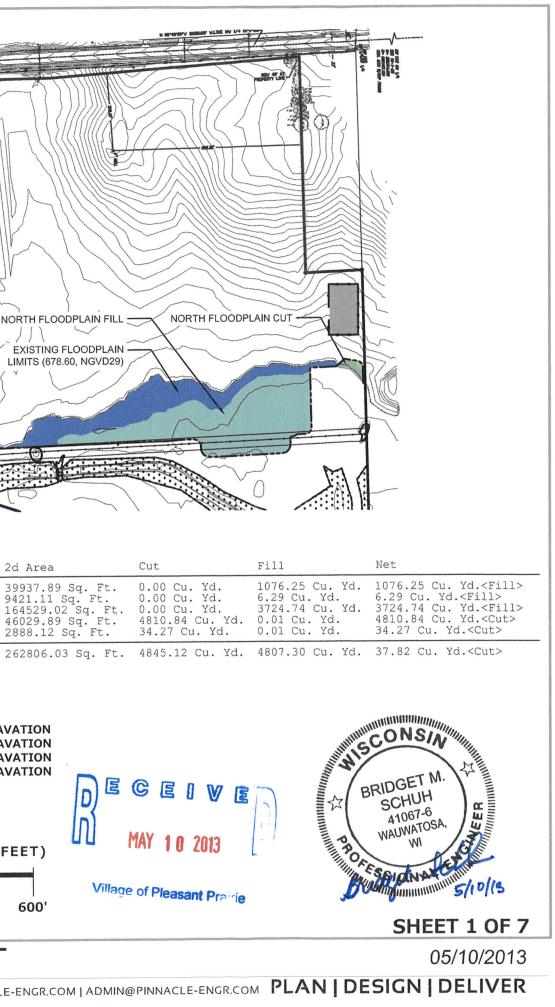
Name	Cut Factor	Fill Factor	2d Area	Cut
Volume - Fill South Volume - Fill Center Volume - Fill North Volume - Cut Volume - Cut north	1.00 1.00 1.00 1.00 1.00	1.00 1.00 1.00 1.00 1.00	39937.89 Sq. Ft. 9421.11 Sq. Ft. 164529.02 Sq. Ft. 46029.89 Sq. Ft. 2888.12 Sq. Ft.	0.00 Cu. Yd. 0.00 Cu. Yd. 0.00 Cu. Yd. 4810.84 Cu. Y 34.27 Cu. Yd
			860006 00 Gr Et	1045 12 Cm

Totals

## **INDEX OF SHEETS**

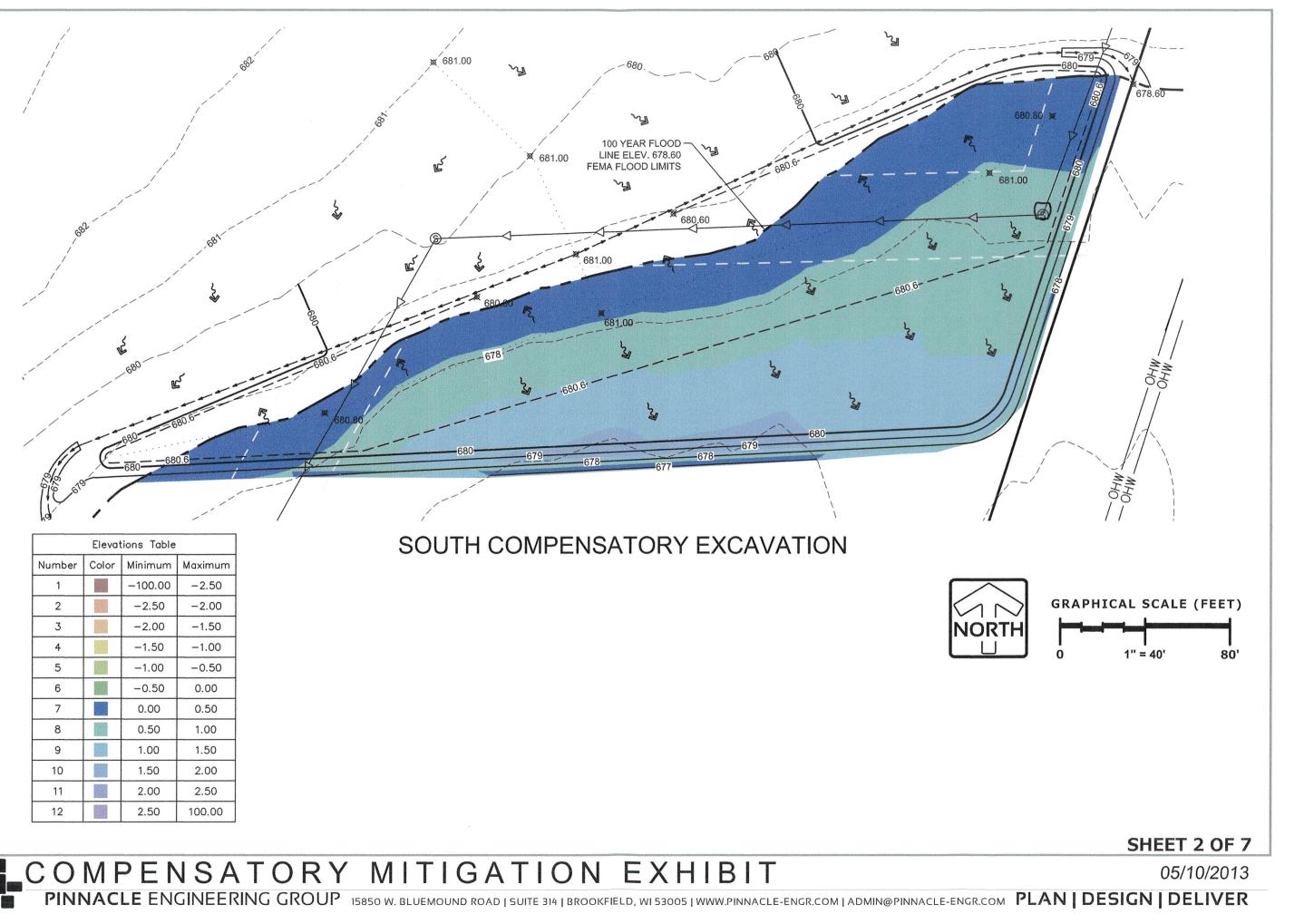
- COVER SHEET
- SOUTH COMPENSATORY EXCAVATION
- NORTH COMPENSATORY EXCAVATION
- NORTH COMPENSATORY EXCAVATION
- NORTH COMPENSATORY EXCAVATION
- SOUTH CUT EXCAVATION 6 CROSS SECTIONS 7



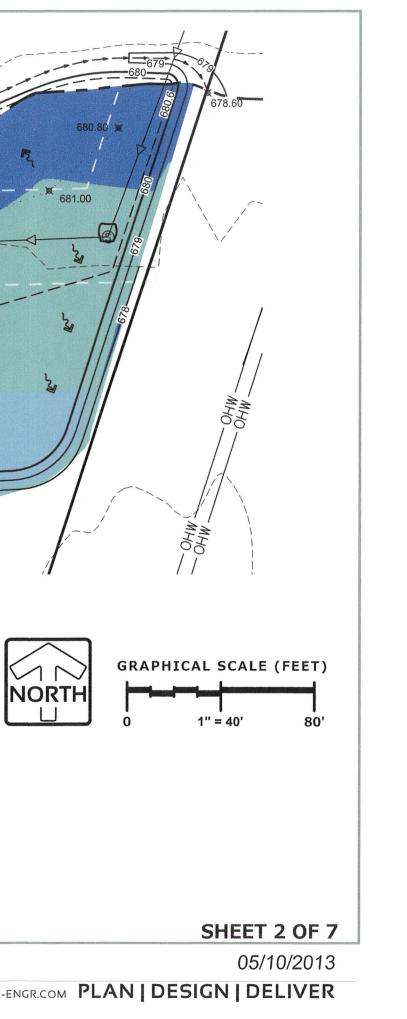


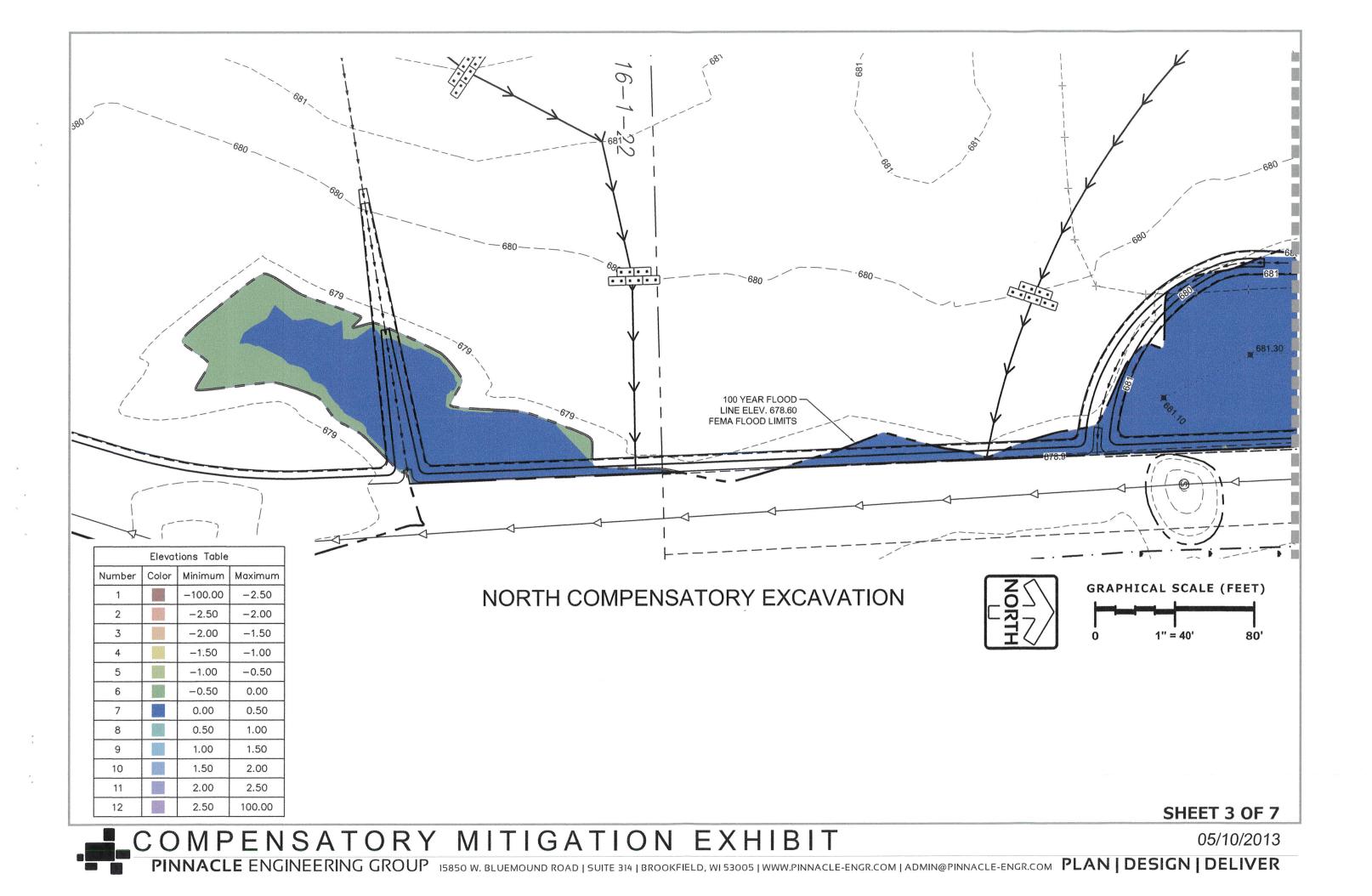
COMPENSATORY MITIGATION EXHIBIT

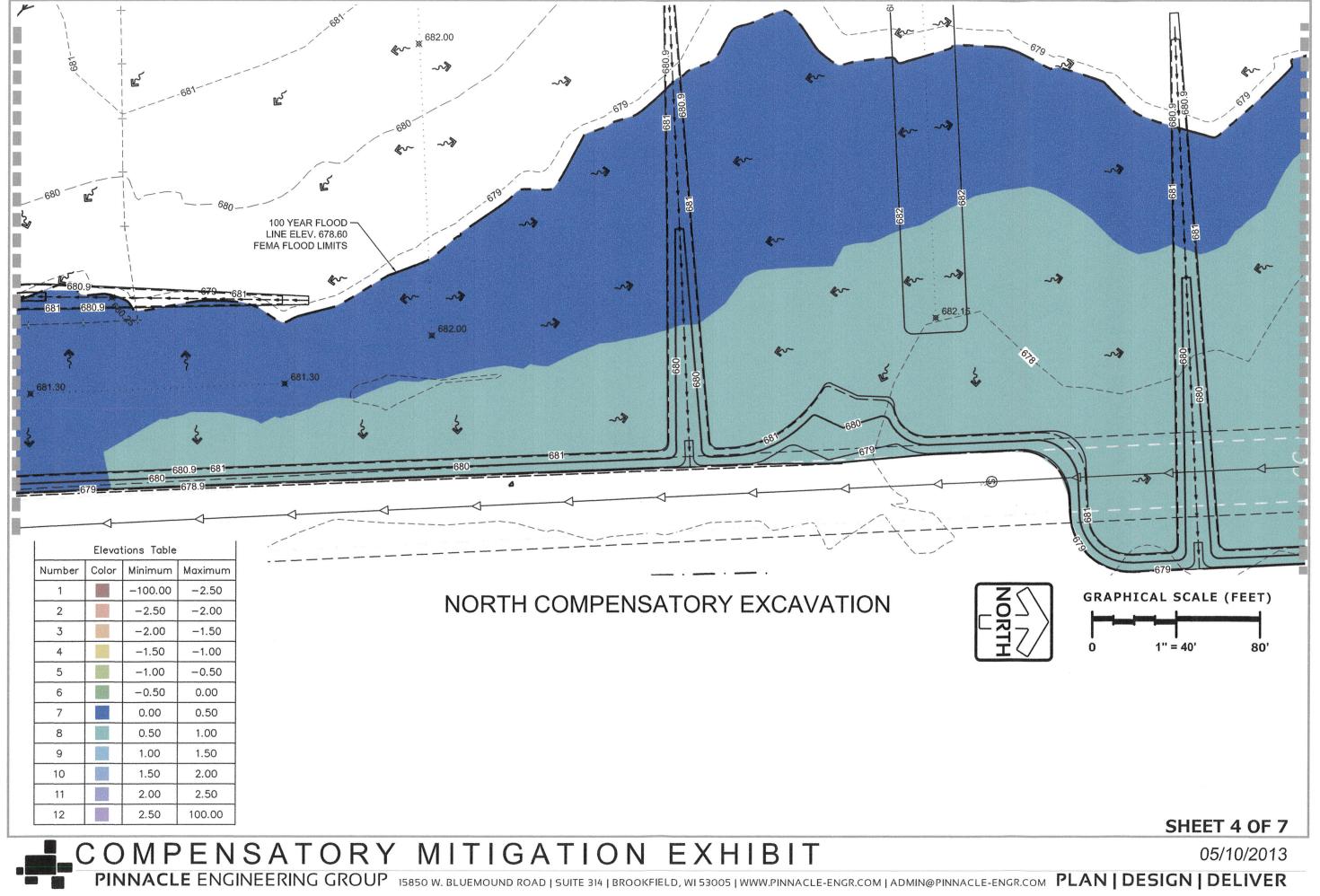
PINNACLE ENGINEERING GROUP 15850 W. BLUEMOUND ROAD | SUITE 314 | BROOKFIELD, WI 53005 | WWW.PINNACLE-ENGR.COM | ADMIN@PINNACLE-ENGR.COM | ADMIN@PINN

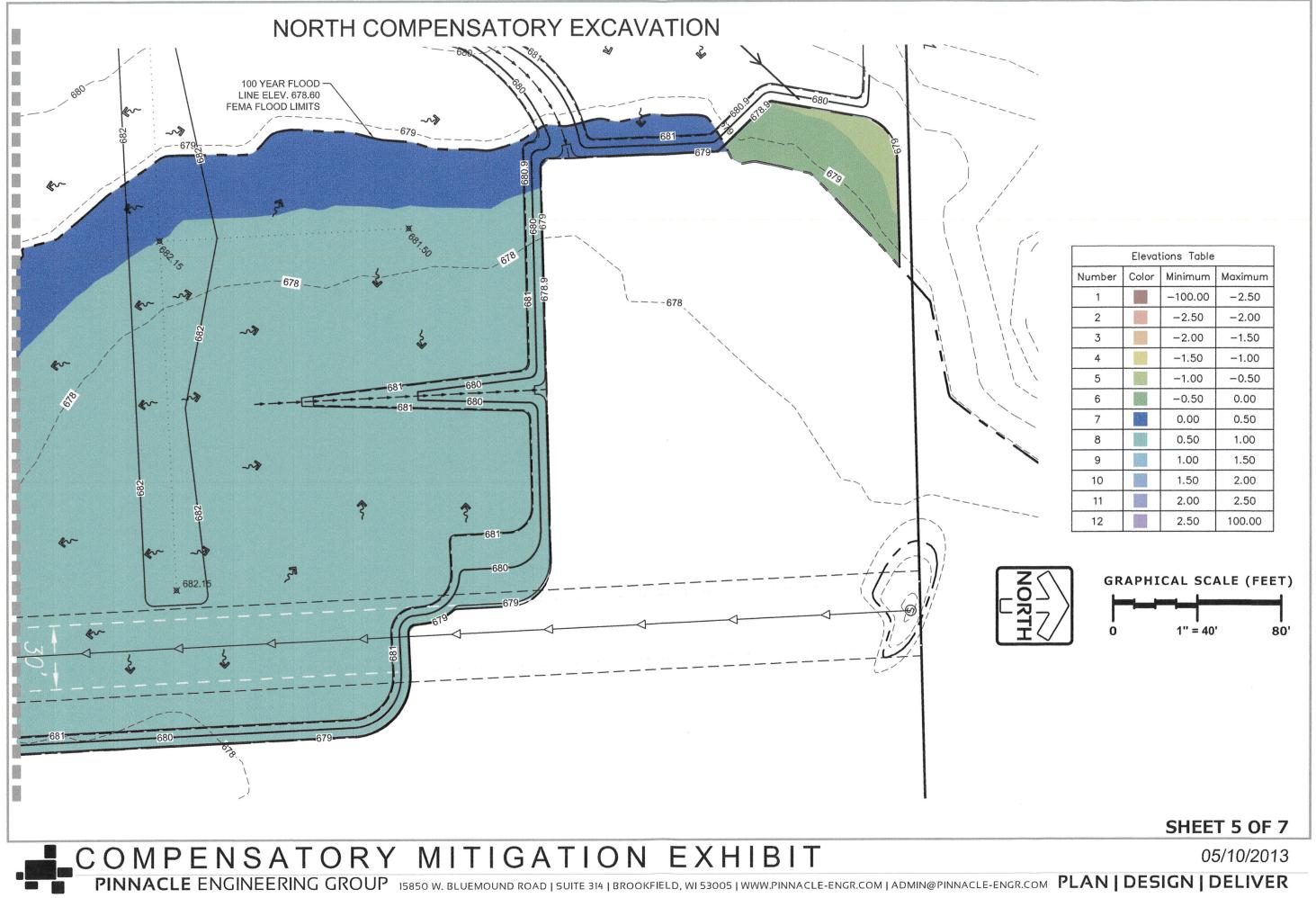


Elevations Table				
Number	Color	Minimum	Maximum	
1		-100.00	-2.50	
2		-2.50	-2.00	
3		-2.00	-1.50	
4		-1.50	-1.00	
5		-1.00	-0.50	
6		-0.50	0.00	
7		0.00	0.50	
8		0.50	1.00	
9		1.00	1.50	
10		1.50	2.00	
11		2.00	2.50	
12		2.50	100.00	





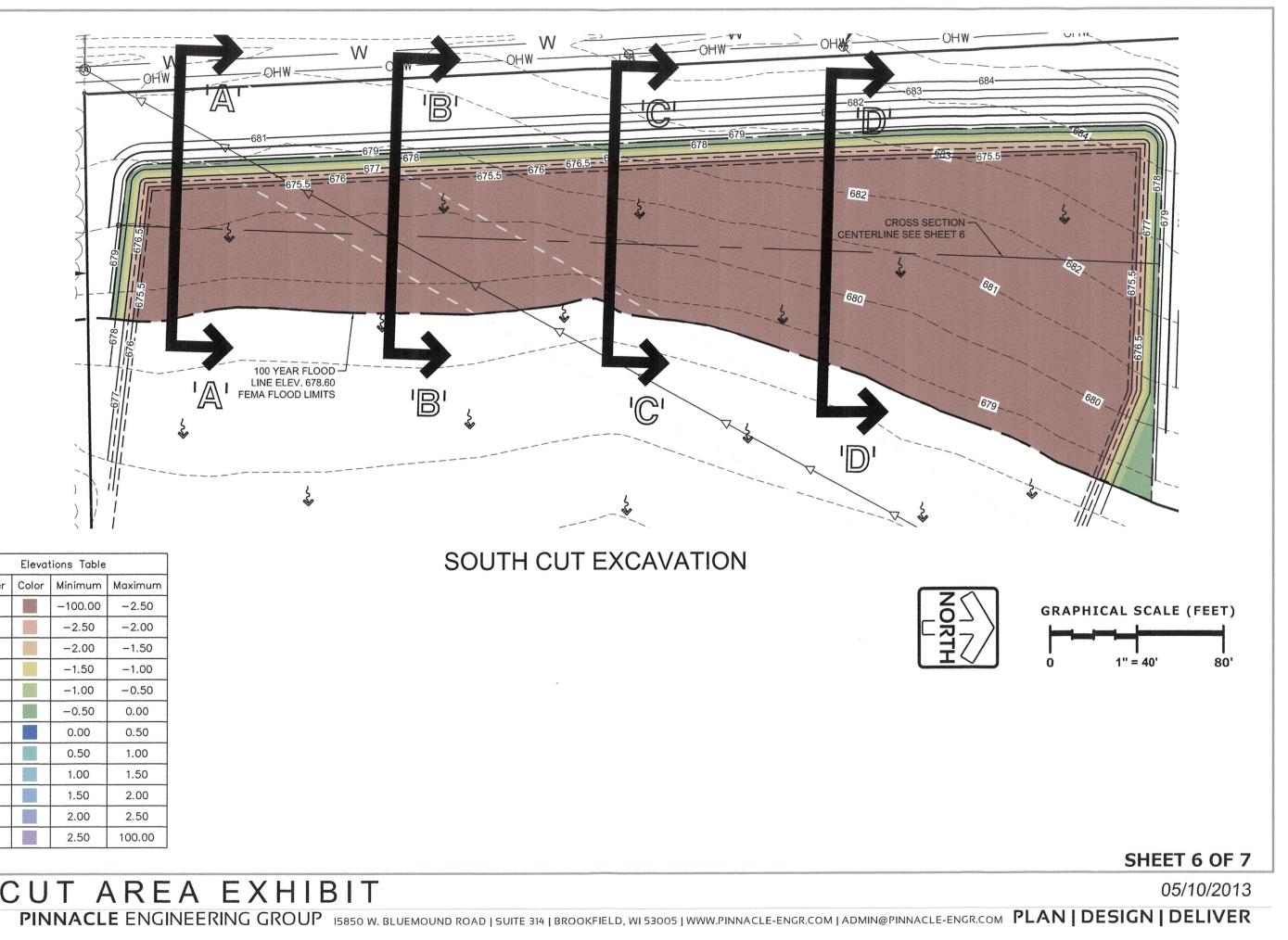


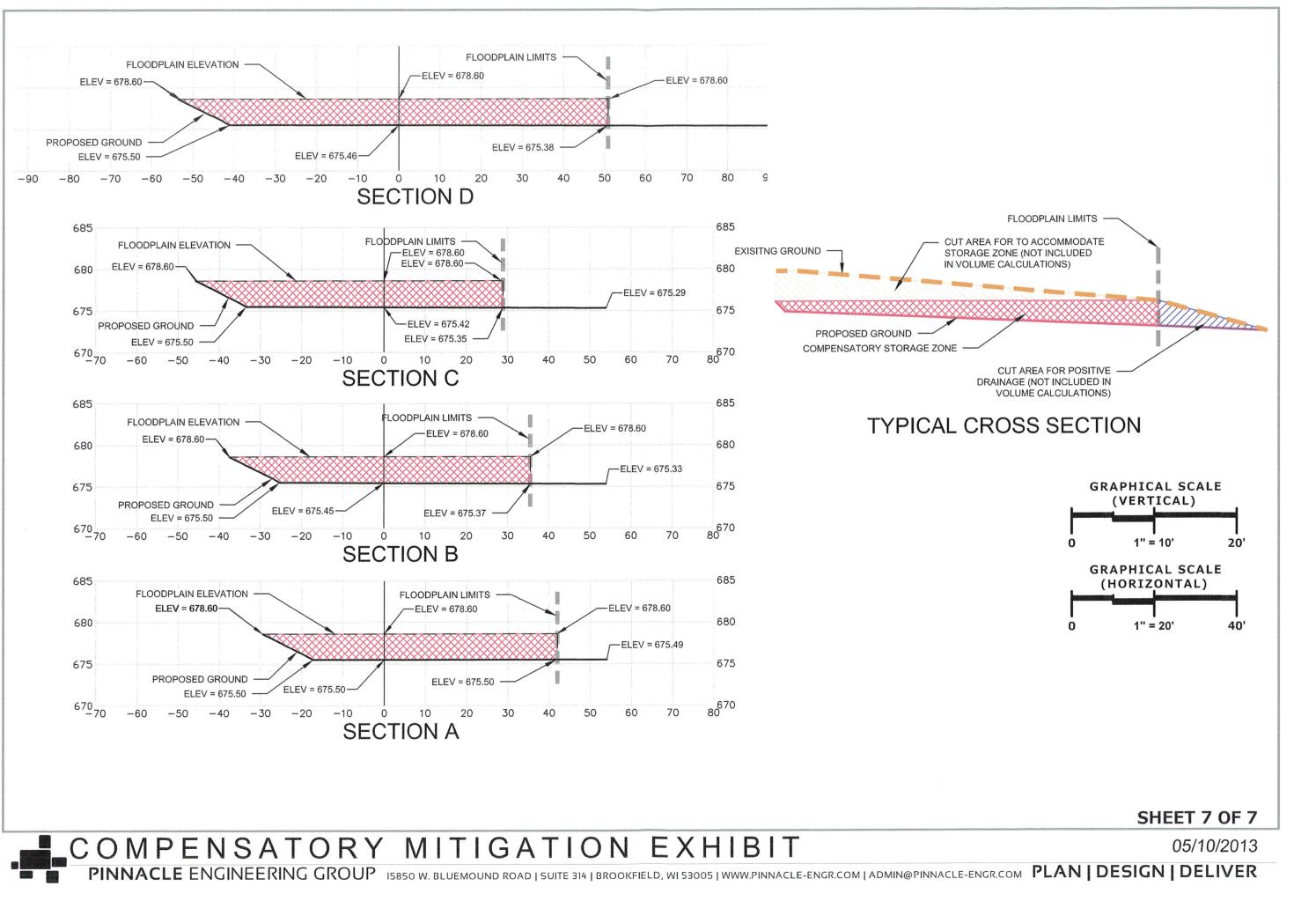


Elevations Table			
Number	Color	Minimum	Maximum
1		-100.00	-2.50
2		-2.50	-2.00
3		-2.00	-1.50
4		-1.50	-1.00
5		-1.00	-0.50
6		-0.50	0.00
7		0.00	0.50
8		0.50	1.00
9		1.00	1.50
10		1.50	2.00
11		2.00	2.50
12		2.50	100.00

NGINEERING GROUP 15850 W. BLUEMOUND ROAD   SUITE 314   BROOKFIELD, WI 53005   WWW.PINNACLE-ENGR.COM   ADMIN@	PINNACLE-ENG
--------------------------------------------------------------------------------------------------------------	--------------

Elevations Table			
Number	Color	Minimum	Maximum
1		-100.00	-2.50
2		-2.50	-2.00
3		-2.00	-1.50
4		-1.50	-1.00
5		-1.00	-0.50
6		-0.50	0.00
7		0.00	0.50
8		0.50	1.00
9		1.00	1.50
10		1.50	2.00
11		2.00	2.50
12		2.50	100.00





## ORD. # 13-18

### ORDINANCE TO AMEND THE OFFICIAL ZONING MAP OF THE VILLAGE OF PLEASANT PRAIRIE, KENOSHA COUNTY, WISCONSIN PURSUANT TO CHAPTER 420-13 OF THE VILLAGE ZONING ORDINANCE

### BE IT ORDAINED by the Village of Pleasant Prairie Board of Trustees, Kenosha County, Wisconsin, that the Official Village Zoning Map is hereby amended as follows:

The subject property generally located approximately ¼ mile south of Bain Station Road on the east side of 88<sup>th</sup> Avenue and located in U.S. Public Land Survey Section 16, Township 1 North, Range 22 East in the Village of Pleasant Prairie and further identified as Tax Parcel Number 92-4-122-162-0301 and the adjacent portion of the right-of-way is hereby rezoned into the PUD, Planned Unit Development Overlay District. The existing M-2, General Manufacturing District, C-1, Lowland Resource Conservancy District and FPO, Floodplain Overlay District will remain unchanged.

The Village Zoning Administrator is hereby directed to record this Zoning Map Amendment on the appropriate sheet of the Official Village Zoning Map and Appendix B in Chapter 420 of the Village Municipal Code shall be updated to include said amendments.

## Adopted this 13<sup>th</sup> day of May, 2013.

## VILLAGE BOARD OF TRUSTEES

John P. Steinbrink Village President

ATTEST:

Jane M. Romanowski Village Clerk

Posted:\_\_\_\_\_

18-Majestic PUD .doc

## **ORDINANCE # 13-19**

### ORDINANCE TO CREATE A PLANNED UNIT DEVELOPMENT PURSUANT TO SECTION 420-137 OF THE VILLAGE ZONING ORDINANCE FOR THE MAJESTIC BADGER LOGISTICS CENTER IN THE VILLAGE OF PLEASANT PRAIRIE, KENOSHA COUNTY, WISCONSIN

**BE IT ORDAINED** by the Village Board of Trustees of the Village of Pleasant Prairie, Kenosha County, Wisconsin, that the following Planned Unit Development (PUD) Ordinance is hereby created for Majestic Badger Logistics Center pursuant to Chapter 420-137 of the Village Zoning Ordinance to read as follows:

## **Majestic Badger Logistics Center Planned Unit Development**

- a. It is the intent that Majestic Badger Logistics Center be developed on the property as legally described below in conformity with the Village of Pleasant Prairie (Village) adopted Comprehensive Land Use Plan; that the development will not be contrary to the general health, safety, welfare and economic prosperity of the community; and that the architectural, building and site design, sidewalks, landscaping, grading and drainage, fences, signage, lighting and general site development is an attractive and harmonious industrial development of sustained desirability and economic stability and will not have an adverse effect on the property values of the surrounding neighborhood.
- b. Legal Description: The property is known as CSM #\_\_\_\_\_\_ as recorded at the Kenosha County Register of Deeds Office Document No. \_\_\_\_\_\_ and is located in U.S. Public Land Survey Section 16, Township 1 North, Range 22 East of the Fourth Principal meridian lying and being in the Village of Pleasant Prairie and hereinafter referred to as the "DEVELOPMENT".

## c. Requirements within the DEVELOPMENT:

- (i) The DEVELOPMENT shall be in compliance with all Federal, State, County and Village Ordinances and regulations, except as expressly modified by this PUD Ordinance.
- (ii) The owner shall install and maintain a 14 foot wide sanitary sewer maintenance path to the public sanitary sewer manholes, including providing snow removal, pursuant to the Site and Operational Plans as conditionally approved by the Plan Commission on May 13, 2013 on file with the Village.
- (iii) The DEVELOPMENT, including but not limited to, the buildings, signs, fences, garbage dumpster enclosures, landscaping, parking lot(s), exterior site lighting, etc., and the DEVELOPMENT as a whole, shall be maintained on a regular basis in a neat, presentable, aesthetically pleasing, structurally sound and non-hazardous condition. This maintenance shall also include the daily picking up and disposal of trash and debris which may accumulate within the DEVELOPMENT.
- (iv) The DEVELOPMENT shall be in compliance with all applicable Site and Operational Plan(s), Conditional Use Permit(s) and Planned Unit Development Ordinance(s) (if applicable), as on file with the Village.
- (v) All buildings and site alterations and modifications, excluding general building and site maintenance within the DEVELOPMENT, shall be made in accordance with the applicable Village Ordinances and Codes at the time the modification is proposed.
- (vi) All buildings and structures and all exterior additions, remodeling and alterations to the any buildings/structures within the DEVELOPMENT shall be constructed of the same or complimentary exterior materials, colors and architectural style to ensure a quality, well maintained development, including signage, lighting, fencing, berming, landscaping, etc. as approved by the Village.
- (vii) The DEVELOPMENT shall be operated and maintained in a uniform manner, regardless of property ownership. If the DEVELOPMENT is sold to another entity(s),

the DEVELOPMENT shall continue to comply with this PUD, which may be amended from time to time.

- (viii) Temporary or permanent storage containers (some having brand names such as P.O.D.S, S.A.M.S., etc.) and compactors are not allowed within the DEVELOPMENT. All merchandise, product, crate, pallet, etc., storage shall be stored inside a building.
- (ix) No flags, pennants, streamers, inflatable signage, plastic banner-type signage, spot lights, walking signs, shall be allowed or affixed to any building, landscaping, vehicle, antenna, roof-top, or the ground unless expressly permitted by the Zoning Ordinance.
- (x) The DEVELOPMENT shall be maintained in a neat, presentable, aesthetically pleasing, structurally sound and nonhazardous condition. All litter and debris shall be promptly removed.
- (xi) The DEVELOPMENT shall not be used for any outside overnight or daytime parking of junked, inoperable, dismantled or unlicensed vehicles. All junked, inoperable, dismantled or unlicensed vehicles that are parked outside will be issued citations.
- (xii) The DEVELOPMENT shall comply with all applicable performance standards set forth in Section 420-38 of the Village Zoning Ordinance.
- d. Specific modifications to the Village of Pleasant Prairie Zoning Ordinance for the DEVELOPMENT:
  - (i) Section 420-47 L related to setbacks for parking spaces and parking lots shall be amended as follows:
    - L. Parking areas including maneuvering lanes shall be set back a minimum of 20 feet from CTH H (88<sup>th</sup> Avenue) and the south property line, a minimum of 20 feet from the north property line for 685.91 feet south of CTH H (88<sup>th</sup> Avenue), a minimum of 70 feet for passenger vehicles parking lots and a minimum of 90 feet for truck and trailers parking lot from the north property line for the remaining 1132.29 feet of the property; and no setback to the eastern property line.

## e. Amendments

- (i) The PUD regulations for the DEVELOPMENT may be amended pursuant to Chapter 420-137 of the Village Municipal Code.
- (ii) The Zoning Administrator has the discretion to approve minor changes, adjustments and additions to this PUD ordinance document without the need for Village Plan Commission and Village Board review and approval.

## Adopted this 20<sup>th</sup> day of May, 2013.

VILLAGE OF PLEASANT PRAIRIE

ATTEST:

John P. Steinbrink Village President

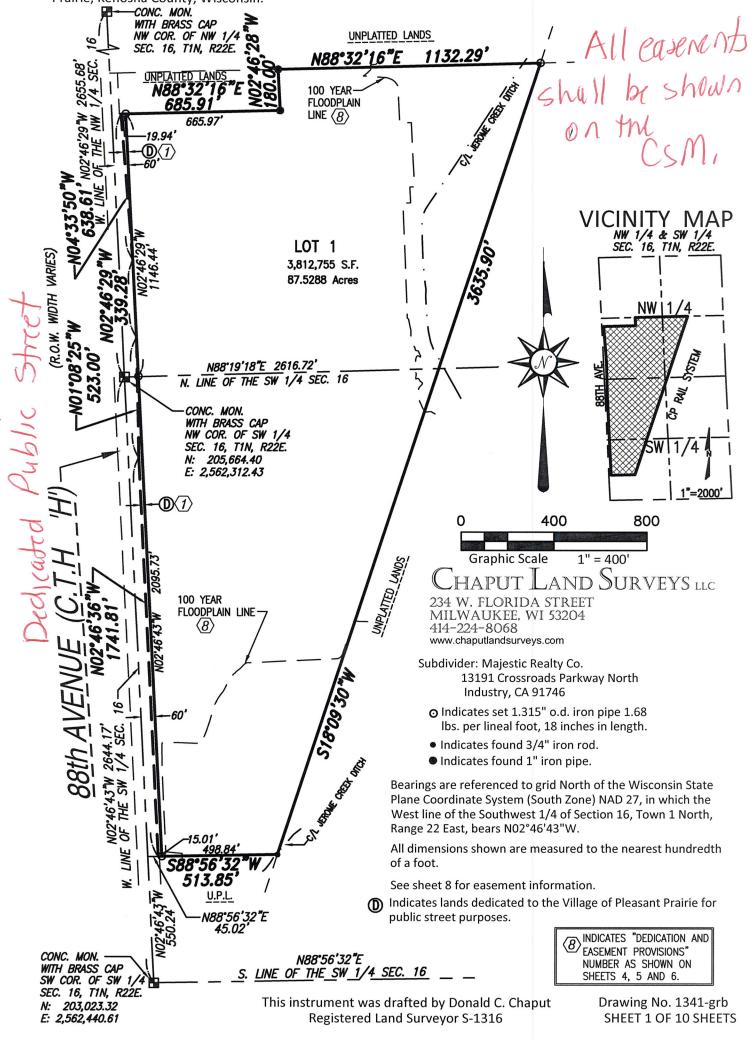
Jane M. Romanowski Village Clerk

Posted: \_\_\_\_\_

19- Majestic Badger Logistics Center PUD.doc

5 9 13

Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.



¥ V V

Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.

## SURVEYOR'S CERTIFICATE

STATE OF WISCONSIN

MILWAUKEE COUNTY}

I, DONALD C. CHAPUT, Registered Land Surveyor, do hereby certify:

THAT I have surveyed, divided and mapped part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin) which is bounded and described as follows:

COMMENCING at the Southwest corner of said Southwest 1/4 Section; thence North 02°46'43" West along the West line of said 1/4 Section 550.24 feet to a point; thence North 88°56'32" East 45.02 feet to a point on the East line of 88th Avenue (C.T.H. "H") and the point of beginning of the lands to be described; thence North 02°46'36" West along said East line 1741.81 feet to a point; thence North 01°08'25" West along said East line 523.00 feet to a point; thence North 02°46'29" West along said East line 339.28 feet to a point; thence North 04°33'50" West along said East line 638.61 feet to a point; thence North 88°32'16" East 685.91 feet to a point; thence North 02°46'28" West 180.00 feet to a point; thence North 88°32'16" East 1132.29 feet to a point; thence South 18°09'30" West 3635.90 feet to a point; thence South 88°56'32" West 513.85 feet to the point of beginning.

Said lands contain 3,849,097 square feet, or 88.3631 acres.

THAT I have made the survey, land division and map by the direction of Majestic Realty Co., owner.

THAT the map is a correct representation of all the exterior boundaries of the land surveyed and the land division thereof made.

THAT I have fully complied with the provisions of Chapter 236 of the Wisconsin Statutes and the Land Division and Ordinances of the Village of Pleasant Prairie in surveying, dividing and mapping the same.

Development Control

DATE

DONALD C. CHAPUT REGISTERED LAND SURVEYOR S-1316

The following "Dedication of Easement Provisions" and "Restrictive Covenants" were drafted by the Village of Pleasant Prairie and is shown hereon as a condition of map approval. Inclusion thereof on this document is not to be considered practicing law in the State of Wisconsin by the above signed Land Surveyor, the Land Surveyor is not responsible for rights granted, perceived or otherwise stated herein.

.are

This instrument was drafted by Donald C. Chaput Registered Land Surveyor S-1316 Drawing No. 1341-grb SHEET 2 OF 10 SH

Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.

### DEDICATION AND EASEMENT PROVISIONS

- The fee interest the in the area shown as a **Dedicated Public Street** on this Certified Survey Map (CSM) for 88th Avenue 1. (a/k/a County Trunk Highway H) is hereby dedicated, given, granted and conveyed by Majestic Realty Co., (the "Owner") to the Village of Pleasant Prairie, its successors and assigns (the "Village") for the construction, installation, repair, alteration, replacement, and planting and maintenance of the terrace areas referred to collectively as the public highway improvements, uses and purposes, including, without limitation, pavement, curbs and gutters, bike lanes, if required by the Village or Kenosha County ("Kenosha County"), street signs, street lights, street trees, sanitary sewerage system improvements, water system improvements, storm sewer and drainage system improvements, utility and communications facilities, street tree landscaping, and for all related ingress and egress. Such fee interest is subject to the following: a nonexclusive easement hereby reserved by the current Owner for the future Owner(s) of the Lot shown on this CSM which is adjacent to the Dedicated Public Street for the required planting, mowing, watering and maintenance of grass within the grassy terrace area, for the maintenance and replanting of street trees and the clearance in the area between the roadway and the Lot. In the event of any conflict between the rights of the Village or Kenosha County under its fee interest in the Dedicated Public Street area shown on this CSM and the rights of the Owner, or of the future land Owners, or of any future Commercial Owners Association pursuant to the dedication retained herein, the rights of the Village shall be deemed to be superior.
- and A perpetual nonexclusive easement coextensive with the area shown on this CSM as a Dedicated 30' Wide Sanitary 2 Sewer Main, Access, Maintenance Easement was dedicated, given, granted and conveyed by the previous landowner to the Village and recorded as Document # \_ at the Kenosha County Register of Deeds Office to the Village for public sanitary sewerage system improvements, uses and purposes, and for all related ingress and egress, construction, installation, repair, alteration, replacement and maintenance activities. This CSM also hereby dedicates, gives, grants and conveys an additional 10' to the existing Sanitary Sewer Easement for a total of a Dedicated 40' Sanitary Sewer Main, Access and Maintenance Easement to the Village for public sanitary sewerage system improvements, uses and purposes, and for all related ingress and egress, construction, installation, repair, alteration, replacement and maintenance activities. In addition, there are new or additional areas shown as Dedicated 40' Sanitary Sewer Main, Access and Maintenance Easement on this CSM to provide Village gravel access for maintenance to the existing sanitary sewer a sanitary sever maintenence path and system. the)

The Dedicated 40' Sanitary Sewer Main, Access and Maintenance Easement areas shall be exclusive, except for: (1) the Dedicated 14' Sanitary Sewer Maintenance Path and Access Easement area dedicated to the Village on this CSM with respect to the same area or any portion thereof; (2) the Owner's use, planting and irrigating, care and maintenance of the Dedicated Sanitary Sewer, Access and Maintenance Easement vegetative area on Lot 1 as it will not interfere with the improvements, uses and purposes of the Village; and (3) \_\_\_\_\_\_\_. In

the event of any conflicts between the rights of the Village pursuant to this Dedicated Sanitary Sewer, Access and Maintenance Easement and the rights of any other persons or entities with respect to this Easement, the Village's rights under this Easement shall be deemed to be superior.

3 Perpetual nonexclusive easements coextensive with the areas shown as a Dedicated 20' General Utility Easement on this CSM are hereby dedicated, given, granted and conveyed by the Owner for WE Energies, AT & T, and Time Warner Cable Inc. and their respective successors and assigns (collectively the "Utility and Communications Grantees"), for the purposes of constructing, installing, operating, repairing, altering, replacing and maintaining utility and communication lines and other related facilities to serve the CSM for any related ingress and egress. This Dedicated General Utility Easement shall also include the right to trim or cut down trees, bushes, branches, and roots as may be reasonably required, that are interfering with the Utility and Communication Grantees use of the easement areas. To the extent possible, all such utility and communications lines and facilities shall be installed underground. Upon the installation of the utility cables and related appurtenances, the elevation of the existing ground surface within the Dedicated General Utility Easement area shall not be altered by more than four (4) inches of final grade without the written approval of the Utility and Communications Grantees. Upon the installation of the utilities, the Grantor shall restore or cause to be restored, all such land, as nearly as is reasonably possible, to the condition existing prior to installing such utilities within the General Utility Easement areas on which such easements are located as does not interfere with the purpose of the utility and communications easements and the use of such easements by the Utility and Communications Grantees unless a separate agreement is entered into between the Grantor and Grantees regarding the transfer of the restoration and maintenance responsibilities to the Grantees. No buildings, fences, or structures of any kind shall be placed within the General Utility Easement areas without the prior written approval of the Utility and Communication Grantees.

> This instrument was drafted by Donald C. Chaput Registered Land Surveyor S-1316

Drawing No. 1341-grb SHEET 3 OF 10 5

Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.

The Village and Kenosha County generally allows private utilities, including but not limited to electric and communications facilities, to be installed in public street rights-of-way areas with prior written approval from the Village or Kenosha County, subject to the requirements of applicable ordinances and the requirements of such public uses and purposes of the Village or Kenosha County. Further, each individual private utility, electric or communications company shall be responsible for promptly restoring the public street areas to their pre-existing condition, at its own cost, after any use of such areas. In the event the private companies do not restore the public street areas to a vegetatively stabilized condition, the Owner shall be ultimately responsible for the costs of such restoration and may pursue its remedies against the respective utility company(ies). Under no circumstances shall any private utility, electric or communications company conduct any open cutting of the public street without prior written approval of the Village or Kenosha County. Any such private utility or communications facilities shall be promptly relocated, at the cost of the individual utility, electric or communications company, upon written request of the Village or Kenosha County, to serve the public functions and purposes of the Village or Kenosha County in the public street area. In the event of any conflict between the rights of the Village or Kenosha County and the rights of the private utility, electric or communications company in such public street areas, the Village's or Kenosha County's rights shall be deemed to be superior.

- 4. Perpetual nonexclusive easements coextensive with the two areas shown as a Dedicated Storm Water Management, Retention Basin, Access & Maintenance Easement on this CSM are hereby dedicated, given, granted and conveyed by the Owner to the Village for private storm water drainage system improvements, storm water retention basin storage and conveyance, uses and purposes, and for all related ingress and egress, construction, installation, repair, alteration, replacement and maintenance activities. These Dedicated Storm Water Management, Retention Basin, Access and Maintenance Easements shall be exclusive, except for: (1) the Owner's use, planting and irrigating, care and maintenance of the Dedicated Storm Water Management, Retention Basin, Access and Maintenance Easement areas on Lot 1 as it will not interfere with the improvements, uses and purposes of the Village and (2) the Dedicated 20' General Utility Easement as dedicated on this CSM. In the event of any conflicts between the rights of the Village pursuant to the Dedicated Storm Water Management, Retention Basin, Access and the rights of any other persons or entities with respect to the Dedicated Easement, the Village's rights under these Easements shall be deemed to be superior.
  - 5. Perpetual nonexclusive easements coextensive with the areas shown as a Dedicated 14' Sanitary Sewer Maintenance Path and Access Easement on this CSM are hereby dedicated, given, granted and conveyed by the Owner for access, uses and purposes, and for all related ingress and egress, construction, installation, repair, alteration, replacement and maintenance activities. This Dedicated 14' Sanitary Sewer Maintenance Path and Access Easement shall be exclusive, except for: (1) the corresponding Dedicated 40' Wide Sanitary Sewer, Access and Maintenance Easement shown on this CSM with respect to the same area or any portion thereof and (2) the maintenance obligations of the Owner for the land area adjacent to the gravel path. In the event of any conflicts between the rights of the Village pursuant to these Dedicated 14' Sanitary Sewer Maintenance Path and Access Easement shall be deemed to be superior. Owner shall grade the 14' Sanitary Sewer Maintenance Path and Access Easement in accordance with the standards requested by the Village. The Village will then construct and maintain the road with road base, gravel or any other material it finds acceptable to support the equipment of the Village that will need to operate on the Maintenance Path.

## coextensive path

A perpetual nonexclusive easement with the area shown as a **Dedicated 20' Landscape, Berm, Plantings, Access and Maintenance Easement** on this CSM is hereby dedicated, given, granted and conveyed by the Owners of Lot 1 to the Village for the grading, berming, placing of topsoil and seed/sod, installing and maintaining sprinklers, installing and maintaining fences and retaining walls, installing trees, plants and related landscaping, and for all related ingress and egress, including replanting and other landscape maintenance activities pursuant to the Village approved Site and Operational Plan landscape screening requirements for the approval of the Majestic Building to be constructed on Lot 1 of this CSM. This Dedicated 20' Landscape, Berm, Plantings, Access and Maintenance Easement area shall be exclusive, except for: (1) the Lot Owner's responsibility for the grading, berming, installing of topsoil and seed/sod, installing plants, trees and related landscape materials, installing and maintaining sprinklers, replanting, replacing and sprinklers and other landscape maintenance activities; (2) such other easements as may be dedicated on the CSM with respect to the same area or any portion thereof; and (3) such other uses of the Landscape Easement area that may be approved by the Village and subject to any conditions set forth by the Village. In the event of any conflicts between the rights of the Village pursuant to the Dedicated 20' Landscape, Berm, Plantings, Access and Maintenance Easement and the rights of other persons or entities with respect to the Easement, the Village's rights under this Easement shall be deemed to be superior.

these

This instrument was drafted by Donald C. Chaput Registered Land Surveyor S-1316 Drawing No. 1341-grb SHEET 4 OF 10 SF

THE

Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.

- 7. Nonexclusive easements coextensive within the areas shown on this CSM as Dedicated Wetland Preservation and Protection, Access and Maintenance Easement areas are hereby dedicated, given, granted and conveyed by the Owner to the Village for wetland conservancy preservation, protection, and maintenance purposes and uses and for related ingress and egress. Unless the Village exercises the rights granted to it pursuant to these Easements, the Village shall have no obligation to do anything related to its rights under these easements.
- 8. Nonexclusive easements coextensive within the areas shown on this CSM as **Dedicated Floodplain Preservation and Protection, Access and Maintenance Easement** areas are hereby dedicated, given, granted and conveyed by the Owner to the Village for floodplain conservancy preservation, protection, and maintenance purposes and uses and for related ingress and egress. Unless the Village exercises the rights granted to it pursuant to these Easements, the Village shall have no obligation to do anything related to its rights under these easements.
- 9. Nonexclusive easements coextensive with the areas shown on this CSM as **Dedicated Tree and Large Shrubbery Preservation, Protection, Access and Maintenance Easement** areas are hereby dedicated, given, granted and conveyed by the Owner to the Village for tree and large shrubbery protection, preservation and maintenance purposes and uses and for related ingress and egress. Unless the Village exercises the rights granted to it pursuant to these Easements, the Village shall have no obligation to do anything related to its rights

#### **RESTRICTIVE COVENANTS**

1. Majestic Realty Co. (referred to as the Owner) covenants that the Dedicated 40' Sanitary Sewer, Access and Maintenance Easement areas shown on this CSM hereby places restrictions on the referenced land because of the location of this sanitary sewer main easement which was given, granted and conveyed by the previous and current Owners to the Village for public sanitary sewer purposes and system improvements, uses and purposes, and for all related and incidental ingress and egress, construction, installation, repair, alteration, replacements, plantings and maintenance activities to serve the Development as referenced in the Dedications and Easement language on this CSM. The Owner further covenants that there shall be no structures, fences, parking lots, curb and gutter, driveways or landscaping of any kind placed within the easement area without prior written approval of the Village. However, with respect to fences, the Village hereby approves fencing in the area of Owner's site plan that is required to provide a secure yard for Owner's Tenants. Such fencing will have a gate and a lock ("Knox lock") to provide the Village with unrestricted access to the Sanitary Sewer, Access and Maintenance Easement area. Furthermore, in accordance with Owner's approved site plan the Village has agreed to allow for the placement of structures, fences, parking lots, curb and gutter, driveways and landscaping within the sewer easement area. In the event that the Village exercises its rights to maintain, repair or replace the sanitary sewer and related appurtenances, the Village shall repair or replace the sanitary sewer and related appurtenances but the Owner(s) of the affected property, not the Village, shall be responsible for any and all costs associated with the replacement of said fencing, parking lots, driveways or landscaping. This covenant shall run with the land, shall be binding upon the Owner, its successors, successors and assigns and successors-in-title of the land, in their capacity as the Owner of Lot 1, and shall benefit and be enforceable by the Village.

To the extent that the Village performs any sanitary sewer maintenance activities that involves the reconstruction, replacement or repair of the private infrastructure which must be replaced in conducting the sanitary sewer repair or maintenance work, the Owner shall be liable for any costs which may be incurred by the Village for the private infrastructure reconstruction, replacement or repairs, which the Village may recover from such Owner as special assessments or special charges under Section 66.0627 (or successors or similar provisions) of the Wisconsin Statutes or otherwise according to law.

2. The Owner hereby covenants that the **Lot** Owner shall have the obligation of maintaining the **Dedicated Storm Water Drainage, Retention Basin, Access and Maintenance Easement** areas shown on this CSM in a functional, neat and nuisance free condition to handle storm water in the Development. Such maintenance shall include, without limitation and as needed, grading, seeding or sodding, maintaining erosion control methods to protect the drainageways; ditching to re-establish design capacity; removing of trash, debris, leaves and brush; clearing, repairing and replacing inlets, outlets and catch basin structures; mowing; and weeding to prevent nuisance conditions. The Owner further covenants that there shall be no structures, fences, gates, signs, berming or altering of the grade of the land within the Easement area which blocks, diverts or re-routs the drainage flow or which might interfere with the storm water function and flow, unless express written approval is granted by the Village and subject to any such conditions as the Village may impose. This covenant shall run with the land, shall be binding upon the Owner, its successors, successors and assigns and successors-in-title of the land, in their capacity as the Owner of Lot 1, and shall benefit and be enforceable by the Village.

> This instrument was drafted by Donald C. Chaput Registered Land Surveyor S-1316

Drawing No. 1341-grb SHEET 5 OF 10 SHEET<sup>S</sup>

Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.

To the extent that the Village performs any such storm water drainage or retention basin related maintenance activities as a result of Owner's failure to do so, the Owner's shall be liable for any costs which may be incurred by the Village, which the Village may recover from such Owner's as special assessments or special charges under Section 66.0627 (or successors or similar provisions) of the Wisconsin Statutes or otherwise according to law. Unless the Village exercises the rights granted to it in the Dedications and Easement Provisions on this CSM, the Village shall have no obligation to do anything pursuant to its rights under these easement dedications.

3. The Owner hereby covenants that the Lot 1 Owner shall have the obligation of planting, maintaining and replacing the Street Trees located within the County Trunk Highway H right-of-way area shown on this CSM. Such planting and maintenance shall include without limitation and as needed planting, staking, mulching, weeding, pruning, watering, replanting, and removing of trash, debris, leaves and brush around the trees in order to prevent a nuisance condition. No driveways, signage, mail boxes, parking areas, structures or fences shall be erected within the right-of-ways, which might damage the street trees or might interfere with Kenosha County's or the Village's rights to maintain the public street improvements, unless approved by Kenosha County and the Village. This covenant shall run with the land, shall be binding upon the Owner, its successors, successors and assigns and successors-in-title of the land, in their capacity as the Owner of Lot 1, and shall benefit and be enforceable by the Village. Such street tree planting and maintenance shall be performed regularly, without compensation, and to the satisfaction of the Village.

To the extent that the Village performs any such street tree related maintenance activities as a result of Owner's failure to do so, the Owner shall be liable for any costs which may be incurred by the Village, which the Village may recover from such Owners as special assessments or special charges under Section 66.0627 (or successors or similar provisions) of the Wisconsin Statutes or otherwise according to law. Unless the Village exercises the rights granted to it in the Dedications and Easement Provisions on this CSM, the Village shall have no obligation to do anything pursuant to its rights under this easement dedication.

4. The Owner hereby covenants that the Lot 1 Owner shall have the obligation of installing berms, plantings and groundcover, and landscape maintenance activities pursuant to the Village approved Majestic Site and Operational Plan landscape screening requirements within Lot 1 within the Dedicated 20' Landscape, Berm, Plantings, Access and Maintenance Easement on this CSM. Such planting and maintenance shall include without limitation grading, berming, installing of topsoil and seed/sod, installing plants, trees and related landscape materials, installing and maintaining fences or retaining walls, watering, pruning, replanting, and other landscape maintenance activities. No driveways, signage, mail boxes, parking areas, structures or fences shall be erected within the Easement area, which might damage the landscaping or plantings, unless approved by the Village. This covenant shall run with the land, shall be binding upon the Owner, its successors, successors and assigns and successors-in-title of the land, in their capacity as the Owner of Lot 1, and shall benefit and be enforceable by the Village. Such landscape plantings and maintenance shall be performed regularly, without compensation, and to the satisfaction of the Village.

To the extent that the Village performs any such landscape related maintenance activities as a result of Owner's failure to do so in accordance with the Site and Operational Plan, the Owner shall be liable for any costs which may be incurred by the Village, which the Village may recover from such Owners as special assessments or special charges under Section 66.0627 (or successors or similar provisions) of the Wisconsin Statutes or otherwise according to law. Unless the Village exercises the rights granted to it in the Dedications and Easement Provisions on this CSM, the Village shall have no obligation to do anything pursuant to its rights under this easement dedication.

5. The Owner hereby covenants that the Lot 1 Owner shall have the obligation of protecting and preserving the Dedicated Wetland Preservation and Protection, Access and Maintenance Easement areas shown on this CSM. Such maintenance shall include without limitation and as needed removing of dead, dying or decayed trees, plant material or evasive species, planting of wetland plant life as approved by the Village and the Wisconsin Department of Natural Resources, and removing of trash or debris in order to prevent a nuisance condition. No mowing or cutting of the wetlands shall be allowed unless expressly approved by the Village. No signage or fences shall be erected within the wetlands, which may damage the wetland areas. This covenant shall run with the land, shall be binding upon the Owner, its successors and

This instrument was drafted by Donald C. Chaput Registered Land Surveyor S-1316 Drawing No. 1341-grb SHEET 6 OF 10 SF

Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.

assigns and successors-in-title of the land, in their capacity as the Owner of Lot 1, and shall benefit and be enforceable by the Village. The Owner shall perform such maintenance as may be needed, without compensation, and to the satisfaction of the Village.

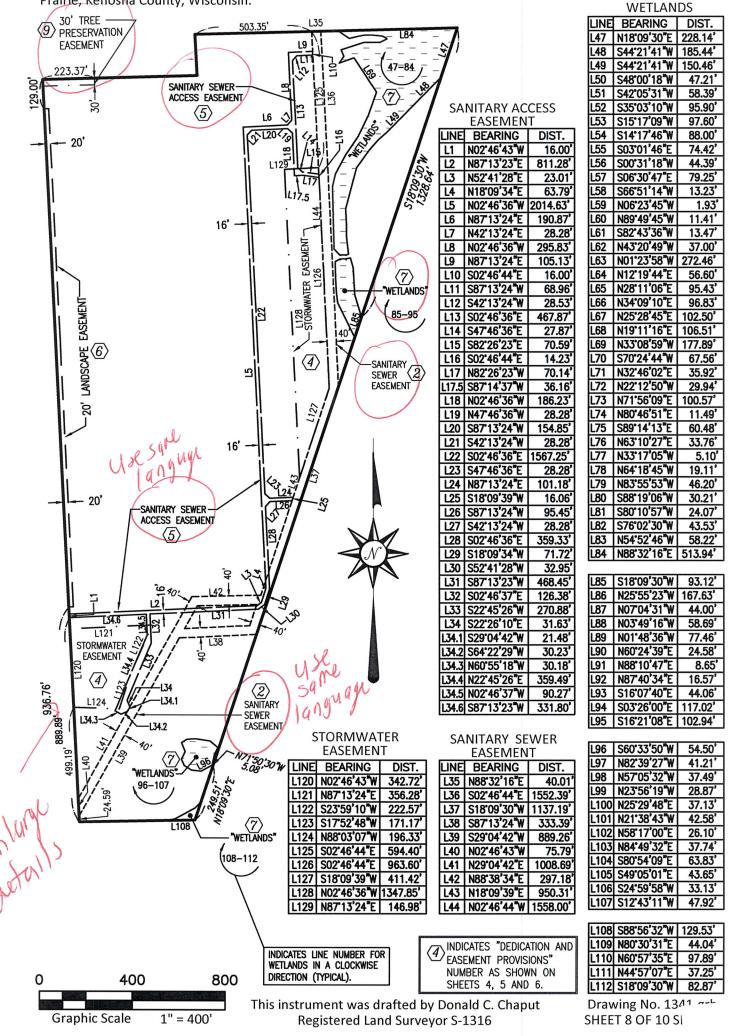
To the extent that the Village performs any such wetland related maintenance activities as a result of Owner's failure to do so, the Owner's shall be liable for any costs which may be incurred by the Village, which the Village may recover from such Owner as special assessments or special charges under Section 66.0627 (or successors or similar provisions) of the Wisconsin Statutes or otherwise according to law. Unless the Village exercises the rights granted to it in the Dedications and Easement Provisions on this CSM, the Village shall have no obligation to do anything pursuant to its rights under these easement dedications.

6. The Owner hereby covenants that the Lot 1 Owner shall have the obligation of protecting and preserving the **Dedicated Floodplain Preservation and Protection, Access and Maintenance Easement** areas shown on this CSM. Such maintenance shall include without limitation and as needed removing of dead, dying or decayed trees which blocks or diverts the natural water flow within the floodplain and removing of trash or debris in the floodplain in order to prevent a nuisance condition. No signage, fences or parking areas shall be installed within the floodplain which may block or redirect overland flow unless approved by the Village. This covenant shall run with the land, shall be binding upon the Owner, its successors, successors and assigns and successors-in-title of the land, in their capacity as the Owner of Lot1, and shall benefit and be enforceable by the Village. The Owner shall perform such maintenance as may be needed, without compensation, and to the satisfaction of the Village.

To the extent that the Village performs any such floodplain related maintenance activities as a result of Owner's failure to do so, the Owner shall be liable for any costs which may be incurred by the Village, which the Village may recover from such Owner as special assessments or special charges under Section 66.0627 (or successors or similar provisions) of the Wisconsin Statutes or otherwise according to law. Unless the Village exercises the rights granted to it in the Dedications and Easement Provisions on this CSM, the Village shall have no obligation to do anything pursuant to its rights under these easement dedications.

This instrument was drafted by Donald C. Chaput Registered Land Surveyor S-1316 Drawing No. 1341-grb SHEET 7 OF 10 S''

Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.



Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.

## **OWNER'S CERTIFICATE**

Majestic Realty Co., duly organized and existing under and by virtue of the laws of the State of Wisconsin, as owner, hereby certifies that said Company caused the land described on this Certified Survey Map to be surveyed, divided, mapped and dedicated as represented on this map in accordance with the requirements of the Village of Pleasant Prairie.

Majestic Realty Co., as owner, does further certify that this map is required by S.236.20 or 236.12 to be submitted to the following for approval of objection: Village of Pleasant Prairie.

IN WITNESS WHEREOF, Majestic Realty Co., has caused these presents to be signed by the hand of \_\_\_\_\_\_ President, on this \_\_\_\_\_ day of \_\_\_\_\_\_, 2013

In the presence of:		
(Witness)	, President	
STATE OF} :SS COUNTY}		
Personally came before me this <u>day of</u> Majestic Realty Co. , to me known as the person acknowledged that he executed the foregoing ins its authority.		
	Notary Public State of Wisconsin My commission expires My commission is permar	
	Ву	lts
	Name:	
STATE OF	Title:	
STATE OF} :SS COUNTY}		
Personally came before me this day of BANK, to me known as the person that he executed the foregoing instrument as such o		
	Notary Public State of Wisconsin My commission expire My commission is peri	

This instrument was drafted by Donald C. Chaput Registered Land Surveyor S-1316 Drawing No. 1341-grb SHEET 9 OF 10 SHEETS

## DRAF'I' ERTIFIED SURVEY MAP NO

**CERTIFIED SURVEY MAP NO.** Part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin.

PLANNING COMMISSION CERTIFICATE OF APPROVAL

APPROVED by the Planning Commission of the Village of Pleasant Prairie on this \_\_\_\_\_ day of \_\_\_\_\_\_, 2013.

Thomas W. Terwall, Chairman

## VILLAGE OF PLEASANT PRAIRIE VILLAGE BOARD APPROVAL

Resolved that this Certified Survey Map, being part of the Southwest 1/4 and Southeast 1/4 of the Northwest 1/4 and part the Northwest 1/4, Northeast 1/4 and Southwest 1/4 of the Southwest 1/4 all in Section 16, Town 1 North, Range 22 East, in the Village of Pleasant Prairie, Kenosha County, Wisconsin, having been approved by the Plan Commission being the same, is hereby approved and accepted by the Village Board of Trustees of the Village of Pleasant Prairie on this \_\_\_\_\_ day of \_\_\_\_\_\_, 2013.

VVV

John P. Steinbrink, Village President

Jane M. Romanowski, Village Clerk

This instrument was drafted by Donald C. Chaput Registered Land Surveyor S-1316 Drawing No. 1341-grb SHEET 10 OF 10 SHEETS

## **ENGINEERING IMPROVEMENT PLANS** FOR **MAJESTIC BADGER LOGISTICS CENTER** PLEASANT PRAIRIE, WISCONSIN

## LEGEND

		EXISTING	PROPOSED
SA	NITARY SEWER MANHOLE	$\bigcirc$	۲
ST	ORM SEWER MANHOLE	_ ©	•
	ORM SEWER AREA DRAIN	C	
		0	I <b>_</b> I
	ORM SEWER INLET (ROUND CASTING)	0	•
ST	ORM SEWER INLET (RECTANGULAR CASTING)		
PR	ECAST FLARED END SECTION	$\triangleleft$	◀
CO	NCRETE HEADWALL	<	Ć
		$\otimes$	•
		-	-
VA	LVE BOX	Ħ	
FIF	RE HYDRANT	Q	≪
BU	FFALO BOX	Φ	•
-			
	EANOUT	0	
SA	NITARY SEWER	$\longrightarrow$	
FO	RCE MAIN		
ST	ORM SEWER	)	
DR	AIN TILE		
		))	20
VV <i>P</i>	ATER MAIN	w	w
	ILITY CROSSING		
			<b>—</b>
LIC	GHTING	-A	•
ELI	ECTRICAL CABLE	—— E——	IEI
OV	ERHEAD WIRES	ОН₩	
CA.	UTION EXISTING UTILITES NEARBY		
	ECTRICAL TRANSFORMER	E	
		^	-
	WER POLE	-0-	- <b>e</b> -
PO	WER POLE WITH LIGHT	- <b>)0</b>	×
ST	REET SIGN	Þ	Þ
GΔ	S MAIN	G	IGI
		- 	
	LEPHONE LINE	I	
CO	NTOUR	749	749
SP	OT ELEVATION	×(750.00)	× 750.00
WE	TLANDS		· _ · _ · _ · _ · _ · _ · _ · _ · _ · _
FI (	OODWAY		
FLC	OODPLAIN		
HI	GH WATER LEVEL (HWL)	0 0 0 0	
NC	RMAL WATER LEVEL (NWL)		
ווס	RECTION OF SURFACE FLOW	<b>&gt;</b>	~
			GRASS PAVEMENT
DI	TCH OR SWALE		
DI	VERSION SWALE		$\rightarrow \rightarrow $
OV	ERFLOW RELIEF ROUTING	$\square$	
	EE WITH TRUNK SIZE	<b>ب</b> 6" ج <sup>س</sup> ر 6"	$\neg$
			<b>⊥</b> <sup>B-1</sup>
SO	IL BORING		<b>-\$</b> -
TO	PSOIL PROBE	- <b>#</b> -•	- <b>#</b> -1
FE	NCE LINE, TEMPORARY SILT	SF	SF
FF	NCE LINE, WIRE	O	o
FEI	NCE LINE, CHAIN LINK OR IRON	X	X
FEI	NCE LINE, WOOD OR PLASTIC		
CO	NCRETE SIDEWALK		
CU	RB AND GUTTER		
CU	RD AND GUTTER		
	PRESSED CURB	<u></u> <u></u>	
RE	VERSE PITCH CURB & GUTTER		
EA	SEMENT LINE		
		TTONC	
	ABBREVIA	<b>SUDITIE</b>	
BL			
C C 8			F CURVATURE
CB			F VERTICAL INTERSECTI
CL	CENTERLINE	R RADIUS	
D		ROW RIGHT-C	
		SAN SANITAR ST STORM S	RY SEWER SEWER
EP FF	FINISHED FLOOR		CY OF CURVE
EP FF FG		T TANGEN	BANK
FF FG FL	FINISHED GRADE FLOW LINE	TB TOP OF I	01 J D D
FF FG FL FP	FINISHED GRADE FLOW LINE FLOODPLAIN	TB TOP OF I TC TOP OF (	
FF FG FL	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME	TB TOP OF I TC TOP OF (	FOUNDATION
FF FG FL FP FR FW HW	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY VL HIGH WATER LEVEL	TBTOP OF ITCTOP OF ITFTOP OF ITPTOP OF ITSTOP OF I	FOUNDATION PIPE SIDEWALK
FF FG FL FP FR FW HW IN	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME / FLOODWAY /L HIGH WATER LEVEL / INVERT	TBTOP OF ITCTOP OF ITFTOP OF ITPTOP OF ITSTOP OF ITWTOP OF I	FOUNDATION PIPE SIDEWALK WALK
FF FG FL FP FR FW HW IN L	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME / FLOODWAY /L HIGH WATER LEVEL / INVERT LENGTH OF CURVE	TBTOP OF ITCTOP OF ITFTOP OF ITPTOP OF ITSTOP OF ITWTOP OF IWMWATER I	FOUNDATION PIPE SIDEWALK WALK MAIN
FF FG FL FP FR FW HW IN	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME / FLOODWAY /L HIGH WATER LEVEL / INVERT LENGTH OF CURVE	TBTOP OF ITCTOP OF ITFTOP OF ITPTOP OF ITSTOP OF ITWTOP OF IWMWATER I	FOUNDATION PIPE SIDEWALK WALK
FF FG FL FP FR FW HW IN L	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME / FLOODWAY /L HIGH WATER LEVEL / INVERT LENGTH OF CURVE	TBTOP OF ITCTOP OF ITFTOP OF ITPTOP OF ITSTOP OF ITWTOP OF IWMWATER I	FOUNDATION PIPE SIDEWALK WALK MAIN
FF FG FL FP FR FW HW IN L	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME / FLOODWAY /L HIGH WATER LEVEL / INVERT LENGTH OF CURVE	TBTOP OF ITCTOP OF ITFTOP OF ITPTOP OF ITSTOP OF ITWTOP OF IWMWATER I	FOUNDATION PIPE SIDEWALK WALK MAIN
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY VL HIGH WATER LEVEL V INVERT LENGTH OF CURVE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I △ INTERSE	FOUNDATION PIPE SIDEWALK WALK MAIN CTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME / FLOODWAY /L HIGH WATER LEVEL / INVERT LENGTH OF CURVE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I △ INTERSE	FOUNDATION PIPE SIDEWALK WALK MAIN CTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY FLOODWAY VL HIGH WATER LEVEL INVERT LENGTH OF CURVE MANHOLE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE	FOUNDATION PIPE SIDEWALK WALK MAIN CTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY VL HIGH WATER LEVEL V INVERT LENGTH OF CURVE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE	FOUNDATION PIPE SIDEWALK WALK MAIN CTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY FLOODWAY VL HIGH WATER LEVEL INVERT LENGTH OF CURVE MANHOLE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE	FOUNDATION PIPE SIDEWALK WALK MAIN CTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY FLOODWAY VL HIGH WATER LEVEL INVERT LENGTH OF CURVE MANHOLE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE	FOUNDATION PIPE SIDEWALK WALK MAIN CTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY FLOODWAY VL HIGH WATER LEVEL INVERT LENGTH OF CURVE MANHOLE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE	FOUNDATION PIPE SIDEWALK WALK MAIN CTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY FLOODWAY VL HIGH WATER LEVEL INVERT LENGTH OF CURVE MANHOLE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE	FOUNDATION PIPE SIDEWALK WALK MAIN CTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY FLOODWAY VL HIGH WATER LEVEL INVERT LENGTH OF CURVE MANHOLE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE RUCTION	FOUNDATION PIPE SIDEWALK WALK MAIN CCTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME FLOODWAY FLOODWAY VL HIGH WATER LEVEL INVERT LENGTH OF CURVE MANHOLE	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I WM WATER I MM WATER I MM WATER I INTERSE RUCTION FUCTION TOIL Free (800	FOUNDATION PIPE SIDEWALK WALK MAIN ECTION ANGLE
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME THE FLOODWAY THE HIGH WATER LEVEL TO INVERT LENGTH OF CURVE MANHOLE FOR REVIEV NOT FOR CONSTI	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I MM WATER I MINTERSE TOI Free (800 Milwaukee Area (4	FOUNDATION PIPE SIDEWALK WALK MAIN ECTION ANGLE <b>HOTLINE</b> <b>JOURDATION</b> <b>JOURDATION</b>
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME THE FLOODWAY THE HIGH WATER LEVEL TO INVERT LENGTH OF CURVE MANHOLE FOR REVIEV NOT FOR CONSTI	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE <b>RUCTION</b> <b>FUCTION</b> <b>TOI Free (800</b> Milwaukee Area (4 earing Impaired TDI	FOUNDATION PIPE SIDEWALK WALK MAIN CCTION ANGLE <b>HOTLINE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b>
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME VENTION FLOODWAY VIENTION INVERT LENGTH OF CURVE MANHOLE FOR REVIEV NOT FOR CONSTI	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TW TOP OF I WM WATER I MM WATER I MINTERSE TOI Free (800 Milwaukee Area (4	FOUNDATION PIPE SIDEWALK WALK MAIN CCTION ANGLE <b>HOTLINE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b>
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME THE FLOODWAY THE HIGH WATER LEVEL TO INVERT LENGTH OF CURVE MANHOLE FOR REVIEV NOT FOR CONSTI	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE <b>RUCTION</b> <b>FUCTION</b> <b>TOI Free (800</b> Milwaukee Area (4 earing Impaired TDI	FOUNDATION PIPE SIDEWALK WALK MAIN ECTION ANGLE <b>HOTLINE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b>
FF FG FL FP FR FW HW IN L MH	FINISHED GRADE FLOW LINE FLOODPLAIN FRAME VENTION FLOODWAY VIENTION INVERT LENGTH OF CURVE MANHOLE FOR REVIEV NOT FOR CONSTI	TB TOP OF I TC TOP OF I TF TOP OF I TP TOP OF I TS TOP OF I TS TOP OF I TS TOP OF I TW TOP OF I WM WATER I A INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE INTERSE IN	FOUNDATION PIPE SIDEWALK WALK MAIN CCTION ANGLE <b>HOTLINE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b> <b>JUNE</b>

TO TAKE WHATEVER STEPS NECESSARY TO RESOLVE THEM. FAILURE TO PROMPTLY NOTIFY THE ENGINEER OF SUCH CONDITIONS SHALL ABSOLVE THE ENGINEER FROM ANY RESPONSIBILITY FOR THE CONSEQUENCES OF SUCH FAILURE. ACTIONS TAKEN WITHOUT THE KNOWLEDGE AND CONSENT TO THE ENGINEER, OR IN CONTRADICTION TO THE ENGINEER'S DELIVERABLES OR RECOMMENDATIONS, SHALL

FURTHERMORE, PINNACLE ENGINEERING GROUP, LLC IS NOT RESPONSIBLE FOR CONSTRUCTION SAFETY OR THE MEANS AND METHODS OF

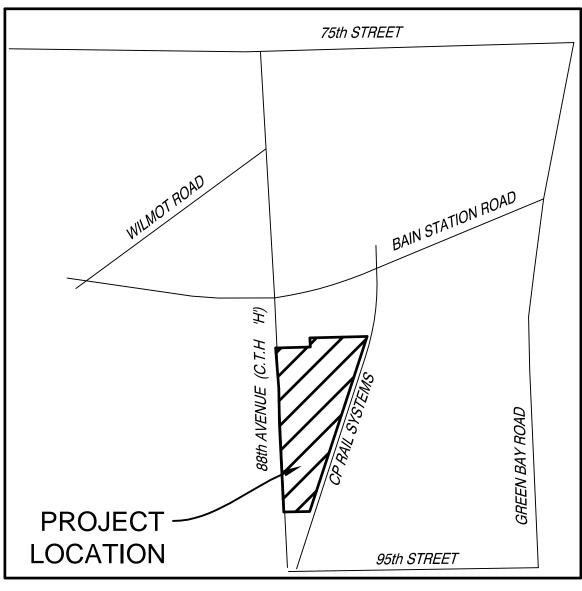
BECOME THE RESPONSIBILITY NOT OF THE ENGINEER BUT OF THE PARTIES RESPONSIBLE FOR TAKING SUCH ACTION.

CONSTRUCTION.

PLANS PREPARED FOR

## **MAJESTIC REALTY COMPANY**

13191 CROSSROAD PARKWAY | SIXTH FLOOR CITY OF INDUSTRY, CA 91746



LOCATION MAP

## CONTACTS

ADAM ARTZ, P.E. PINNACLE ENGINEERING GROUP 15850 BLUEMOUND ROAD, SUITE 310 BROOKFIELD, WI 53005 (262) 754-8888 BRIDGET SCHUH, P.E.

PINNACLE ENGINEERING GROUP 15850 BLUEMOUND ROAD, SUITE 310 BROOKFIELD, WI 53005 (262) 754-8888

JOHN SEMCKEN III, VICE PRESIDENT MAJESTIC REALTY COMPANY 13191 CROSSROADS PKWY NORTH | 6TH FLOOR CITY OF INDUSTRY, CA 91746 (562) 692-9581

### **BENCHMARKS**

BENCHMARK 1: NW FLANGE ON HYDRANT ELEVATION = 687.11
BENCHMARK 2: NW FLANGE ON HYDRANT ELEVATION = 694.94
BENCHMARK 3: NW FLANGE ON HYDRANT ELEVATION = 694.24
BENCHMARK 4: NW FLANGE ON HYDRANT ELEVATION = 691.82
BENCHMARK 5: NW FLANGE ON HYDRANT ELEVATION = 691.81
BENCHMARK 6: NW FLANGE ON HYDRANT ELEVATION = 705.66

## **GENERAL NOTES**

- 1. THE INTENTION OF THE PLANS AND SPECIFICATIONS IS TO SET FORTH PERFORMANCE AND CONSTRUCTION MATERIAL STANDARDS FOR THE PROPER EXECUTION OF WORK. ALL WOR SPECIFICATIONS SHALL BE COMPLETED IN ACCORDANCE WITH ALL REG OR OTHER GOVERNING AGENCY'S LAWS, REGULATIONS, JURISDICTIONA OWNER'S DIRECTION.
- 2. A GEOTECHNICAL REPORT HAS BEEN PREPARED BY GILES ENGINEERIN FOR THE PROJECT SITE. THE DATA ON SUB-SURFACE SOIL CONDITIONS WARRANTY OF THE CONTINUITY OF SUCH CONDITIONS BETWEEN BORIN SHALL BE EXPRESSLY UNDERSTOOD THAT OWNER WILL NOT BE RES CONCLUSIONS DRAWN THERE FROM BY THE CONTRACTOR. DATA IS MAD CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR PERFORMING FEEL IS NECESSARY FOR THE PROPER EVALUATION OF THE SITE CONSTRUCTING THE PROJECT AT NO ADDITIONAL COST TO THE OWNER.
- 3. THE CONTRACTOR IS RESPONSIBLE TO REVIEW AND UNDERSTAND SPECIFICATIONS, INCLUDING FIELD VERIFYING SOIL CONDITIONS, PRIOR
- 4. THE CONTRACTOR SHALL PROMPTLY REPORT ANY ERRORS OR AMBIGUI PLANS, SPECIFICATIONS, REPORTS AND FIELD INVESTIGATIONS.
- 5. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE COMPUTATION COMPLETE THIS PROJECT. THE CONTRACTOR'S BID SHALL BE BASED O INSTANCE RELY ON THE ENGINEER'S ESTIMATE.
- 6. QUESTIONS/CLARIFICATIONS WILL BE INTERPRETED BY ENGINEER/OW ENGINEER/OWNER WILL SUBMIT OFFICIAL RESPONSES IN WRITING. RESPONSES SHALL BE BINDING ON ALL PARTIES ASSOCIATED WORD-OF-MOUTH DIALOG CONSTITUTE AN OFFICIAL RESPONSE.
- 7. PRIOR TO START OF WORK, CONTRACTOR SHALL BE COMPLETELY FAMIL SHALL ACCOUNT FOR CONDITIONS THAT AFFECT, OR MAY AFFECT CONS LIMITATIONS OF WORK ACCESS, SPACE LIMITATIONS, OVERHEAD REQUIREMENTS, ADJACENT ACTIVITIES, ETC. FAILURE TO CONSIDER CLAIM OF JOB EXTRAS.
- 8. COMMENCEMENT OF CONSTRUCTION SHALL EXPLICITLY CONFIRM TH PLANS AND SPECIFICATIONS IN ENTIRETY AND CERTIFIES THAT TH PROVISIONS TO COMPLETE THE PROJECT, WITH THE EXCEPTION APPLICABLE PERMITS HAVE BEEN OBTAINED; AND CONTRACTOR UNDER PROJECT.
- 9. SHOULD ANY DISCREPANCIES OR CONFLICTS IN THE PLANS OR SPECIFIC OF CONTRACT, ENGINEER SHALL BE NOTIFIED IN WRITING IMMEDIATELY THE DISCREPANCIES/CONFLICTS SHALL NOT COMMENCE, OR CONT ENGINEER/OWNER IS DISTRIBUTED. IN THE EVENT OF A CONFLICT BE SPECIFICATIONS AND PLANS, THE ONE ESTABLISHING THE MOST STRING
- 10. THE CONTRACTOR SHALL, AT ITS OWN EXPENSE, OBTAIN ALL NECESSAR PROJECT. OBTAINING PERMITS, OR DELAYS, IS NOT CAUSE FOR I CONTRACTOR SHALL COMPLY WITH ALL PERMIT REQUIREMENTS.
- 11. THE CONTRACTOR SHALL NOTIFY ALL INTERESTED GOVERNING AGENC CONSTRUCTION PROJECT, AND DIGGER'S HOTLINE IN ADVANCE JURISDICTIONAL ORDINANCES/CODES/RULES/ETC., PERMIT STIPULATION
- 12. SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTR INSTITUTE, ENFORCE, MAINTAIN, AND SUPERVISE ALL SAFETY PRECAU
- CONNECTION WITH THE WORK. 13. CONTRACTOR SHALL KEEP THE JOBSITE CLEAN AND ORDERLY AT ALL KEPT IN A WORKING MANNER SUCH THAT DEBRIS IS REMOVED CONTINUE OPERATE UNDER GENERAL "GOOD HOUSEKEEPING."
- 14. THE CONTRACTOR SHALL INDEMNIFY THE OWNER, ENGINEER, AND THEIR AGENTS FROM ALL LIABILITY INVOLVED WITH THE CONSTRUCTION, INSTALLATION, AND TESTING OF THE WORK ON THIS PROJECT.

## **INDEX OF SHEETS**

C-1	COVER SHEET
C-2	EXISTING CONDITIONS
C-3 - C-8	SITE DIMENSIONAL AND PAVING PLAN
C-9 - C-14	GRADING PLAN
C-15 - C-20	UTILITY PLAN
C-21 - C-26	SITE STABILIZATION
C-27 - C-31	CONSTRUCTION DETAILS

PERFORMANCE AND CONSTRUCTION MATERIAL ORKS CONTAINED WITHIN THE PLANS AND EQUIREMENTS FROM LOCAL, STATE, FEDERAL JAL ORDINANCES/CODES/RULES/ETC., AND THE
ING ASSOCIATES, DATED SEPTEMBER 12, 2012, S IS NOT INTENDED AS A REPRESENTATION OR RINGS OR INDICATED SAMPLING LOCATIONS. IT ESPONSIBLE FOR ANY INTERPRETATIONS OR NDE AVAILABLE FOR THE CONVENIENCE OF THE ANY ADDITIONAL SOILS INVESTIGATIONS THEY FOR PURPOSES OF PLANNING, BIDDING, OR R.
ND ALL COMPONENTS OF THE PLANS AND R TO SUBMISSION OF A BID PROPOSAL.
UITIES LEARNED AS PART OF THEIR REVIEW OF
ON OF QUANTITIES AND WORK REQUIRED TO ON ITS OWN COMPUTATIONS AND IN NO SUCH
WNER PRIOR TO THE AWARD OF CONTRACT. INTERPRETATIONS PRESENTED IN OFFICIAL WITH THE CONTRACT. IN NO WAY SHALL
MILIAR WITH ALL CONDITIONS OF THE SITE, AND NSTRUCTION INCLUDING, BUT NOT LIMITED TO, OBSTRUCTIONS, TRAFFIC PATTERNS, LOCAL SITE CONDITIONS SHALL NOT BE CAUSE FOR
THAT THE CONTRACTOR HAS REVIEWED THE THEIR SUBMITTED BID PROPOSAL CONTAINS I OF UNFORESEEN FIELD CONDITIONS; ALL RSTANDS ALL OF THE REQUIREMENTS OF THE
FICATIONS BE DISCOVERED AFTER THE AWARD Y AND CONSTRUCTION OF ITEMS AFFECTED BY ITINUE, UNTIL A WRITTEN RESPONSE FROM BETWEEN REFERENCED CODES, STANDARDS, GENT REQUIREMENTS SHALL BE FOLLOWED.
RY PERMITS AND LICENSES TO COMPLETE THE DELAY OF THE CONTRACT OR SCHEDULE.
ICIES, UTILITY COMPANIES AFFECTED BY THIS OF CONSTRUCTION TO COMPLY WITH ALL NS, AND OTHER APPLICABLE STANDARDS.
TRACTOR SHALL BE RESPONSIBLE TO INITIATE, JUTIONS AND JOB SITE SAFETY PROGRAMS IN
TIMES. ALL LOCATIONS OF THE SITE SHALL BE JUOUSLY AND ALL RESPECTIVE CONTRACTORS

PLAN I DESIGN I DELIVER www.pinnacle-engr.com

WISCONSIN OFFICE:

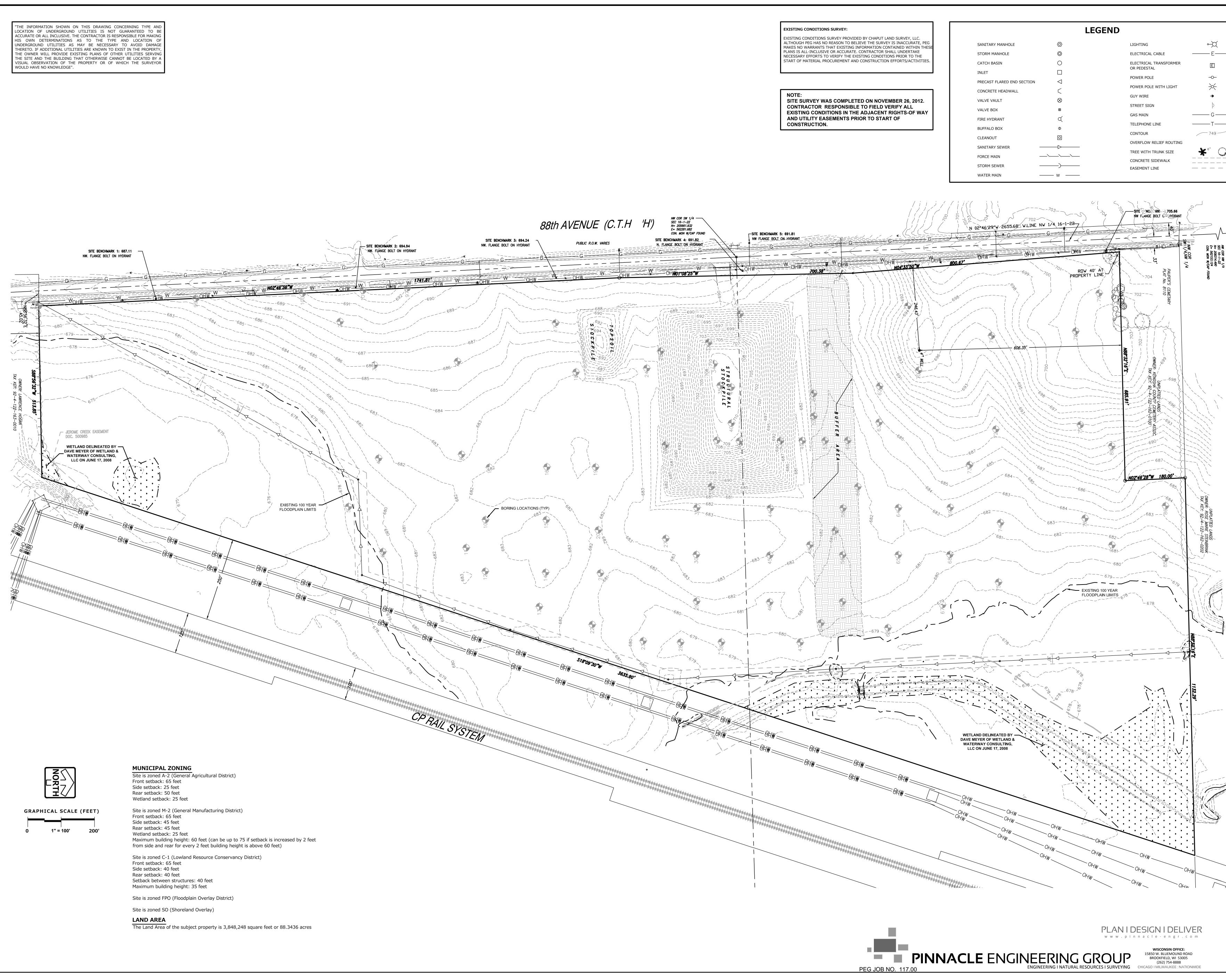
15850 W. BLUEMOUND ROAD

BROOKFIELD, WI 53005

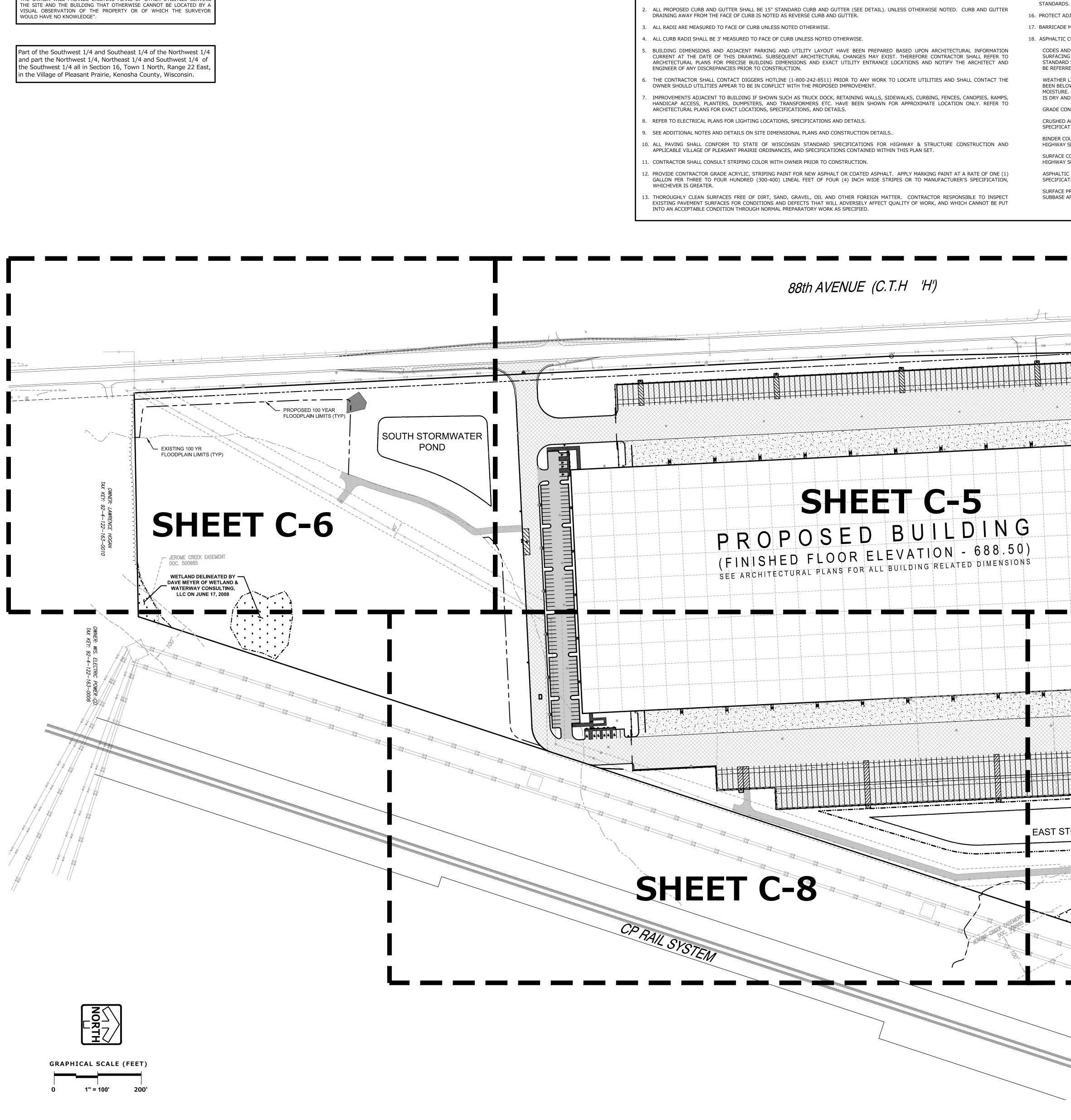
(262) 754-8888

CHICAGO I MILWAUKEE : NATIONWIDE

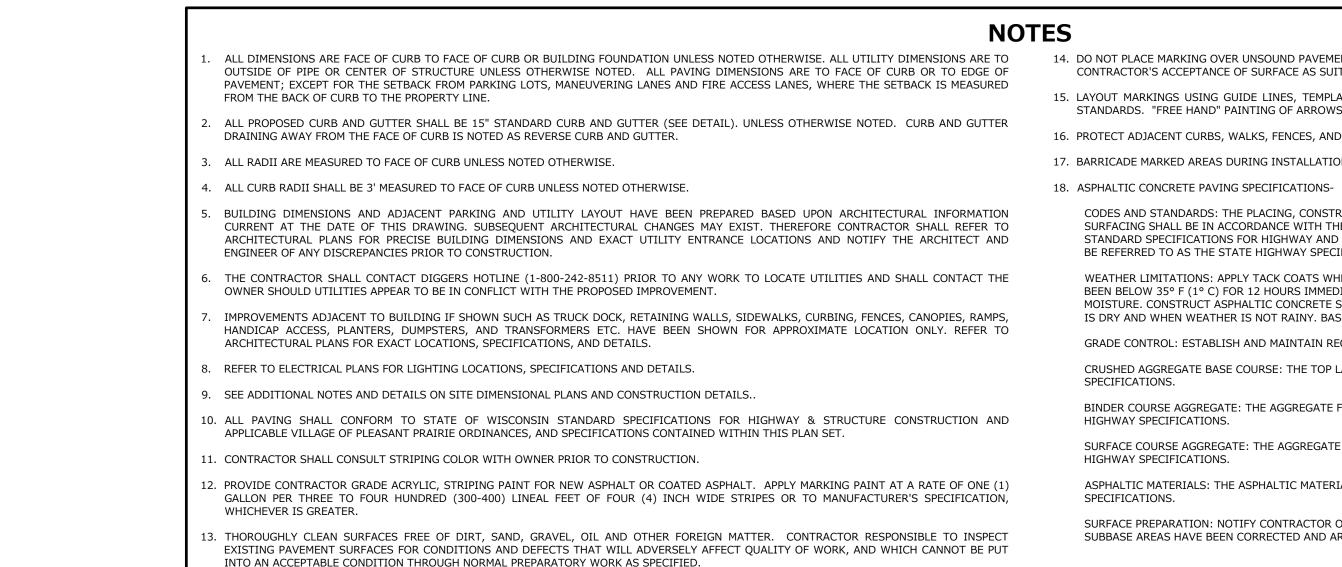
This Document, Either as Drawing or the Assemblage of Electronic Data, is the Property of Commerce Construction Co., L.P. ("Commerce")         This document, including all electronic data attached	tretent, may hor be reproduced, distributed, modified, used or implemented in any way, in whole or in part, without the written consent of Commerce. This document shall not be taken as appurtenant to, associated with, or as a replacement for the officially sealed document, and is	Any berson or organization making use of or relying upon Any person or organization making use of or relying upon this document is responsible for confirming accuracy and completeness. Commerce is not responsible for edited or reproduced versions of dirivial data from this	BY DATE document. The acceptance of use of this document will be construed as an acceptance of the foregoing.		
			BY DATE REVISIONS		
			REVISIONS		
COMMERCE CONSTRUCTION CO., L.P	Sixth Floor City of Industry, California 91746-3497	Telephone: (562) 699-0453 License No. 723302			
COVER SHEET ENGINEERING IMPROVEMENTS PLANS FOR MAJESTIC BADGER LOGISTICS CENTER CTH "H" VILLAGE OF PLEASANT PRAIRIE, WI					
Use this p has not b from origin exactly 1 of drawing DRA' PEG	rawing Ple plot bar to c peen reduce nal. If this plinch long the has change WN BY ,LLC CC JOE <b>17.</b>	1" onfirm dra d or enla e physical d. DAT 4/11/1 3 NO. 00	arged s not size		



This Docurnent, Either as Drawing or the Assemblage of Electronic Data, is the Property of Commerce Construction Co., L.P. ("Commerce") This document, including all electronic data attached therein, may not be reproduced, distributed, modified, used or implemented in any way, in whole or in part, without the written consent of Commerce. This document as a replacement for the officially sealed document, and is wat person or organization making use of or relying upon	
• -	this occument is responsible for continuing its accuracy and completeness. Commerce is not responsible for edited or reproduced versions of digital data from this document. The accentance or use of this document will
NY COR NY 1/4	
COMMERCE CONSTRUCTION CO., L.P. 13191 Crossroads Parkway North Sixth Floor City of Industry, California 91746-3497 Telephone: (562) 699-0453	License No. 723302
ONS TS PLANS FOR	CTH "H" VILLAGE OF PLEASANT PRAIRIE, WI
EXISTING CONDITIONS ENGINEERING IMPROVEMENTS PLANS FOR MAJESTIC BADGER LOGISTICS CENTER	
Drawing Plot B O" Use this plot bar to confir has not been reduced or from original. If this plot exactly 1 inch long the phy of drawing has changed. DRAWN BY DE PEG,LLC 4/1 CCC JOB N 1177.0	1" n drawi <sup>r</sup> enlarg bar is r ysical si DATE



THE INFORMATION SHOWN ON THIS DRAWING CONCERNING TYPE AN LOCATION OF UNDERGROUND UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATIONS AS TO THE TYPE AND LOCATION O UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. IF ADDITIONAL UTILITIES ARE KNOWN TO EXIST IN THE PROPERTY, THE OWNER WILL PROVIDE EXISTING PLANS OF OTHER UTILITIES SERVING



14. DO NOT PLACE MARKING OVER UNSOUND PAVEMENTS. IF THESE CONDITIONS EXIST, NOTIFY OWNER. STARTING INSTALLATION CONSTITUTES CONTRACTOR'S ACCEPTANCE OF SURFACE AS SUITABLE FOR INSTALLATION. 15. LAYOUT MARKINGS USING GUIDE LINES, TEMPLATES AND FORMS. STENCILS AND TEMPLATES SHALL BE PROFESSIONALLY MADE TO INDUSTRY STANDARDS. "FREE HAND" PAINTING OF ARROWS, SYMBOLS, OR WORDING SHALL NOT BE ALLOWED. APPLY STRIPES STRAIGHT AND EVEN.

16. PROTECT ADJACENT CURBS, WALKS, FENCES, AND OTHER ITEMS FROM RECEIVING PAINT.

17. BARRICADE MARKED AREAS DURING INSTALLATION AND UNTIL THE MARKING PAINT IS DRIED AND READY FOR TRAFFIC.

CODES AND STANDARDS: THE PLACING, CONSTRUCTION AND COMPOSITION OF THE ASPHALTIC BASE COURSE AND ASPHALTIC CONCRETE SURFACING SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTIONS 450, 455, 460 AND 465 OF THE STATE OF WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION, EDITION OF 2005. HEREAFTER, THIS PUBLICATION WILL

BE REFERRED TO AS THE STATE HIGHWAY SPECIFICATIONS. WEATHER LIMITATIONS: APPLY TACK COATS WHEN AMBIENT TEMPERATURE IS ABOVE 50° F (10° C) AND WHEN TEMPERATURE HAS NOT BEEN BELOW 35° F (1° C) FOR 12 HOURS IMMEDIATELY PRIOR TO APPLICATION. DO NOT APPLY WHEN BASE IS WET OR CONTAINS EXCESS OF MOISTURE. CONSTRUCT ASPHALTIC CONCRETE SURFACE COURSE WHEN ATMOSPHERIC TEMPERATURE IS ABOVE 40° (4° C) AND WHEN BASE IS DRY AND WHEN WEATHER IS NOT RAINY. BASE COURSE MAY BE PLACED WHEN AIR TEMPERATURE IS ABOVE 30° F (-1° C).

GRADE CONTROL: ESTABLISH AND MAINTAIN REQUIRED LINES AND ELEVATIONS FOR EACH COURSE DURING CONSTRUCTION. CRUSHED AGGREGATE BASE COURSE: THE TOP LAYER OF BASE COURSE SHALL CONFORM TO SECTIONS 301 AND 305, STATE HIGHWAY

SPECIFICATIONS. BINDER COURSE AGGREGATE: THE AGGREGATE FOR THE BINDER COURSE SHALL CONFORM TO SECTIONS 460.2.7 AND 315, STATE

HIGHWAY SPECIFICATIONS. SURFACE COURSE AGGREGATE: THE AGGREGATE FOR THE SURFACE COURSE SHALL CONFORM TO SECTIONS 460.2.7 AND 465, STATE

HIGHWAY SPECIFICATIONS. ASPHALTIC MATERIALS: THE ASPHALTIC MATERIALS SHALL CONFORM TO SECTION 455 AND 460, STATE HIGHWAY

SPECIFICATIONS.

SURFACE PREPARATION: NOTIFY CONTRACTOR OF UNSATISFACTORY CONDITIONS. DO NOT BEGIN PAVING WORK UNTIL DEFICIENT SUBBASE AREAS HAVE BEEN CORRECTED AND ARE READY TO RECEIVE PAVING.

SHEET C-4	OWNER: KENOSHA COUNTY CEMETERY TAX KEY: 92-4-122-162-0100
ORMWATER POND SHEET-C-7	
EXISTING 100 YR FLOODPLAIN LIMITS (TYP)	PROPOSED 100 YEAR FLOODPLAIN LIMITS (TYP)
WETLAND DELINEATED BY DAVE MEYER OF WETLAND & WATERWAY CONSULTING, LLC ON JUNE 17, 2008	

TYPE 1 PAVEMENT (SEE DETAIL) TYPE 2 PAVEMENT (SEE DETAIL) GRAVEL ACCESS ROAD (SEE DETAIL) CONCRETE SIDEWALK (SEE DETAIL) CONCRETE SIDEWALK (SEE DETAIL) **4" SOLID WHITE STRIPE (B)** 4" DIAGONAL AT 45° SPACED 2' O.C.  $\bigcirc$ R7-8 HANDICAP PARKING SIGN (SEE DETAIL) PARKING COUNTS HANDICAP PARKING = 22 CAR PARKING = 283 TRAILER PARKING = 485

LEGEND

PLAN I DESIGN I DELIVER www.pinnacle-engr.com

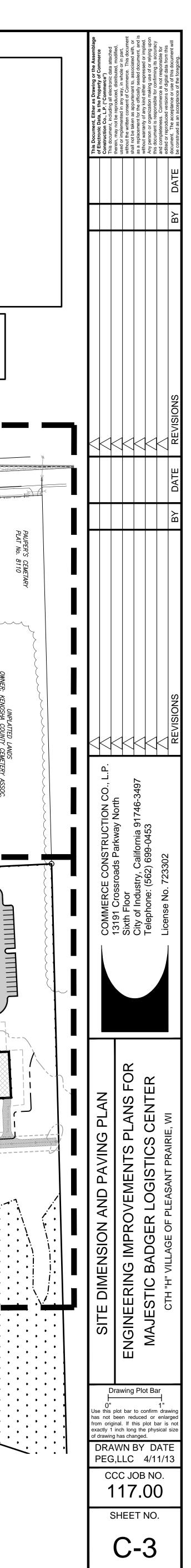
WISCONSIN OFFICE:

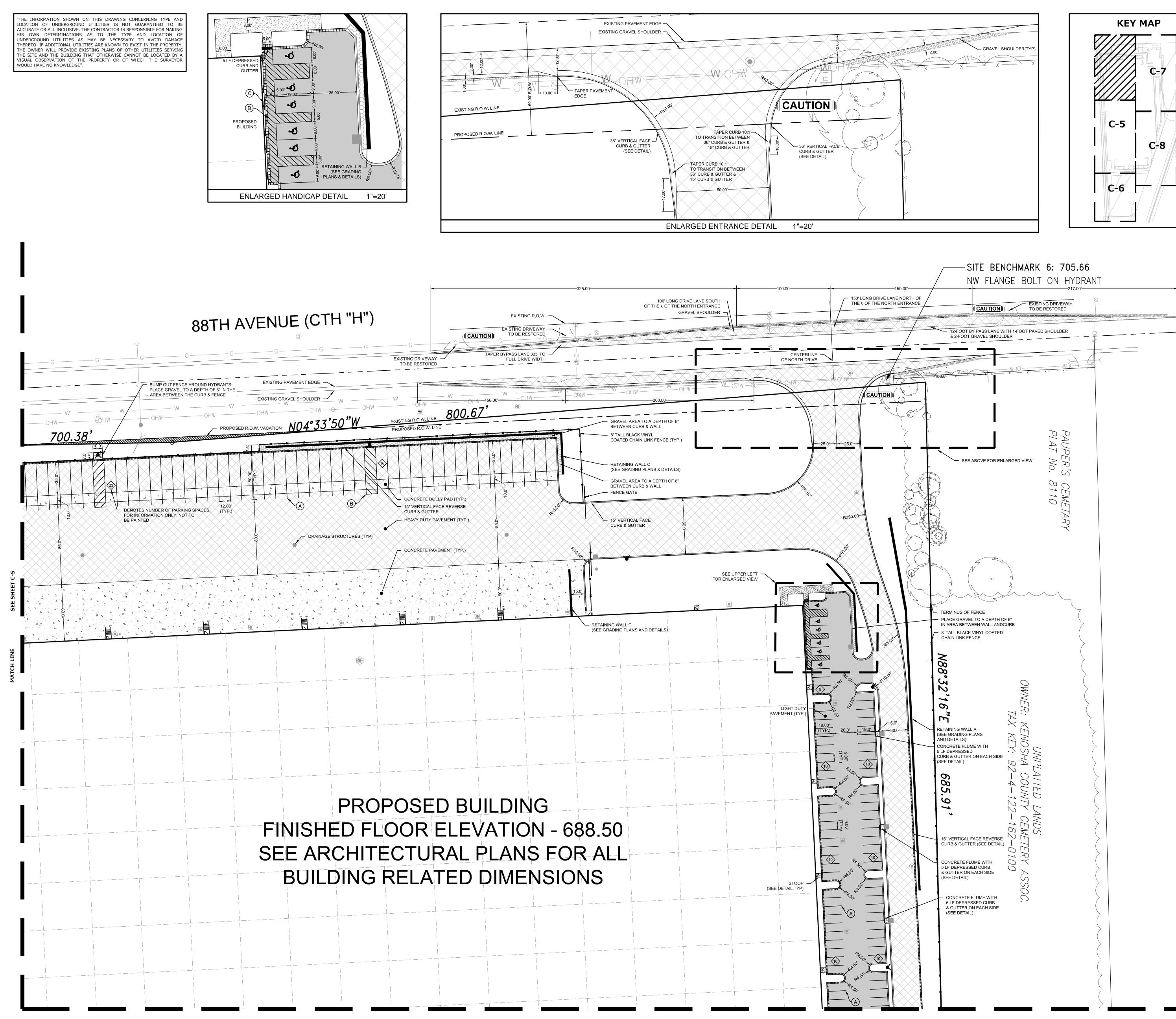
15850 W. BLUEMOUND ROAD

BROOKFIELD, WI 53005 (262) 754-8888

CHICAGO I MILWAUKEE : NATIONWID

**PINNACLE** ENGINEERING GROUP FNGINFFRING I NATURAL RESOURCES I SURVEYING PEG JOB NO. 117.00117.00



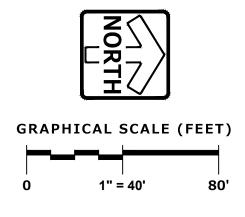


MATCH LINE SEE SHEET C-7

	LEGEND
	LIGHT DUTY PAVEMENT (SEE DETAIL)
	HEAVY DUTY PAVEMENT (SEE DETAIL)
	GRAVEL ACCESS ROAD (SEE DETAIL)
	CONCRETE SIDEWALK (SEE DETAIL)
	CONCRETE SIDEWALK (SEE DETAIL)
A	4" SOLID WHITE STRIPE
B	4" DIAGONAL AT 45° SPACED 2' O.C.
©	R7-8 HANDICAP PARKING SIGN (SEE DETAIL)

## NOTES

- ALL DIMENSIONS ARE FACE OF CURB TO FACE OF CURB OR FOUNDATION UNLESS NOTED OTHERWISE. ALL UTILITY DIMENSION OUTSIDE OF PIPE OR CENTER OF STRUCTURE UNLESS OTHERWISE N PAVING DIMENSIONS ARE TO FACE OF CURB OR TO EDGE OF PAVEMEN FOR THE SETBACK FROM PARKING LOTS, MANEUVERING LANES AND FI LANES, WHERE THE SETBACK IS MEASURED FROM THE BACK OF CU PROPERTY LINE.
- 2. ALL PROPOSED CURB AND GUTTER SHALL BE 15" STANDARD CURB AN (SEE DETAIL). UNLESS OTHERWISE NOTED. CURB AND GUTTER DRAIN FROM THE FACE OF CURB IS NOTED AS REVERSE CURB AND GUTTER.
- ALL RADII ARE MEASURED TO FACE OF CURB UNLESS NOTED OTHERWI
   ALL CURB RADII SHALL BE MEASURED TO FACE OF CURB UNLE OTHERWISE.
- 5. BUILDING DIMENSIONS AND ADJACENT PARKING AND UTILITY LAY BEEN PREPARED BASED UPON ARCHITECTURAL INFORMATION CURRE DATE OF THIS DRAWING. SUBSEQUENT ARCHITECTURAL CHANGES I THEREFORE CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FO BUILDING DIMENSIONS AND EXACT UTILITY ENTRANCE LOCATIONS A THE ARCHITECT AND ENGINEER OF ANY DISCREPANCIES I CONSTRUCTION.
- 6. THE CONTRACTOR SHALL CONTACT DIGGERS HOTLINE (1-800-242-8 TO ANY WORK TO LOCATE UTILITIES AND SHALL CONTACT THE OWN UTILITIES APPEAR TO BE IN CONFLICT WITH THE PROPOSED IMPROVEM
- 7. IMPROVEMENTS ADJACENT TO BUILDING IF SHOWN SUCH AS TRI RETAINING WALLS, SIDEWALKS, CURBING, FENCES, CANOPIES HANDICAP ACCESS, PLANTERS, DUMPSTERS, AND TRANSFORMERS BEEN SHOWN FOR APPROXIMATE LOCATION ONLY. REFER TO ARCH PLANS FOR EXACT LOCATIONS, SPECIFICATIONS, AND DETAILS.
- REFER TO ELECTRICAL PLANS FOR LIGHTING LOCATIONS, SPECIFICA DETAILS.
   SEE ADDITIONAL NOTES AND DETAILS ON SITE DIMENSIONAL I CONSTRUCTION DETAILS..
- 10. ALL PAVING SHALL CONFORM TO STATE OF WISCONSIN SPECIFICATIONS FOR HIGHWAY & STRUCTURE CONSTRUCTION AND VILLAGE OF PLEASANT PRAIRIE ORDINANCES, AND SPECIFICATIONS WITHIN THIS PLAN SET.
- CONTRACTOR SHALL CONSULT STRIPING COLOR WITH OWNER CONSTRUCTION.
   PROVIDE CONTRACTOR GRADE ACRYLIC, STRIPING PAINT FOR NEW A COATED ASPHALT. APPLY MARKING PAINT AT A RATE OF ONE (1) CONTRACTOR CO
- THREE TO FOUR HUNDRED (300-400) LINEAL FEET OF FOUR (4) STRIPES OR TO MANUFACTURER'S SPECIFICATION, WHICHEVER IS GRI
  13. THOROUGHLY CLEAN SURFACES FREE OF DIRT, SAND, GRAVEL, OIL FOREIGN MATTER. CONTRACTOR RESPONSIBLE TO INSPECT EXISTING SURFACES FOR CONDITIONS AND DEFECTS THAT WILL ADVERS QUALITY OF WORK, AND WHICH CANNOT BE PUT INTO AN
- CONDITION THROUGH NORMAL PREPARATORY WORK AS SPECIFIED. 14. DO NOT PLACE MARKING OVER UNSOUND PAVEMENTS. IF THESE C EXIST, NOTIFY OWNER. STARTING INSTALLATION CONSTITUTES CON ACCEPTANCE OF SURFACE AS SUITABLE FOR INSTALLATION.
- 15. LAYOUT MARKINGS USING GUIDE LINES, TEMPLATES AND FORMS. STE TEMPLATES SHALL BE PROFESSIONALLY MADE TO INDUSTRY STANDAR HAND" PAINTING OF ARROWS, SYMBOLS, OR WORDING SHALL NOT BE APPLY STRIPES STRAIGHT AND EVEN.
- 16. PROTECT ADJACENT CURBS, WALKS, FENCES, AND OTHER I RECEIVING PAINT.
- BARRICADE MARKED AREAS DURING INSTALLATION AND UNTIL THE PAINT IS DRIED AND READY FOR TRAFFIC.
   ASPHALTIC CONCRETE PAVING SPECIFICATIONS-
- CODES AND STANDARDS: THE PLACING, CONSTRUCTION AND COMPO THE ASPHALTIC BASE COURSE AND ASPHALTIC CONCRETE SURFACING IN ACCORDANCE WITH THE REQUIREMENTS OF SECTIONS 450, 455 465 OF THE STATE OF WISCONSIN STANDARD SPECIFICATIONS FOR AND STRUCTURE CONSTRUCTION, EDITION OF 2005. HEREAF PUBLICATION WILL BE REFERRED TO AS THE STATE HIGHWAY SPECIFICATION
- WEATHER LIMITATIONS: APPLY TACK COATS WHEN AMBIENT TEMP ABOVE 50° F (10° C) AND WHEN TEMPERATURE HAS NOT BEEN BELO C) FOR 12 HOURS IMMEDIATELY PRIOR TO APPLICATION. DO NOT A BASE IS WET OR CONTAINS EXCESS OF MOISTURE. CONSTRUCT CONCRETE SURFACE COURSE WHEN ATMOSPHERIC TEMPERATURE IS (4° C) AND WHEN BASE IS DRY AND WHEN WEATHER IS NOT R COURSE MAY BE PLACED WHEN AIR TEMPERATURE IS ABOVE 30° F (-GRADE CONTROL: ESTABLISH AND MAINTAIN REQUIRED LINES AND E
- FOR EACH COURSE DURING CONSTRUCTION. CRUSHED AGGREGATE BASE COURSE: THE TOP LAYER OF BASE CO CONFORM TO SECTIONS 301 AND 305, STATE HIGHWAY SPECIFICATI
- BINDER COURSE AGGREGATE: THE AGGREGATE FOR THE BINDER CO CONFORM TO SECTIONS 460.2.7 AND 315, STATE HIGHWAY SPECIFIC SURFACE COURSE AGGREGATE: THE AGGREGATE FOR THE SURFA
- SHALL CONFORM TO SECTIONS 460.2.7 AND 465, STATE SPECIFICATIONS. ASPHALTIC MATERIALS: THE ASPHALTIC MATERIALS SHALL CO
- SECTION 455 AND 460, STATE HIGHWAY SPECIFICATIONS. SURFACE PREPARATION: NOTIFY CONTRACTOR OF UNSAT CONDITIONS. DO NOT BEGIN PAVING WORK UNTIL DEFICIENT SUBB HAVE BEEN CORRECTED AND ARE READY TO RECEIVE PAVING.

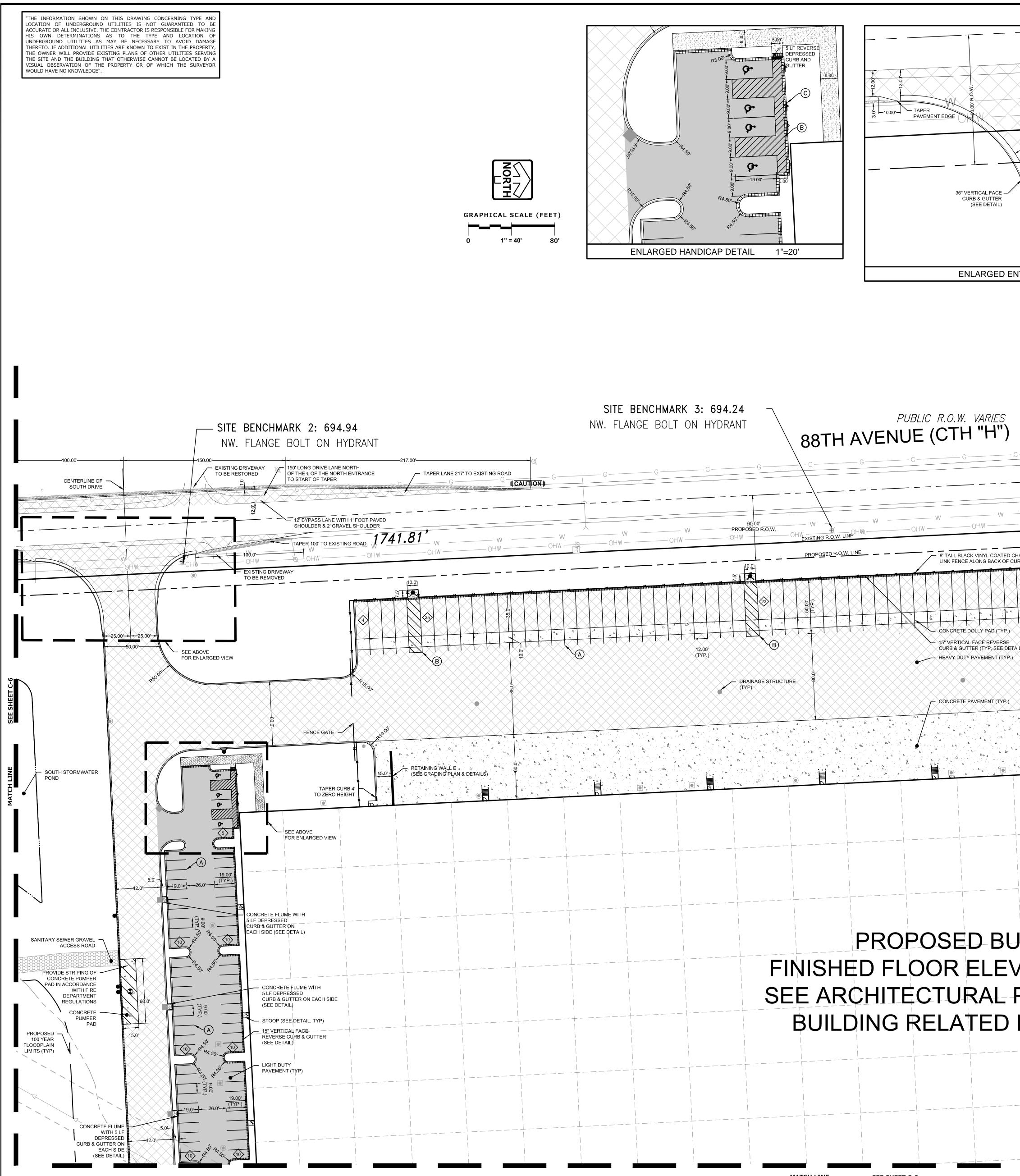


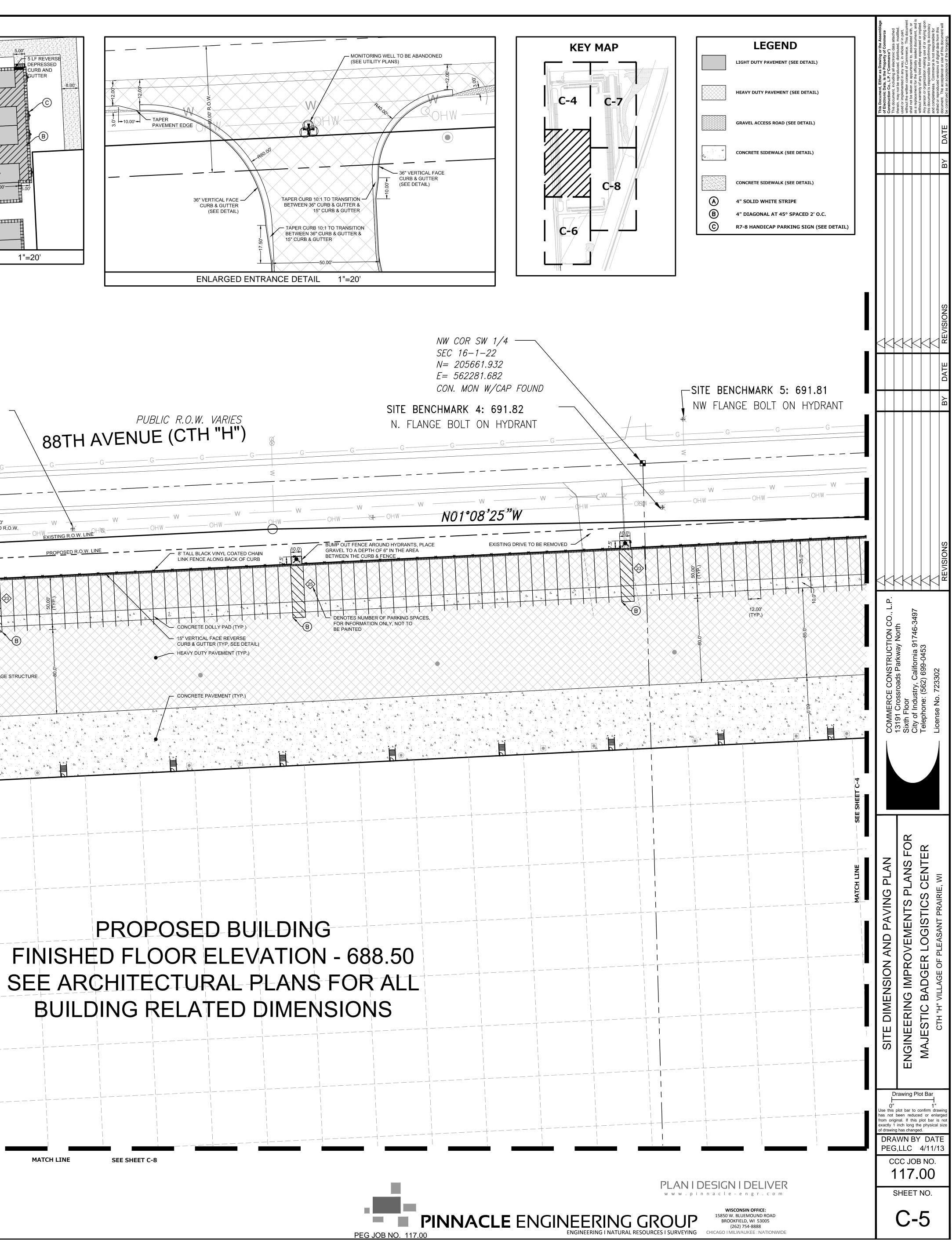
PLANIDESIGNIDELIVER www.pinnacle-engr.com

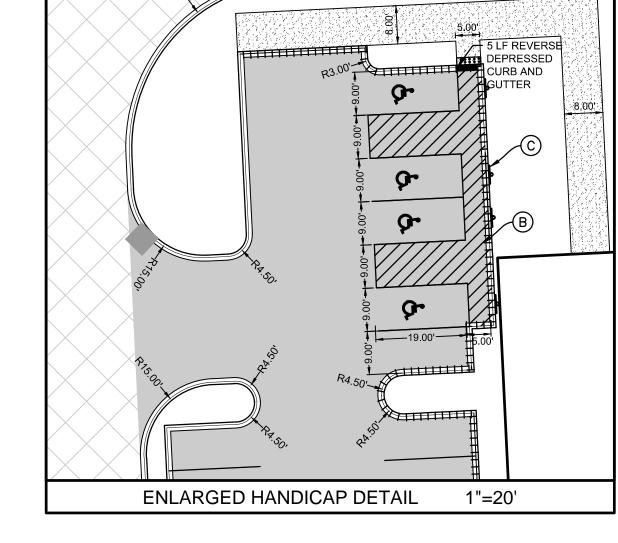
PEG JOB NO. 117.00 PINNACLE ENGINEERING I NATURAL RESOURCES I SURVEYING

15850 W. BLUEMOUND ROAD BROOKFIELD, WI 53005 (262) 754-8888 CHICAGO I MILWAUKEE : NATIONWIE

WISCONSIN OFFICE:



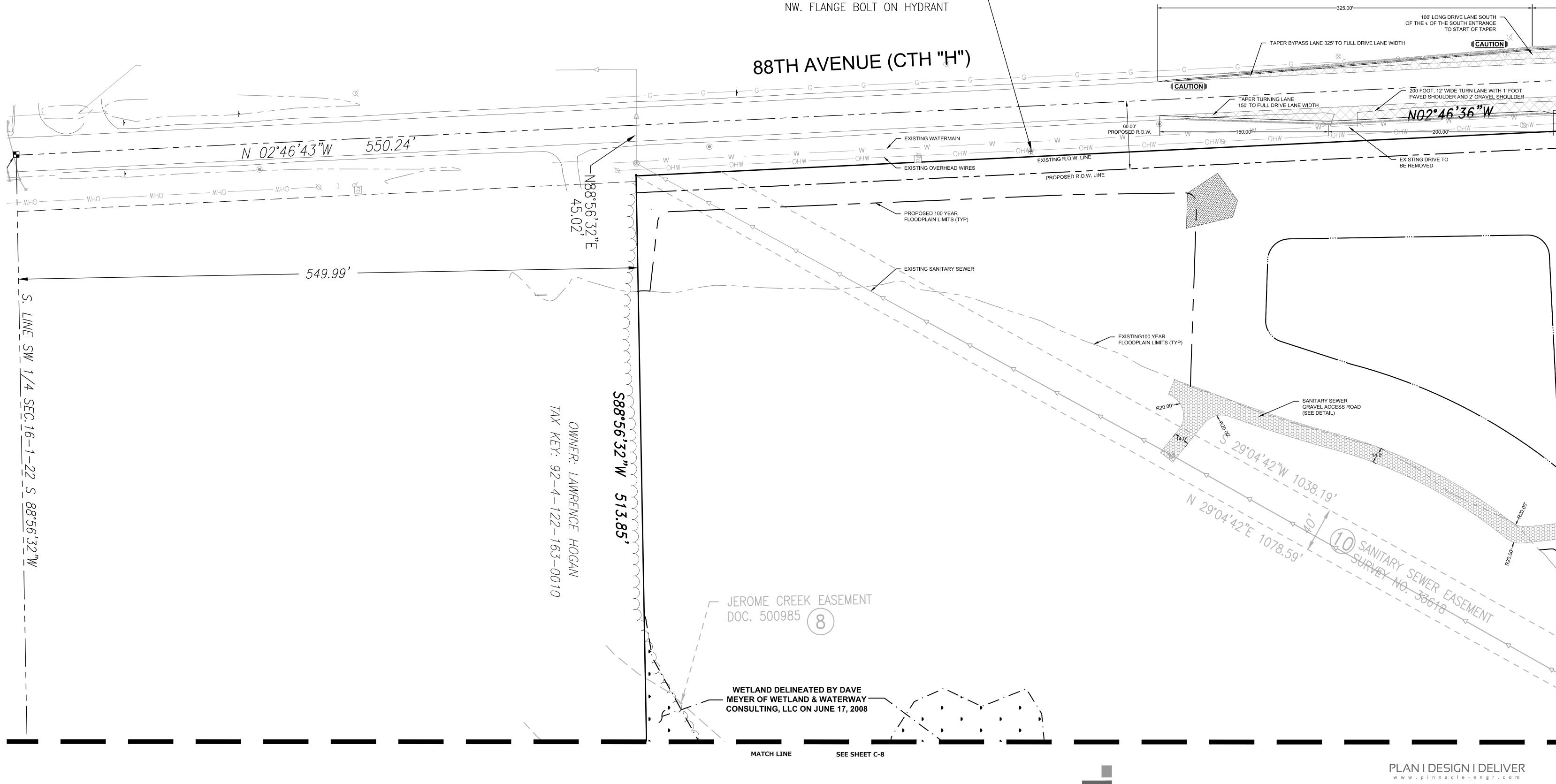




MATCH LINE

THE INFORMATION SHOWN ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATIONS AS TO THE TYPE AND LOCATION O UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. IF ADDITIONAL UTILITIES ARE KNOWN TO EXIST IN THE PROPERTY, THE OWNER WILL PROVIDE EXISTING PLANS OF OTHER UTILITIES SERVING THE SITE AND THE BUILDING THAT OTHERWISE CANNOT BE LOCATED BY A VISUAL OBSERVATION OF THE PROPERTY OR OF WHICH THE SURVEYOR WOULD HAVE NO KNOWLEDGE".

PAVEMENT; EXCEPT FOR THE SETBACK FROM PARKING LOTS, MANEUVERING LANES AND FIRE ACCESS LANES, WHERE THE SETBACK IS MEASURED FROM THE BACK OF CURB TO THE PROPERTY LINE.



### NOTES

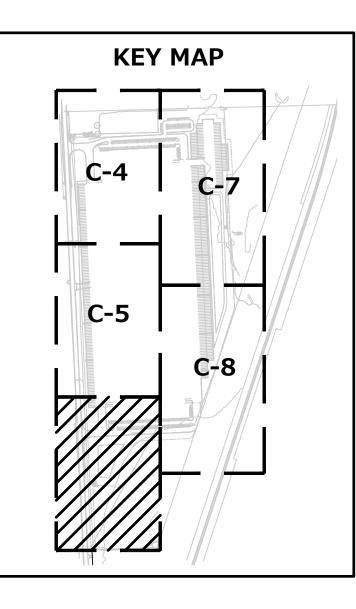
- OUTSIDE OF PIPE OR CENTER OF STRUCTURE UNLESS OTHERWISE NOTED. ALL PAVING DIMENSIONS ARE TO FACE OF CURB OR TO EDGE OF 14. DO NOT PLACE MARKING OVER UNSOUND PAVEMENTS. IF THESE CONDITIONS EXIST, NOTIFY OWNER. STARTING INSTALLATION CONSTITUTES CONTRACTOR'S ACCEPTANCE OF SURFACE AS SUITABLE FOR INSTALLATION.
  - 15. LAYOUT MARKINGS USING GUIDE LINES, TEMPLATES AND FORMS. STENCILS AND TEMPLATES SHALL BE PROFESSIONALLY MADE TO INDUSTRY STANDARDS. "FREE HAND" PAINTING OF ARROWS, SYMBOLS, OR WORDING SHALL NOT BE ALLOWED. APPLY STRIPES STRAIGHT AND EVEN.
  - 16. PROTECT ADJACENT CURBS, WALKS, FENCES, AND OTHER ITEMS FROM RECEIVING PAINT.
  - 17. BARRICADE MARKED AREAS DURING INSTALLATION AND UNTIL THE MARKING PAINT IS DRIED AND READY FOR TRAFFIC. 18. ASPHALTIC CONCRETE PAVING SPECIFICATIONS-
  - CODES AND STANDARDS: THE PLACING, CONSTRUCTION AND COMPOSITION OF THE ASPHALTIC BASE COURSE AND ASPHALTIC CONCRETE SURFACING SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTIONS 450, 455, 460 AND 465 OF THE STATE OF WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION, EDITION OF 2005. HEREAFTER, THIS PUBLICATION WILL BE REFERRED TO AS THE STATE HIGHWAY SPECIFICATIONS.
  - WEATHER LIMITATIONS: APPLY TACK COATS WHEN AMBIENT TEMPERATURE IS ABOVE 50° F (10° C) AND WHEN TEMPERATURE HAS NOT BEEN BELOW 35° F (1° C) FOR 12 HOURS IMMEDIATELY PRIOR TO APPLICATION. DO NOT APPLY WHEN BASE IS WET OR CONTAINS EXCESS OF MOISTURE. CONSTRUCT ASPHALTIC CONCRETE SURFACE COURSE WHEN ATMOSPHERIC TEMPERATURE IS ABOVE 40° (4° C) AND WHEN BASE IS DRY AND WHEN WEATHER IS NOT RAINY. BASE COURSE MAY BE PLACED WHEN AIR TEMPERATURE IS ABOVE 30° F (-1° C). GRADE CONTROL: ESTABLISH AND MAINTAIN REQUIRED LINES AND ELEVATIONS FOR EACH COURSE DURING CONSTRUCTION. CRUSHED AGGREGATE BASE COURSE: THE TOP LAYER OF BASE COURSE SHALL CONFORM TO SECTIONS 301 AND 305, STATE HIGHWAY SPECIFICATIONS.
  - BINDER COURSE AGGREGATE: THE AGGREGATE FOR THE BINDER COURSE SHALL CONFORM TO SECTIONS 460.2.7 AND 315, STATE¥ HIGHWAY SPECIFICATIONS. SURFACE COURSE AGGREGATE: THE AGGREGATE FOR THE SURFACE COURSE SHALL CONFORM TO SECTIONS 460.2.7 AND 465, STATE HIGHWAY
  - SPECIFICATIONS. ASPHALTIC MATERIALS: THE ASPHALTIC MATERIALS SHALL CONFORM TO SECTION 455 AND 460, STATE HIGHWAY SPECIFICATIONS.

SITE BENCHMARK 1: 687.11  $-\gamma$ 

- SURFACE PREPARATION: NOTIFY CONTRACTOR OF UNSATISFACTORY CONDITIONS. DO NOT BEGIN PAVING WORK UNTIL DEFICIENT SUBBASE AREAS HAVE BEEN CORRECTED AND ARE READY TO RECEIVE PAVING.
- ALL PROPOSED CURB AND GUTTER SHALL BE 15" STANDARD CURB AND GUTTER (SEE DETAIL). UNLESS OTHERWISE NOTED. CURB AND GUTTER DRAINING AWAY FROM THE FACE OF CURB IS NOTED AS REVERSE CURB AND GUTTER. ALL RADII ARE MEASURED TO FACE OF CURB UNLESS NOTED OTHERWISE. 4. ALL CURB RADII SHALL BE 3' MEASURED TO FACE OF CURB UNLESS NOTED OTHERWISE. BUILDING DIMENSIONS AND ADJACENT PARKING AND UTILITY LAYOUT HAVE BEEN PREPARED BASED UPON ARCHITECTURAL INFORMATION CURRENT AT THE DATE OF THIS DRAWING. SUBSEQUENT ARCHITECTURAL CHANGES MAY EXIST. THEREFORE CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR PRECISE BUILDING DIMENSIONS AND EXACT UTILITY ENTRANCE LOCATIONS AND NOTIFY THE ARCHITECT AND ENGINEER OF ANY DISCREPANCIES PRIOR TO CONSTRUCTION.
- . THE CONTRACTOR SHALL CONTACT DIGGERS HOTLINE (1-800-242-8511) PRIOR TO ANY WORK TO LOCATE UTILITIES AND SHALL CONTACT THE OWNER SHOULD UTILITIES APPEAR TO BE IN CONFLICT WITH THE PROPOSED IMPROVEMENT.

ALL DIMENSIONS ARE FACE OF CURB TO FACE OF CURB OR BUILDING FOUNDATION UNLESS NOTED OTHERWISE. ALL UTILITY DIMENSIONS ARE TO

- IMPROVEMENTS ADJACENT TO BUILDING IF SHOWN SUCH AS TRUCK DOCK, RETAINING WALLS, SIDEWALKS, CURBING, FENCES, CANOPIES, RAMPS, HANDICAP ACCESS, PLANTERS, DUMPSTERS, AND TRANSFORMERS ETC. HAVE BEEN SHOWN FOR APPROXIMATE LOCATION ONLY. REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS, SPECIFICATIONS, AND DETAILS. 8. REFER TO ELECTRICAL PLANS FOR LIGHTING LOCATIONS, SPECIFICATIONS AND DETAILS.
- 9. SEE ADDITIONAL NOTES AND DETAILS ON SITE DIMENSIONAL PLANS AND CONSTRUCTION DETAILS..
- 10. ALL PAVING SHALL CONFORM TO STATE OF WISCONSIN STANDARD SPECIFICATIONS FOR HIGHWAY & STRUCTURE CONSTRUCTION AND APPLICABLE VILLAGE OF PLEASANT PRAIRIE ORDINANCES, AND SPECIFICATIONS CONTAINED WITHIN THIS PLAN SET. 11. CONTRACTOR SHALL CONSULT STRIPING COLOR WITH OWNER PRIOR TO CONSTRUCTION.
- 2. PROVIDE CONTRACTOR GRADE ACRYLIC, STRIPING PAINT FOR NEW ASPHALT OR COATED ASPHALT. APPLY MARKING PAINT AT A RATE OF ONE (1) GALLON PER THREE TO FOUR HUNDRED (300-400) LINEAL FEET OF FOUR (4) INCH WIDE STRIPES OR TO MANUFACTURER'S SPECIFICATION, WHICHEVER IS GREATER.
- 3. THOROUGHLY CLEAN SURFACES FREE OF DIRT, SAND, GRAVEL, OIL AND OTHER FOREIGN MATTER. CONTRACTOR RESPONSIBLE TO INSPECT EXISTING PAVEMENT SURFACES FOR CONDITIONS AND DEFECTS THAT WILL ADVERSELY AFFECT QUALITY OF WORK, AND WHICH CANNOT BE PUT INTO AN ACCEPTABLE CONDITION THROUGH NORMAL PREPARATORY WORK AS SPECIFIED.

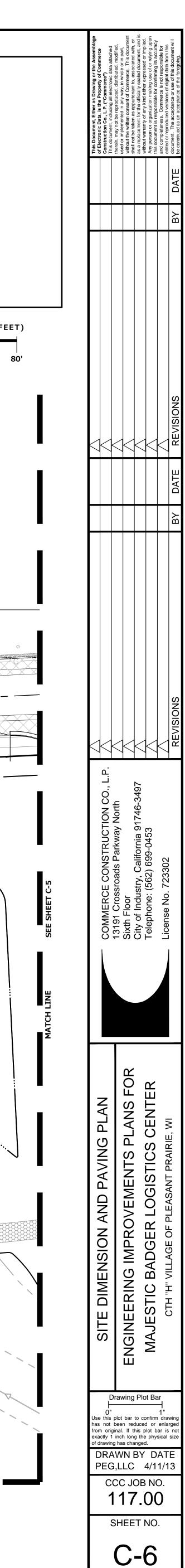


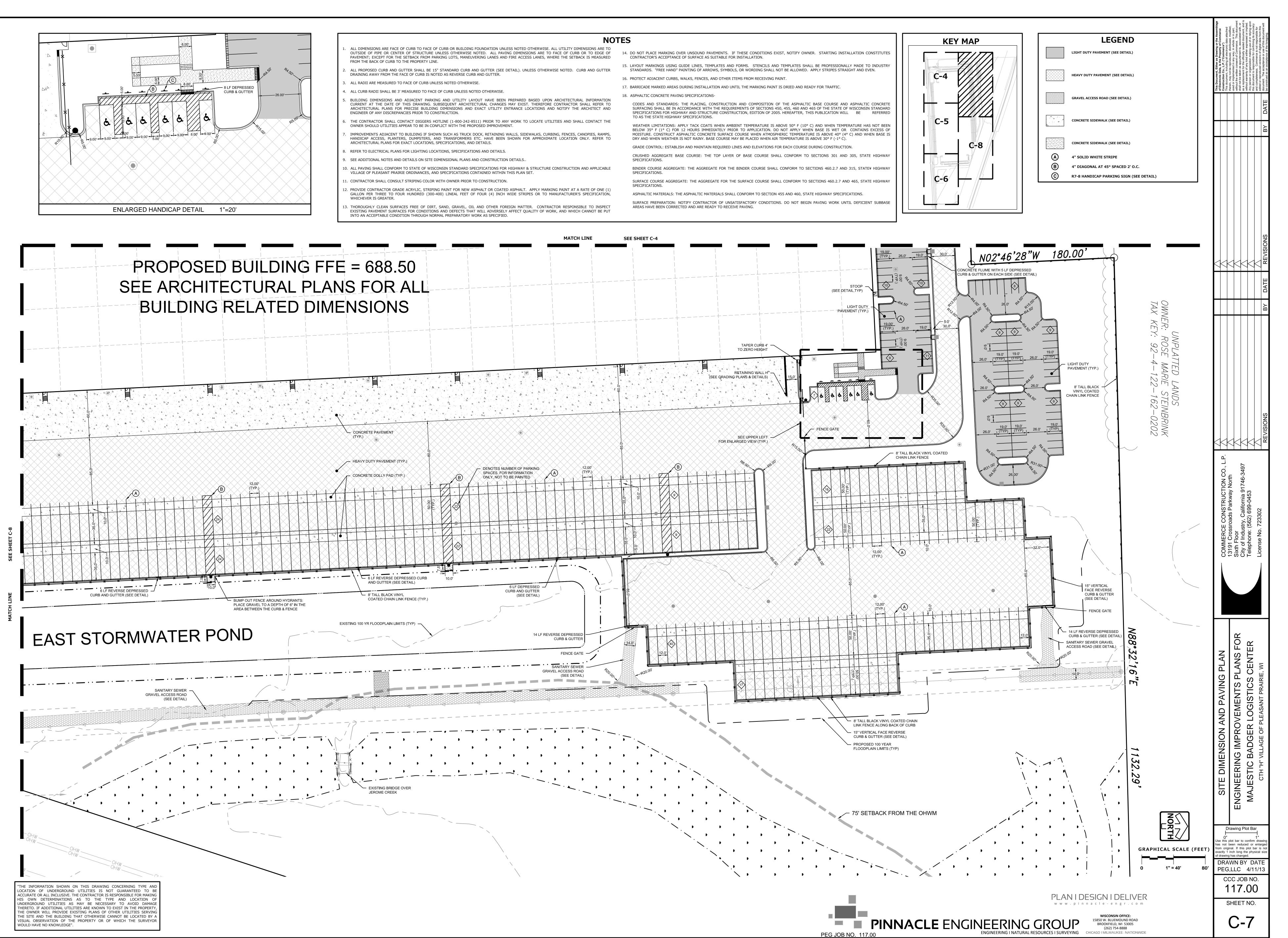
	LEGEND
	LIGHT DUTY PAVEMENT (SEE DETAIL)
	HEAVY DUTY PAVEMENT (SEE DETAIL)
	GRAVEL ACCESS ROAD (SEE DETAIL)
α,α α Δ <sub>α</sub>	CONCRETE SIDEWALK (SEE DETAIL)
	CONCRETE SIDEWALK (SEE DETAIL)
A	4" SOLID WHITE STRIPE
B	4" DIAGONAL AT 45° SPACED 2' O.C.
©	R7-8 HANDICAP PARKING SIGN (SEE DETAIL)
	GRAPHICAL SCALE (F
Ĺ <u></u> ᠴᢅᢩ	0 1" = 40'

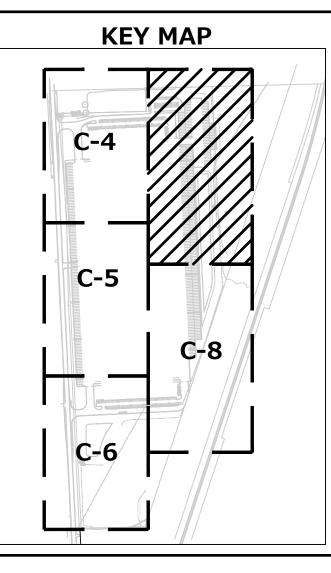
WISCONSIN OFFICE:

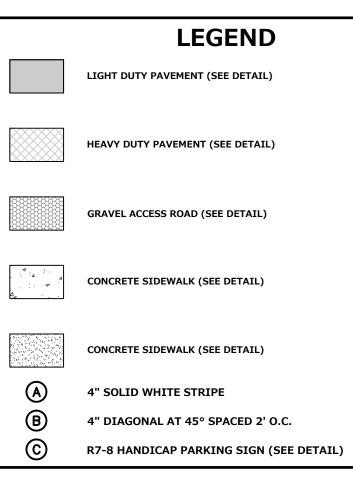
15850 W. BLUEMOUND ROAD BROOKFIELD, WI 53005 (262) 754-8888

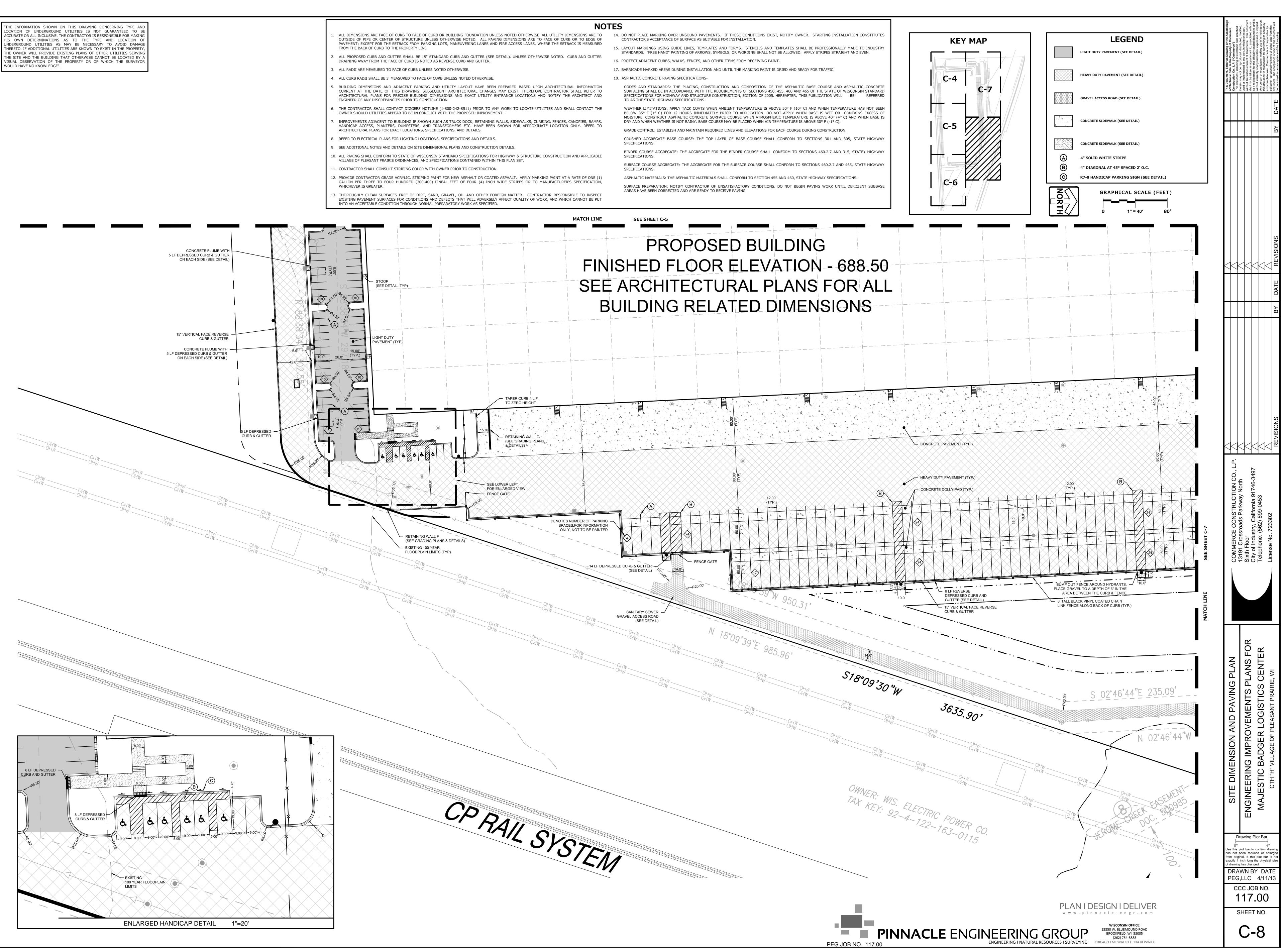
CHICAGO I MILWAUKEE : NATIONWID

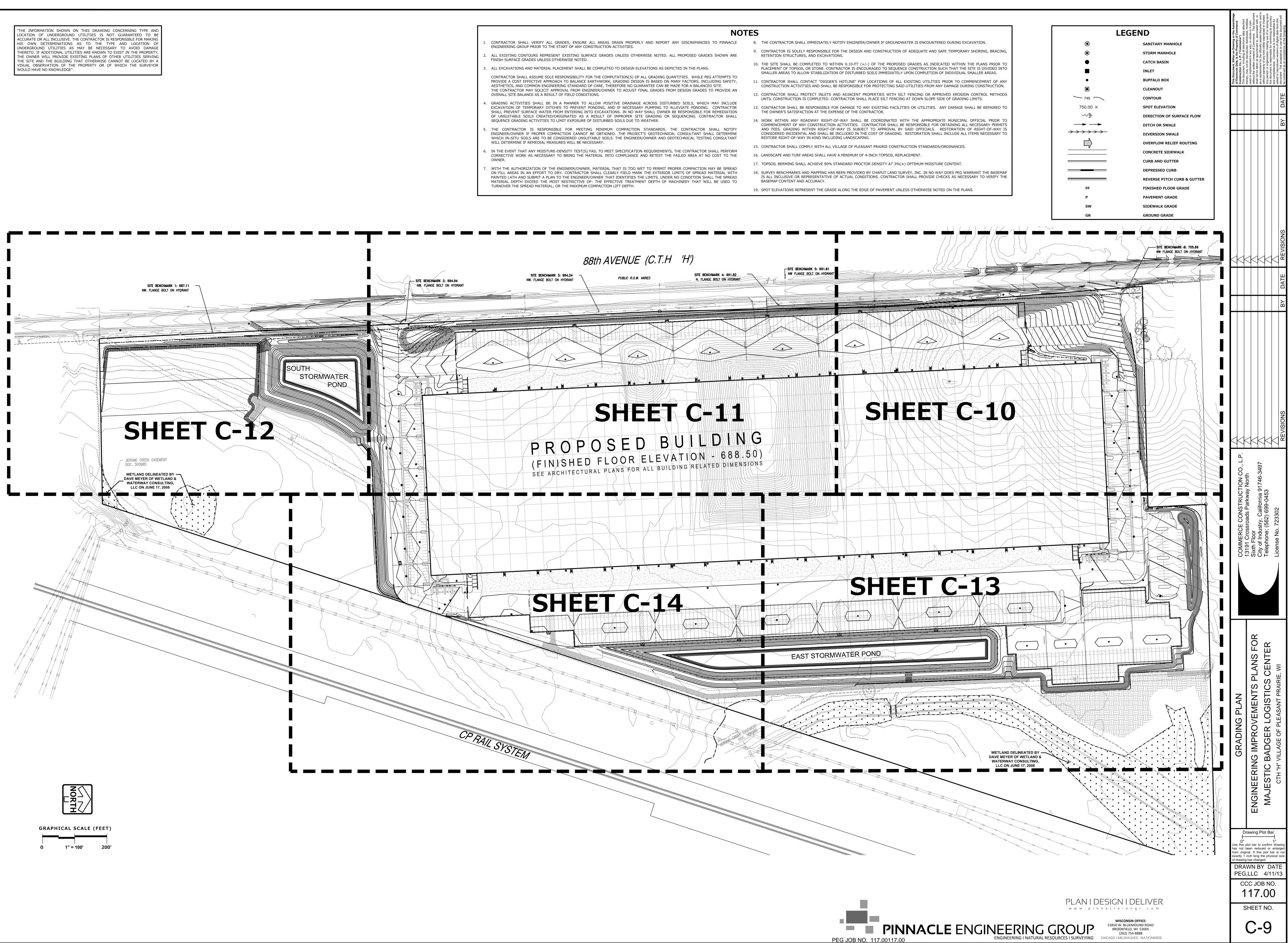


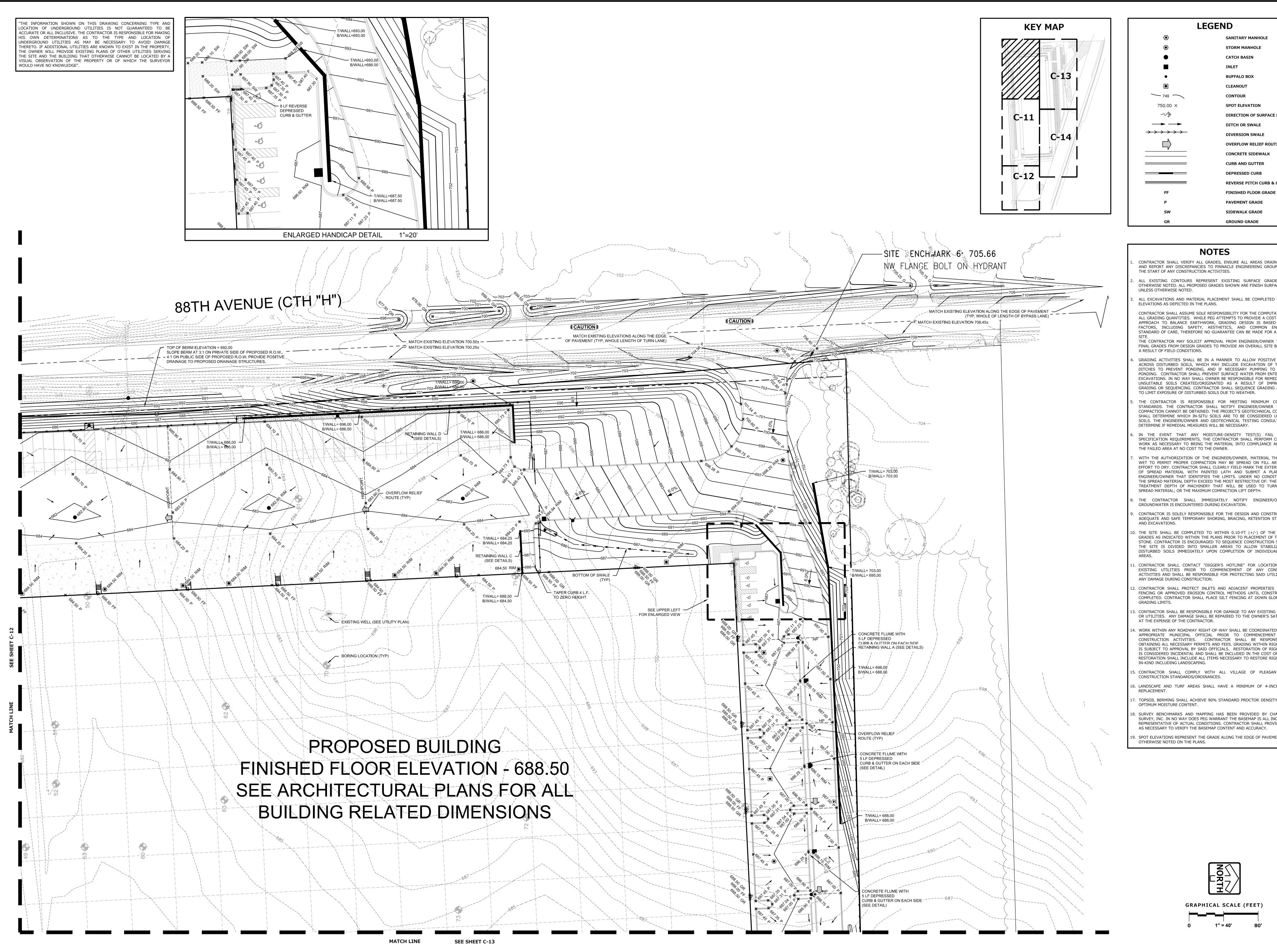












PLANIDESIGNIDELIVER www.pinnacle-engr.com

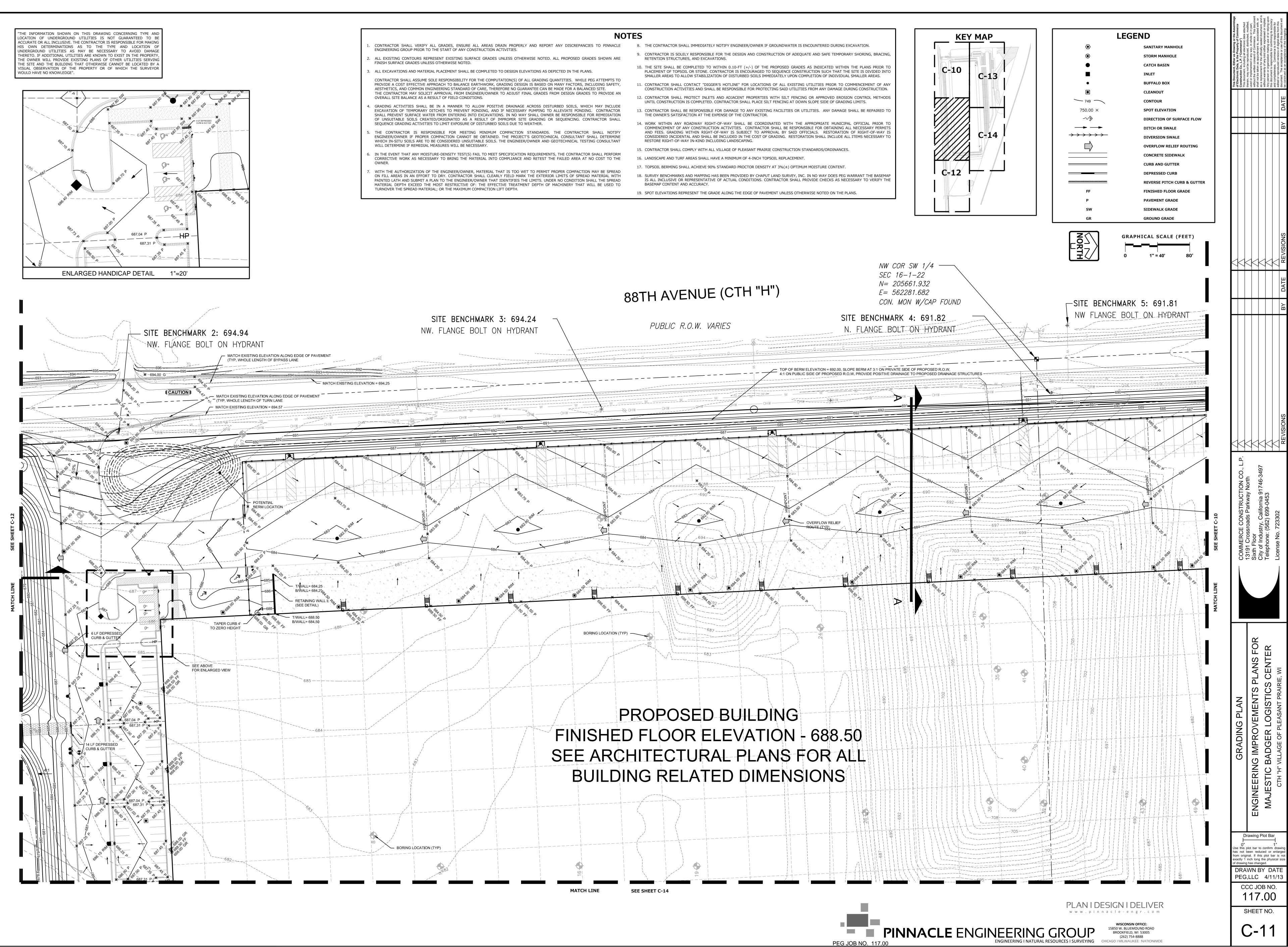
PINNACLE ENGINEERING GROUP

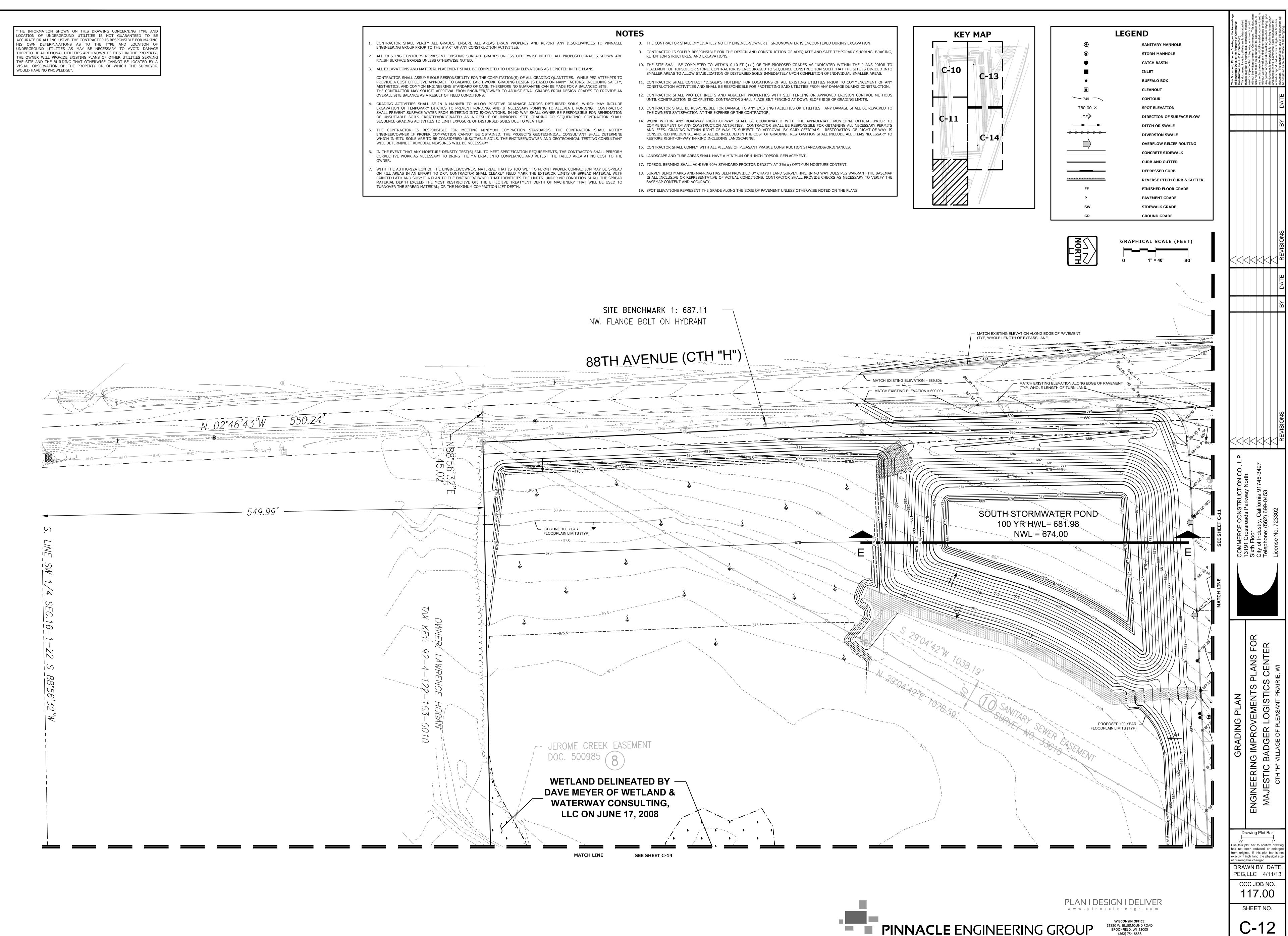
15850 W. BLUEMOUND ROAD BROOKFIELD, WI 53005 (262) 754-8888 CHICAGO I MILWAUKEE : NATIONWIE

WISCONSIN OFFICE:

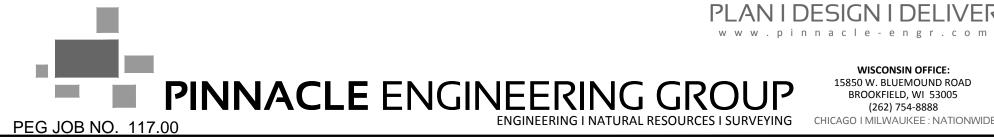
PEG JOB NO. 117.00

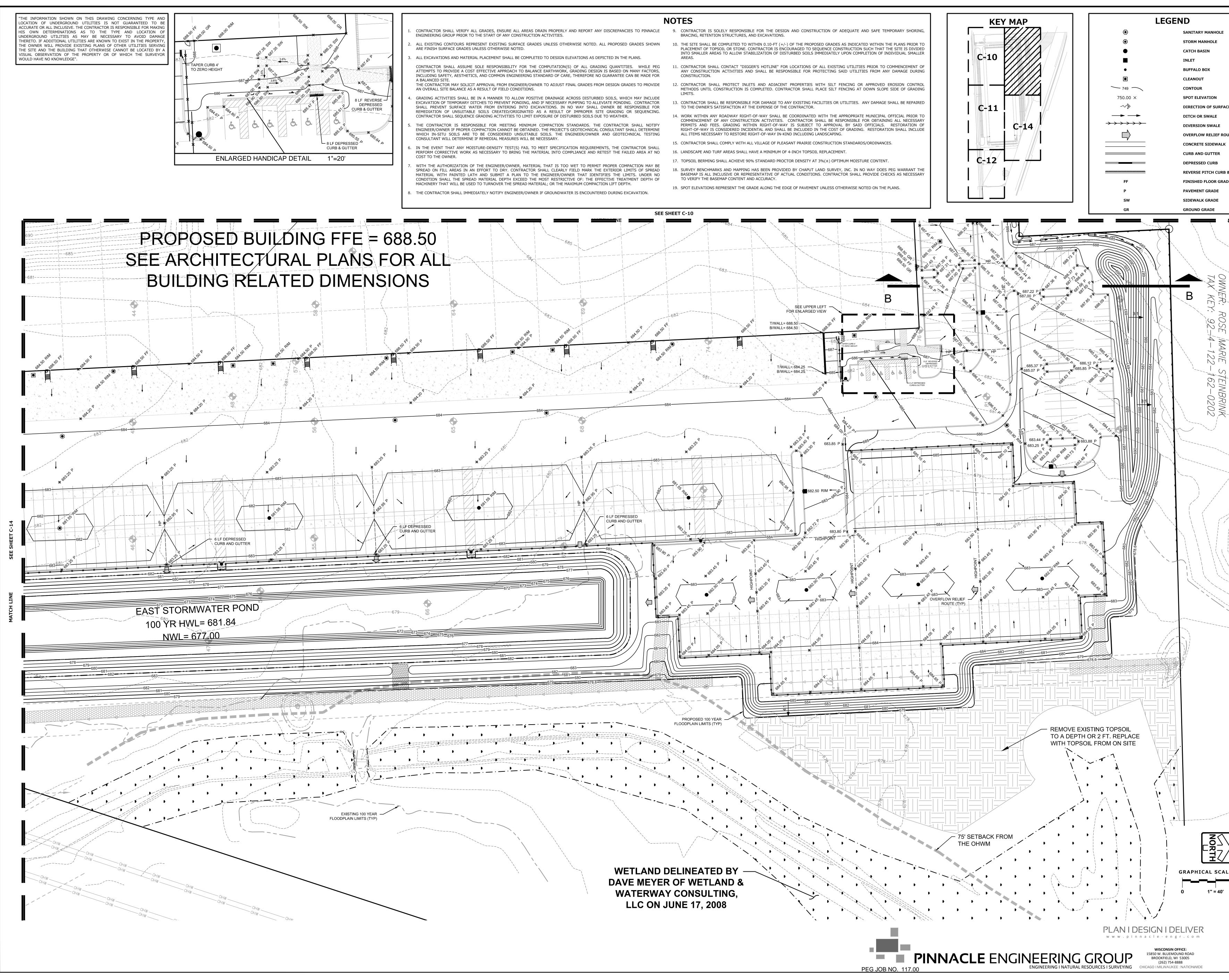
	This Document, Either as Drawing or the Assemblage of Electronic Data. is the Property of Commerce	Construction Co., L.P. ("Commerce") This document including all electronic data attached	there in man on the reproduced, distributed, modified, used or implemented in any way in whole or in part	without the written consent of Commerce. This part, shall not be taken as anountenant to associated with or	as a replacement for the officially scaled document, and is without warranty of any kind either expressed or implied.	Any person or organization making use of or relying upon this document is responsible for confirming its accuracy	and completeness. Commerce is not responsible for edited or reproduced versions of clicital data from this	document. The acceptance or use of this document will be construed as an acceptance of the foregoing.
	Εđ	ŬÊ	th	5 > <del>1</del> 2	× 39	t A	ec	
FLOW								BΥ
TING								
GUTTER								
								REVISIONS
N PROPERLY P PRIOR TO ES UNLESS	$\triangleleft$	$\triangleleft$		<	<	<	<	REVI
TO DESIGN								DATE
ATION(S) OF T EFFECTIVE O ON MANY NGINEERING A BALANCED TO ADJUST								ВΥ
E DRAINAGE TEMPORARY ALLEVIATE ERING INTO DIATION OF ROPER SITE ACTIVITIES								
COMPACTION IF PROPER ONSULTANT JNSUITABLE TANT WILL								
TO MEET CORRECTIVE ND RETEST								REVISIONS
AT IS TOO EAS IN AN IOR LIMITS N TO THE ION SHALL EFFECTIVE IOVER THE	$\langle$	, L.P.	$\leq$	$\leq$	67 7	<	<	RE
WNER IF		TION CO	ly North		91746-34	m		
			σ		σ	Ņ		
PROPOSED OPSOIL OR SUCH THAT ZATION OF		<b>CONSTRUC</b>	roads Parkwa		ry, Californi	562) 699-04	723302	
PROPOSED TOPSOIL OR SUCH THAT ZATION OF L SMALLER NS OF ALL STRUCTION		MMERCE CONSTRUC	191 Crossroads Parkwa	th Floor	y of Industry, Californi	ephone: (562) 699-04	ense No. 723302	
PROPOSED OPSOIL OR SUCH THAT ZATION OF SMALLER IS OF ALL STRUCTION TIES FROM		COMMERCE CONSTRUCTION CO., L.P.	13191 Crossroads Parkway North	Sixth Floor	City of Industry, Californi	Telephone: (562) 699-0453	License No. 723302	
PROPOSED OPSOIL OR SUCH THAT ZATION OF L SMALLER NS OF ALL STRUCTION ITIES FROM WITH SILT RUCTION IS PE SIDE OF FACILITIES FISFACTION		COMMERCE CONSTRUC	13191 Crossroads Parkwa	Sixth Floor	City of Industry, Californi	Telephone: (562) 699-04	License No. 723302	
PROPOSED OPSOIL OR SUCH THAT ZATION OF L SMALLER NS OF ALL STRUCTION TIES FROM WITH SILT SUCTION IS PE SIDE OF FACILITIES FISFACTION WITH THE OF ANY SIBLE FOR HT-OF-WAY F GRADING.		COMMERCE CONSTRUC	13191 Crossroads Parkwa	Sixth Floor	City of Industry, Californi	Telephone: (562) 699-04	License No. 723302	
PROPOSED OPSOIL OR SUCH THAT ZATION OF L SMALLER NS OF ALL STRUCTION ITIES FROM WITH SILT CUCTION IS PE SIDE OF FACILITIES FISFACTION WITH THE OF ANY GIBLE FOR HT-OF-WAY = GRADING. HT-OF-WAY T PRAIRIE		COMMERCE CONSTRUC						
PROPOSED OPSOIL OR SUCH THAT ZATION OF SMALLER IS OF ALL STRUCTION TIES FROM WITH SILT UCTION IS PE SIDE OF FACILITIES TISFACTION WITH THE OF ANY SIBLE FOR HT-OF-WAY HT-OF-WAY T PRAIRIE H TOPSOIL T AT 3%(±)		COMMERCE CONSTRUC						
PROPOSED OPSOIL OR SUCH THAT ZATION OF SUCH THAT ZATION OF SMALLER IS OF ALL STRUCTION TIES FROM WITH SILT UCTION IS PE SIDE OF FACILITIES TSFACTION WITH THE OF ANY SIBLE FOR HT-OF-WAY HT-OF-WAY GRADING. HT-OF-WAY T PRAIRIE H TOPSOIL T PRAIRIE								
PROPOSED OPSOIL OR SUCH THAT ZATION OF SUCH THAT ZATION OF SMALLER ASS OF ALL STRUCTION WITH SILT CUCTION IS PE SIDE OF FACILITIES FISFACTION WITH THE OF ANY SIBLE FOR HT-OF-WAY HT-OF-WAY T PRAIRIE H TOPSOIL Y AT 3%(±) APUT LAND CLUSIVE OR DE CHECKS								
PROPOSED OPSOIL OR SUCH THAT ZATION OF SUCH THAT ZATION OF SMALLER ASS OF ALL STRUCTION WITH SILT CUCTION IS PE SIDE OF FACILITIES FISFACTION WITH THE OF ANY SIBLE FOR HT-OF-WAY HT-OF-WAY T PRAIRIE H TOPSOIL Y AT 3%(±) APUT LAND CLUSIVE OR DE CHECKS								
PROPOSED OPSOIL OR SUCH THAT ZATION OF L SMALLER AS OF ALL STRUCTION ITIES FROM WITH SILT CUCTION IS PE SIDE OF FACILITIES FACTION O WITH THE OF ANY SIBLE FOR HT-OF-WAY GIBLE FOR HT-OF-WAY T PRAIRIE H TOPSOIL Y AT 3%(±) APUT LAND CLUSIVE OR DE CHECKS								
PROPOSED TOPSOIL OR SUCH THAT ZATION OF L SMALLER NS OF ALL STRUCTION ITIES FROM WITH SILT RUCTION IS PE SIDE OF FACILITIES TISFACTION O WITH THE OF ANY SIBLE FOR HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY						MAJESTIC BADGER LOGISTICS CENTER   Telephone: (562) 699-04		
PROPOSED TOPSOIL OR SUCH THAT ZATION OF L SMALLER NS OF ALL ISTRUCTION ITIES FROM WITH SILT RUCTION IS PE SIDE OF FACILITIES TISFACTION O WITH THE OF ANY SIBLE FOR HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY IT PRAIRIE H TOPSOIL Y AT 3%(±) APUT LAND CLUSIVE OR IDE CHECKS	Use has from		Plot					
AL SMALLER NS OF ALL ISTRUCTION ITIES FROM WITH SILT RUCTION IS OPE SIDE OF FACILITIES TISFACTION OF ANY SIBLE FOR HT-OF-WAY F GRADING. HT-OF-WAY F GRADING. HT-OF-WAY IT PRAIRIE CH TOPSOIL Y AT 3%(±)	Use has from exa of d		Plot beel ginal. I in ha W B,L		Plc to ccc ducee his p g ngec BY		ar T 1" dra enla sical AT 1/1	
PROPOSED TOPSOIL OR SUCH THAT ZATION OF L SMALLER NS OF ALL ISTRUCTION ITIES FROM WITH SILT RUCTION IS PE SIDE OF FACILITIES TISFACTION O WITH THE OF ANY SIBLE FOR HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY HT-OF-WAY IT PRAIRIE H TOPSOIL Y AT 3%(±) APUT LAND CLUSIVE OR IDE CHECKS	Use has from exa of d		Plote ginal. I incl ig ha S,L CC		Plc to cc ducee his p g the angec BY	O Z TO TABLE TIC BADGER LOGISTICS CENTER		



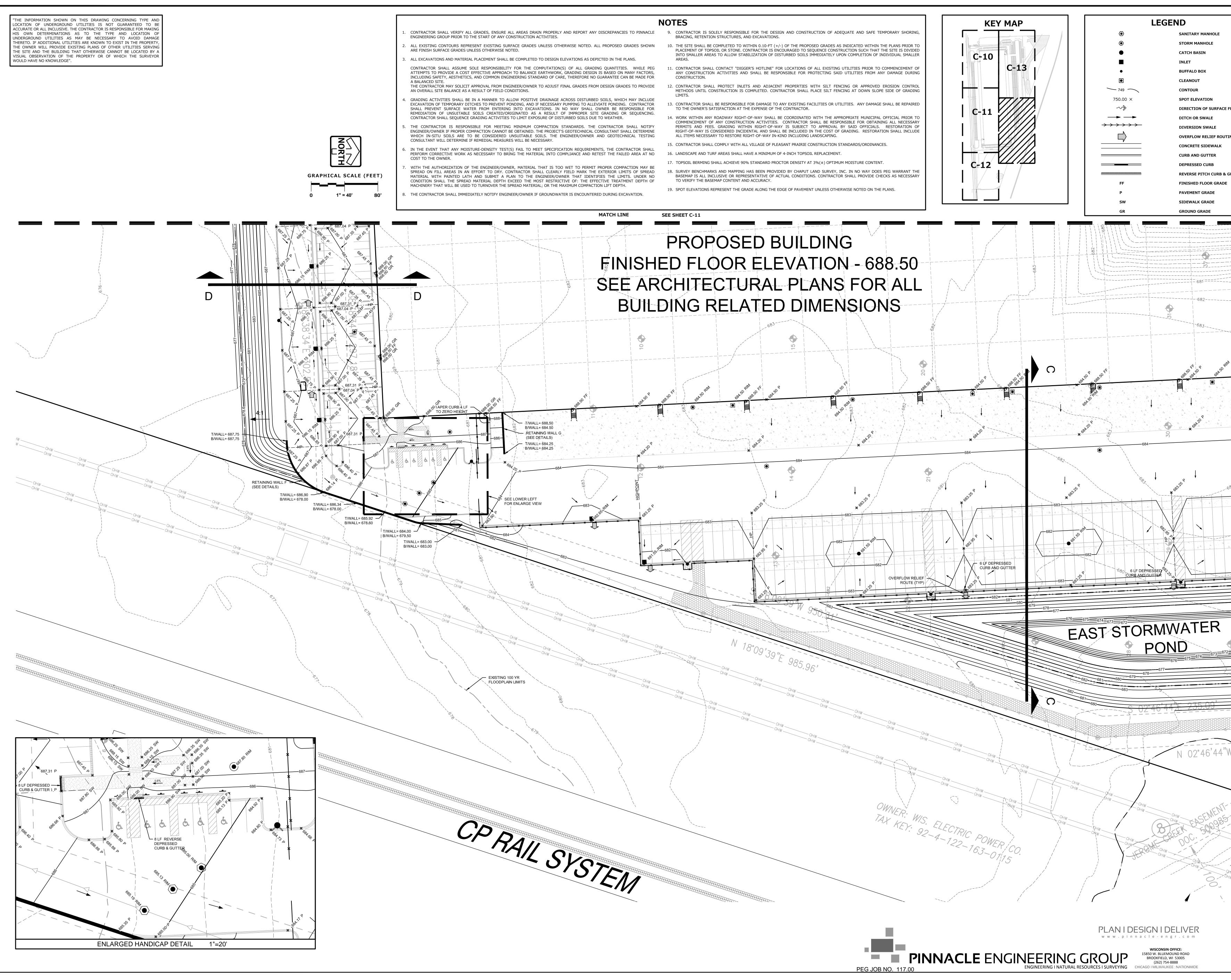


NOT	ΓES
NTRACTOR SHALL VERIFY ALL GRADES, ENSURE ALL AREAS DRAIN PROPERLY AND REPORT ANY DISCREPANCIES TO PINNACLE GINEERING GROUP PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES.	8. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY ENGINEER/OWNER IF GRO
L EXISTING CONTOURS REPRESENT EXISTING SURFACE GRADES UNLESS OTHERWISE NOTED. ALL PROPOSED GRADES SHOWN ARE IISH SURFACE GRADES UNLESS OTHERWISE NOTED.	9. CONTRACTOR IS SOLELY RESPONSIBLE FOR THE DESIGN AND CONSTRUCT RETENTION STRUCTURES, AND EXCAVATIONS.
L EXCAVATIONS AND MATERIAL PLACEMENT SHALL BE COMPLETED TO DESIGN ELEVATIONS AS DEPICTED IN THE PLANS.	10. THE SITE SHALL BE COMPLETED TO WITHIN 0.10-FT (+/-) OF THE PRO PLACEMENT OF TOPSOIL OR STONE. CONTRACTOR IS ENCOURAGED TO SE SMALLER AREAS TO ALLOW STABILIZATION OF DISTURBED SOILS IMMEDIA
NTRACTOR SHALL ASSUME SOLE RESPONSIBILITY FOR THE COMPUTATION(S) OF ALL GRADING QUANTITIES. WHILE PEG ATTEMPTS TO OVIDE A COST EFFECTIVE APPROACH TO BALANCE EARTHWORK, GRADING DESIGN IS BASED ON MANY FACTORS, INCLUDING SAFETY, STHETICS, AND COMMON ENGINEERING STANDARD OF CARE, THEREFORE NO GUARANTEE CAN BE MADE FOR A BALANCED SITE. E CONTRACTOR MAY SOLICIT APPROVAL FROM ENGINEER/OWNER TO ADJUST FINAL GRADES FROM DESIGN GRADES TO PROVIDE AN	11. CONTRACTOR SHALL CONTACT "DIGGER'S HOTLINE" FOR LOCATIONS OF CONSTRUCTION ACTIVITIES AND SHALL BE RESPONSIBLE FOR PROTECTING
ERALL SITE BALANCE AS A RESULT OF FIELD CONDITIONS.	12. CONTRACTOR SHALL PROTECT INLETS AND ADJACENT PROPERTIES WIT UNTIL CONSTRUCTION IS COMPLETED. CONTRACTOR SHALL PLACE SILT FE
ADING ACTIVITIES SHALL BE IN A MANNER TO ALLOW POSITIVE DRAINAGE ACROSS DISTURBED SOILS, WHICH MAY INCLUDE CAVATION OF TEMPORARY DITCHES TO PREVENT PONDING, AND IF NECESSARY PUMPING TO ALLEVIATE PONDING. CONTRACTOR ALL PREVENT SURFACE WATER FROM ENTERING INTO EXCAVATIONS. IN NO WAY SHALL OWNER BE RESPONSIBLE FOR REMEDIATION UNSUITABLE SOILS CREATED/ORIGINATED AS A RESULT OF IMPROPER SITE GRADING OR SEQUENCING. CONTRACTOR SHALL	13. CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGE TO ANY EXISTING FA THE OWNER'S SATISFACTION AT THE EXPENSE OF THE CONTRACTOR.
QUENCE GRADING ACTIVITIES TO LIMIT EXPOSURE OF DISTURBED SOILS DUE TO WEATHER.	14. WORK WITHIN ANY ROADWAY RIGHT-OF-WAY SHALL BE COORDINATI COMMENCEMENT OF ANY CONSTRUCTION ACTIVITIES. CONTRACTOR SH
E CONTRACTOR IS RESPONSIBLE FOR MEETING MINIMUM COMPACTION STANDARDS. THE CONTRACTOR SHALL NOTIFY GINEER/OWNER IF PROPER COMPACTION CANNOT BE OBTAINED. THE PROJECT'S GEOTECHNICAL CONSULTANT SHALL DETERMINE HICH IN-SITU SOILS ARE TO BE CONSIDERED UNSUITABLE SOILS. THE ENGINEER/OWNER AND GEOTECHNICAL TESTING CONSULTANT LL DETERMINE IF REMEDIAL MEASURES WILL BE NECESSARY.	AND FEES. GRADING WITHIN RIGHT-OF-WAY IS SUBJECT TO APPROVA CONSIDERED INCIDENTAL AND SHALL BE INCLUDED IN THE COST OF GRAI RESTORE RIGHT-OF-WAY IN-KIND INCLUDING LANDSCAPING.
THE EVENT THAT ANY MOTOTURE DENOTO TECT/C) FAIL TO MEET OPERATION REQUIREMENTS. THE CONTRACTOR CHALL REREORM	15. CONTRACTOR SHALL COMPLY WITH ALL VILLAGE OF PLEASANT PRAIRIE CO
THE EVENT THAT ANY MOISTURE-DENSITY TEST(S) FAIL TO MEET SPECIFICATION REQUIREMENTS, THE CONTRACTOR SHALL PERFORM RRECTIVE WORK AS NECESSARY TO BRING THE MATERIAL INTO COMPLIANCE AND RETEST THE FAILED AREA AT NO COST TO THE /NER.	16. LANDSCAPE AND TURF AREAS SHALL HAVE A MINIMUM OF 4-INCH TOPSOII
	17. TOPSOIL BERMING SHALL ACHIEVE 90% STANDARD PROCTOR DENSITY AT
TH THE AUTHORIZATION OF THE ENGINEER/OWNER, MATERIAL THAT IS TOO WET TO PERMIT PROPER COMPACTION MAY BE SPREAD I FILL AREAS IN AN EFFORT TO DRY. CONTRACTOR SHALL CLEARLY FIELD MARK THE EXTERIOR LIMITS OF SPREAD MATERIAL WITH INTED LATH AND SUBMIT A PLAN TO THE ENGINEER/OWNER THAT IDENTIFIES THE LIMITS. UNDER NO CONDITION SHALL THE SPREAD TERIAL DEPTH EXCEED THE MOST RESTRICTIVE OF: THE EFFECTIVE TREATMENT DEPTH OF MACHINERY THAT WILL BE USED TO RNOVER THE SPREAD MATERIAL; OR THE MAXIMUM COMPACTION LIFT DEPTH.	<ol> <li>SURVEY BENCHMARKS AND MAPPING HAS BEEN PROVIDED BY CHAPUT LAN IS ALL INCLUSIVE OR REPRESENTATIVE OF ACTUAL CONDITIONS. CONTR. BASEMAP CONTENT AND ACCURACY.</li> </ol>
	19. SPOT ELEVATIONS REPRESENT THE GRADE ALONG THE EDGE OF PAVEMEN

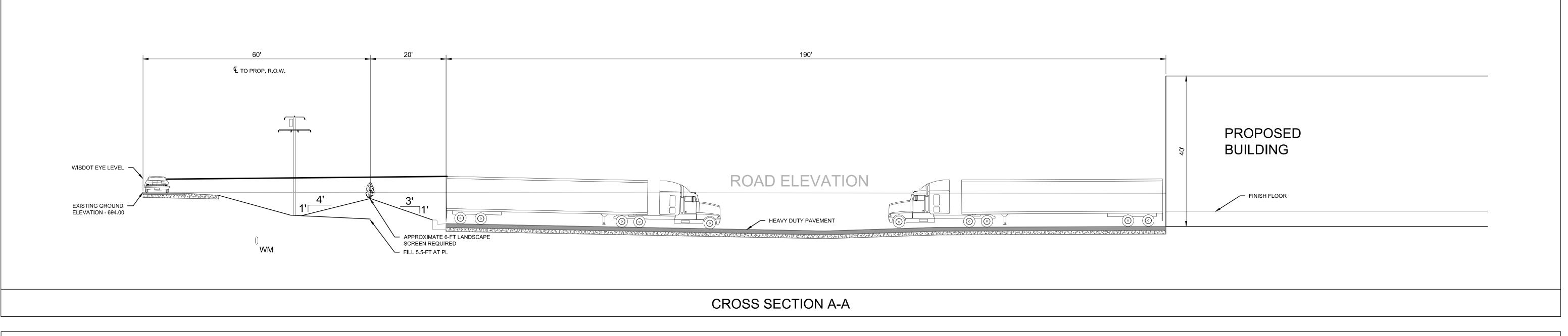


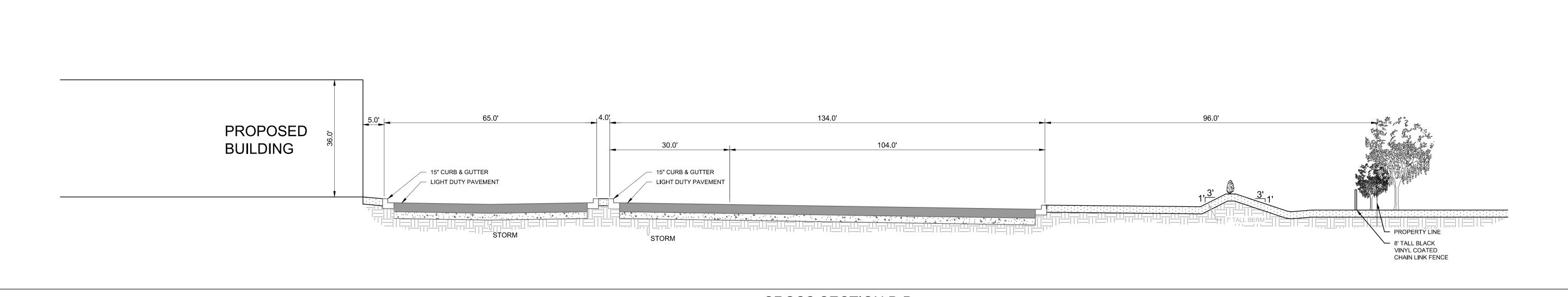


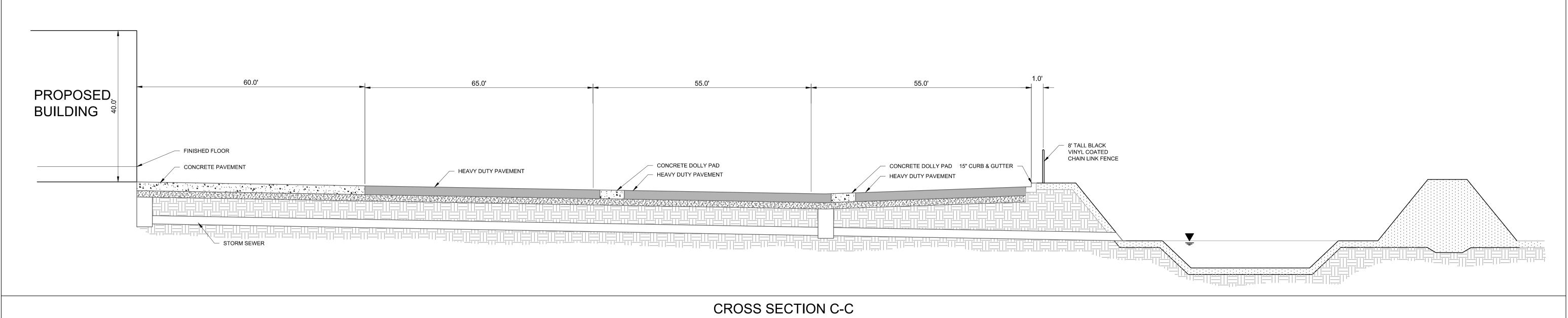
	This Document, Either as Drawing or the Assemblage of Electronic Data, is the Property of Commerce Construction Co., L.P. ("Commerce") This document, including all electronic data attached	<ul> <li>therein, may not be reproduced, distributed, modified,</li> <li>used or implemented in any way, in whole or in part, without the written consent of Commerce. This document as a neutoper as appruntement to, associated with, or as a represement for the officially sealed document, and is</li> </ul>	without warranty of any kind either expressed or implied. Any person or organization making use of or relying upon this document is responsible for confirming its accuracy	and completeness. Commerce is not responsible for edited or reproduced versions of digital data from this document. The acceptance or use of this document will be construed as an acceptance of the foregoing.
CE FLOW				DATE
DUTING				ΒY
& GUTTER DE				
				▲ REVISIONS
				DATE
UNPLATTED LANDS				ΒY
				∧ REVISIONS
	COMMERCE CONSTRUCTION CO., L.P.	13191 Crossroads Parkway North Sixth Floor City of Industry California 91746-3497	Telephone: (562) 699-0453	License No. 723302
	GRADING PLAN	ENGINEERING IMPROVEMENTS PLANS FOR	MAJESTIC BADGER LOGISTICS CENTER	CTH "H" VILLAGE OF PLEASANT PRAIRIE, WI
LE (FEET)	Use this has not from orig exactly 1 of drawing DRA PEG	rawing F plot bar to been reduction inch long to g has change WN B ,LLC CC JO 17	confirm ced or of s plot bas ged. Y D/ 4/1 <sup>-1</sup> B NC	H 1" drawing enlarged ar is not ical size ATE 1/13 ).
		C-	<sup>-</sup> NO.	



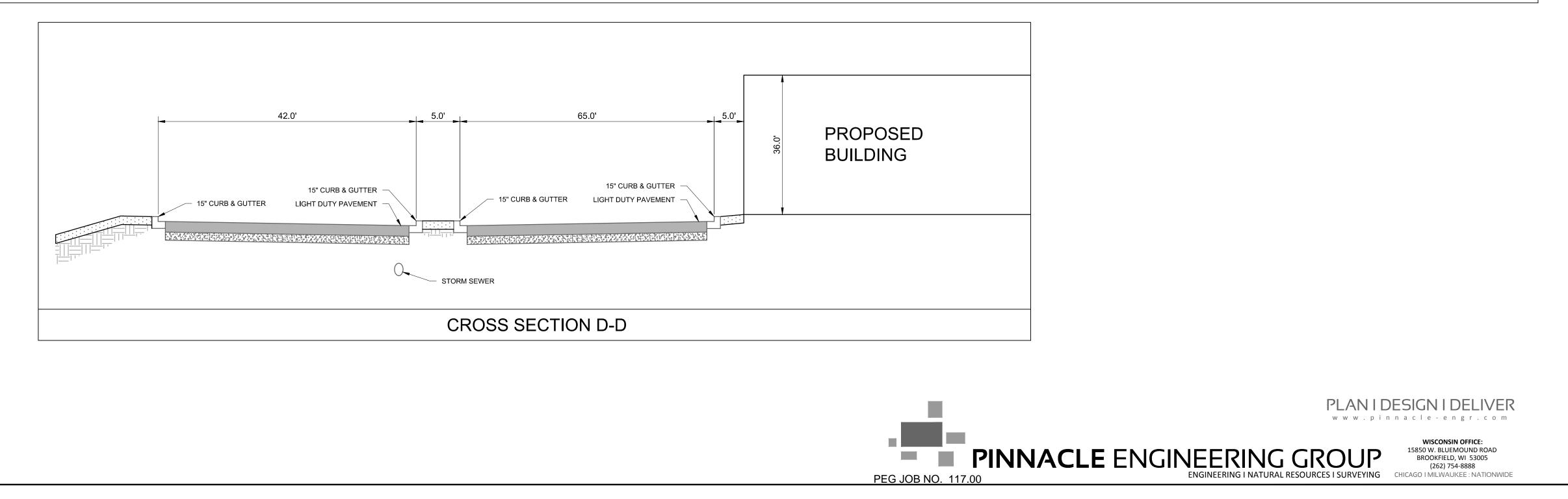
OLTW OLTW		MATCH LINE SEE SHEET C-13	b Fun A A A A A A A		B & GUTTER ADE	ACE FLOW OUTING	
0" Use this p has not b from origi exactly 1 of drawing DRA PEG	GRADING PLAN	COMMERCE CONSTRUCTION CO., L.P.					This Document, Either as Drawing or the Assemblage of Electronic Data, is the Property of Commerce Construction Co., L. P. ("Commerce") This document, including all electronic data attached
rawing Plot plot bar to cor been reduced nal. If this plot inch long the has changed. WN BY ,LLC 4 CC JOB 17.0 GHEET N C-1	ENGINEERING IMPROVEMENTS PLANS FOR	Sixth Floor City of Industry, California 91746-3497 Tolonbono: (562) 600 0452					therein, may not be reproduced, distributed, modified, used or implemented in any way, in whole or in part, without the written consent of Commerce. This document shall not be taken as appurtenant to, associated with, or as a replacement for the officially sealed document, and is writhout warranty of any kind either expressed or implied.
1" firm drawing or enlarged to bar is not physical size DATE /11/13 NO. )0	CTH "H" VILLAGE OF PLEASANT PRAIRIE, WI	License No. 723302	REVISIONS	BY DATE	REVISIONS	BY DATE	Any person or organization making use of or relying upon this document is responsible for confirming its accuracy and completeness. Commerce is not responsible for edited or reproduced versions of digital data from this document. The acceptance or use of this document will be construed as an acceptance of the foregoing.



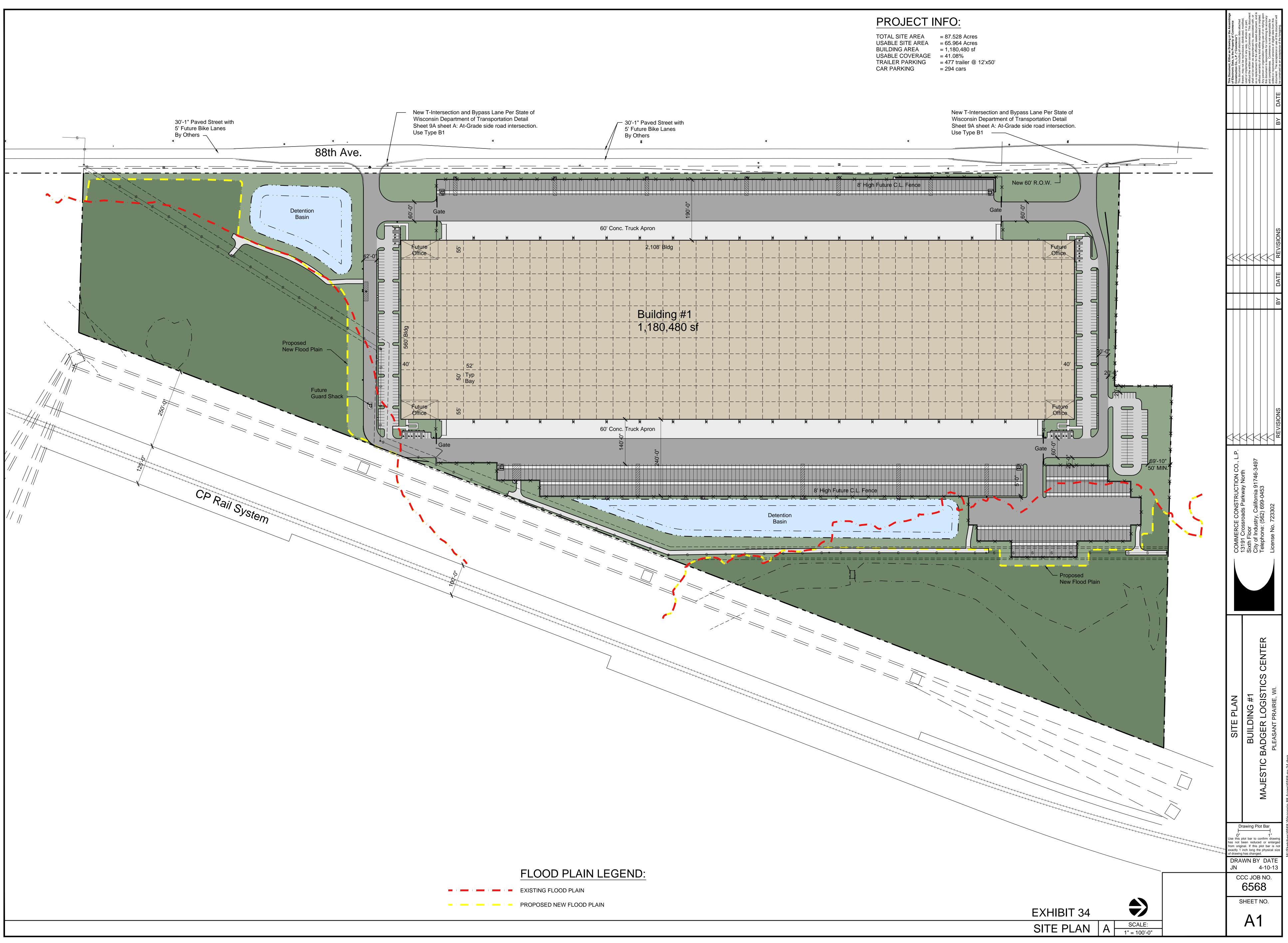


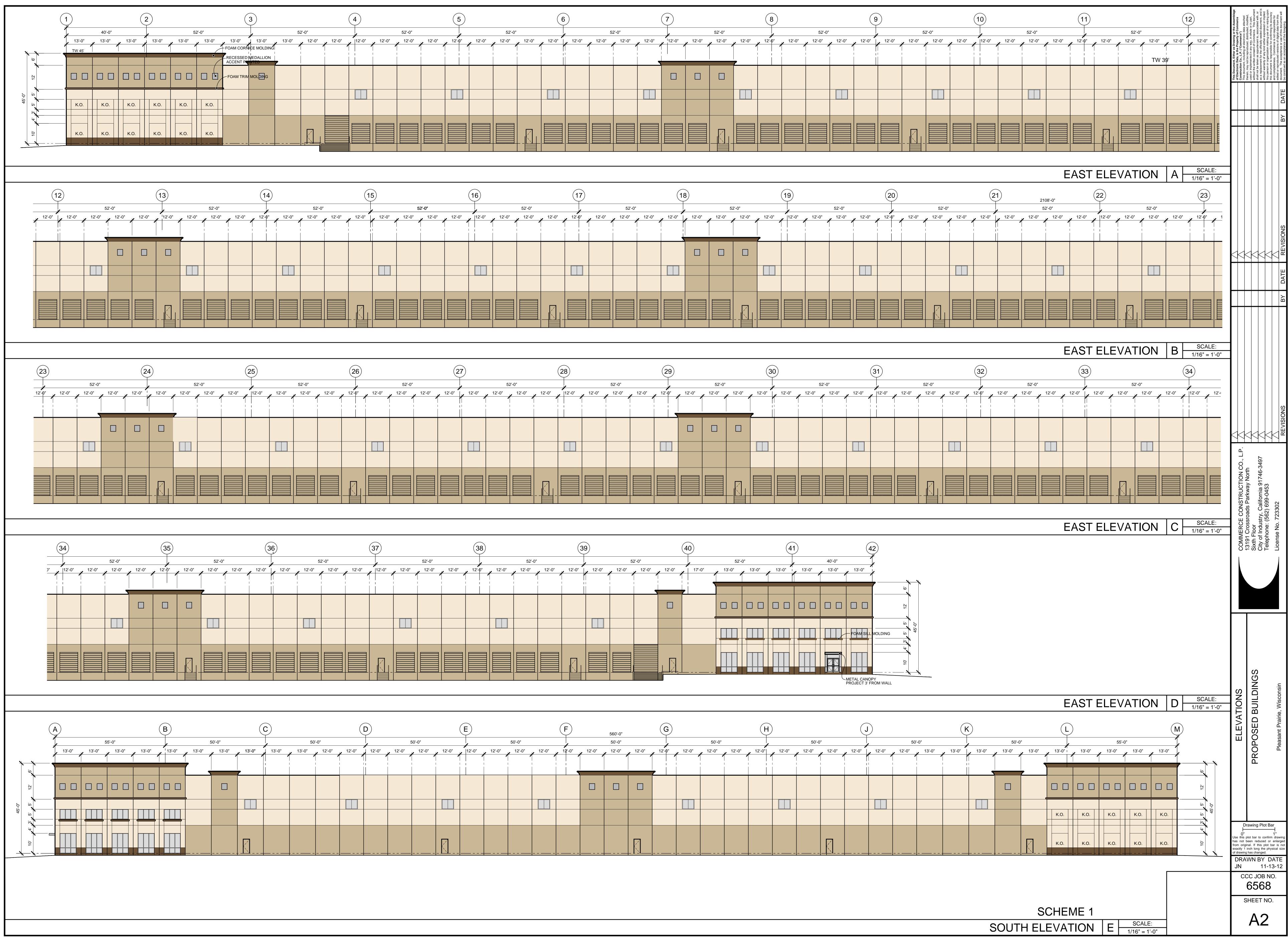


## **CROSS SECTION B-B**

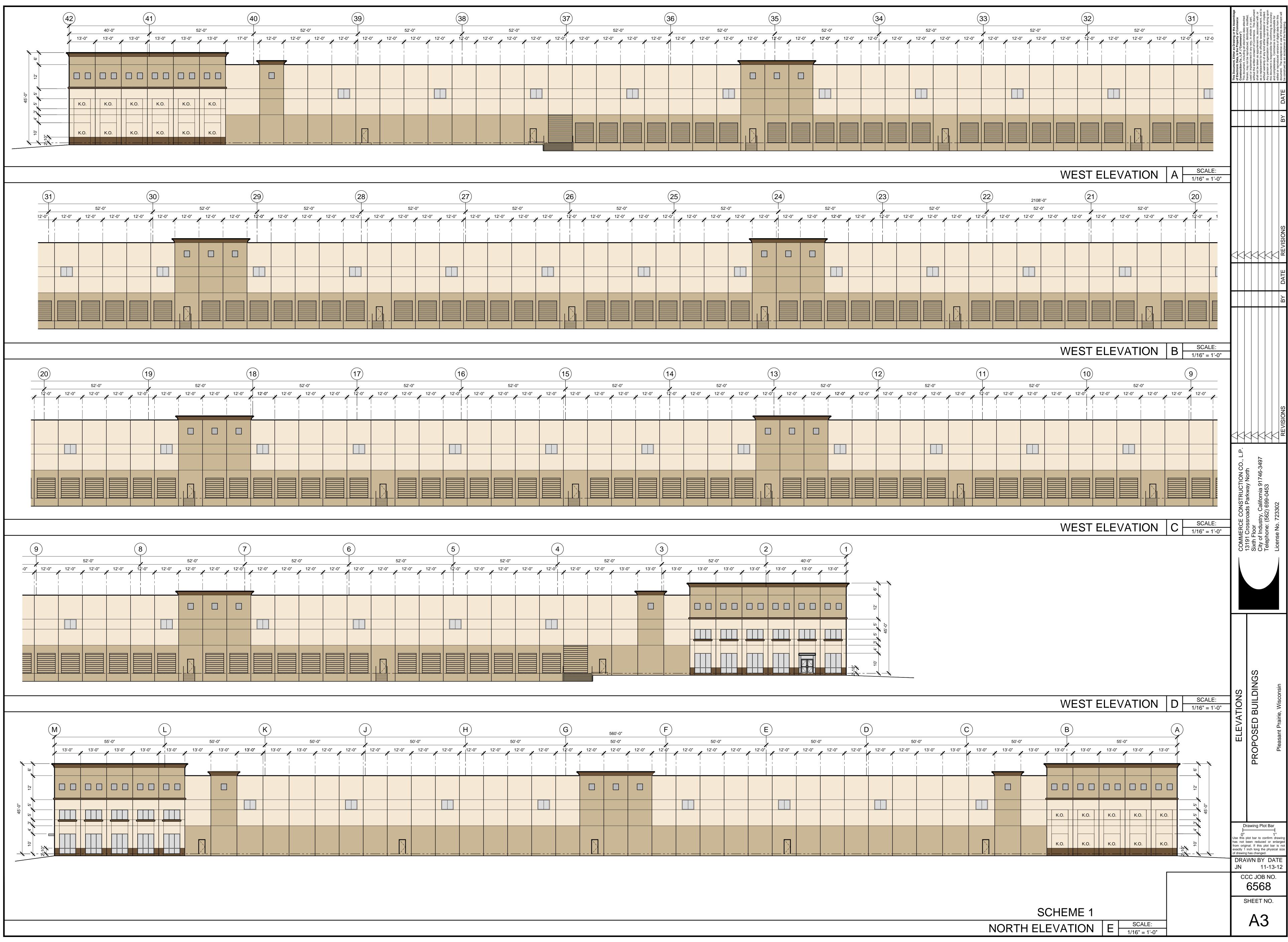


ha fro ex of							This Document, Either as Drawing or the Assemblage
as om kac dr							of Electronic Data, is the Property of Commerce
th nc ctly raw					<		Construction Co., L.P. ("Commerce")
							<ul> <li>This document, including all electronic data attached</li> </ul>
		13191 Crossroads Parkway North	~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~		<		therein, may not be reproduced, distributed, modified,
ot al. nch na: V							used or implemented in any way, in whole or in part.
n i If n k s c		Sixth Floor			~		without the written consent of Commerce. This document
	ワこ						shall not be taken as appurtenant to, associated with, or
to duc his g t ing <b>3</b>		City of Industry. California 91746-3497	~		<		as a replacement for the officially sealed document, and is
							without warranty of any kind either expressed or implied.
nfi lot pl 1		l elephone: (562) 699-0453	~~~		<		Any person or organization making use of or relying upon
							this document is responsible for confirming its accuracy
			~		<		and completeness. Commerce is not responsible for
rav ila is al T		LICENSE NO. 723302					edited or reproduced versions of digital data from this
	CITETI VILLAGE OF FLEADAINT FINAIINIE, WI		REVISIONS	BY DATE	REVISIONS	BY DATE	document. The acceptance or use of this document will be construed as an acceptance of the foregoing.





H				J				K				Ĺ	)			
12'-0"	, 12'-0"	50'-0 12'-0"	12'-0"	12'-0"	12'-0"	50 12'-0"	'-0" 13'-0"	13'-0"	13'-0"	50'-0 13'-0"	" 13'-0"	13'-0" ı	13'-0"	55'-( 13'-0"		13
									•							
												К.О.	К.О.	К.О.	К.О.	K
												К.О.	К.О.	К.О.	К.О.	ĸ

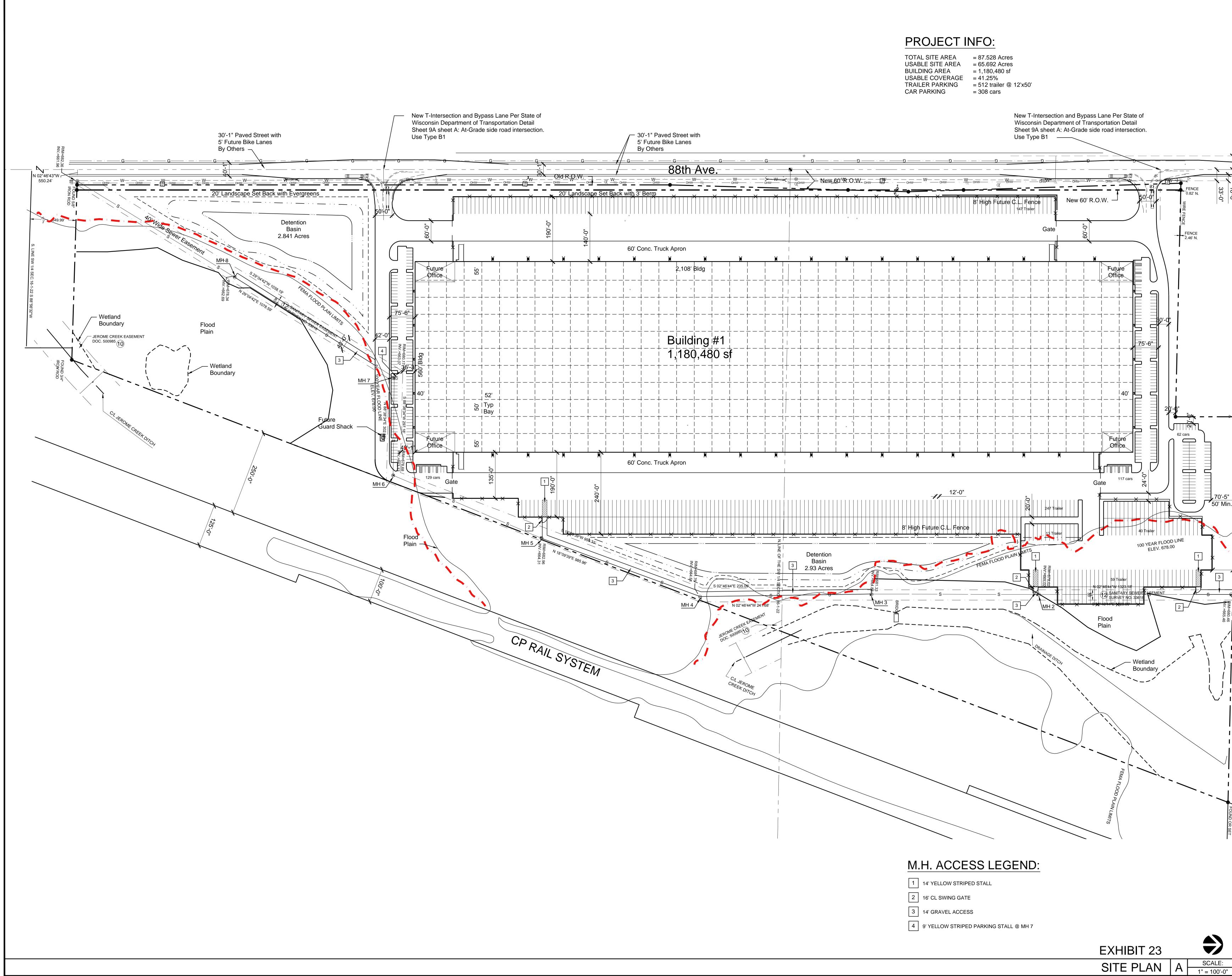


39			38				(37)				36)				35				34)			(	33)			32			
12'-0"	52'-0" 12'-0"	12'-0"	12'-0"	12'-0"	52'-0" 12'-0"	12'-0"	12'-0"	, 12'-0"	52'- 12'-0"	12'-0"	12'-0"	12'-0"	52'-0" 12'-0"	12'-0"	12'-0"	12'-0"	52'-0" 12'-0"	12'-0"	12'-0"	12'-0"	52'-( 12'-0"	12'-0"	12'-0"	52'-0" 12'-0"	12'-0"	12'-0"	12'-0"	52'-0" 12'-0"	12'-0

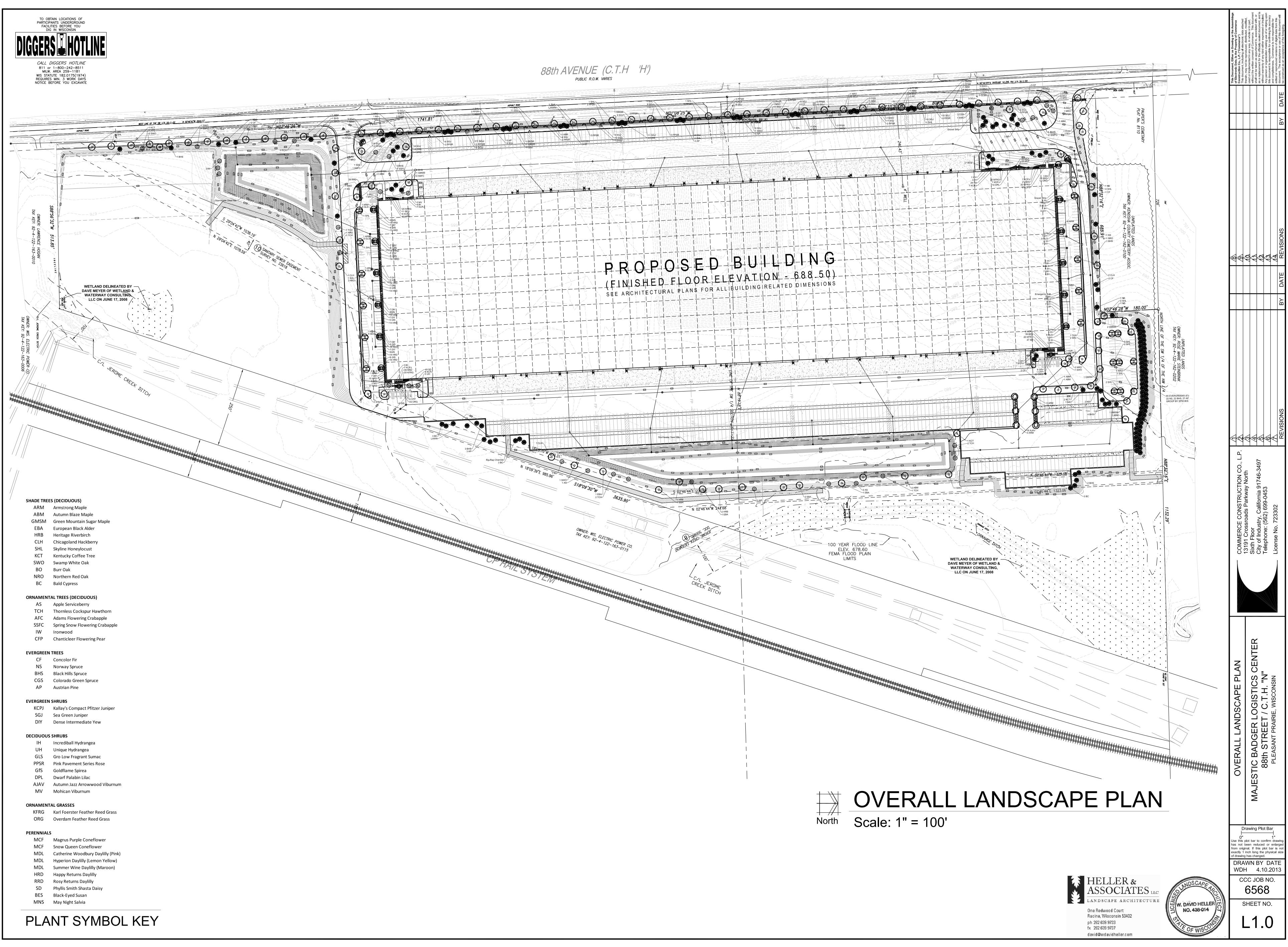
SCHEME 1	
NORTH ELEVATION	SCALE:
NORTHELEVATION	1/16" = 1'-0"

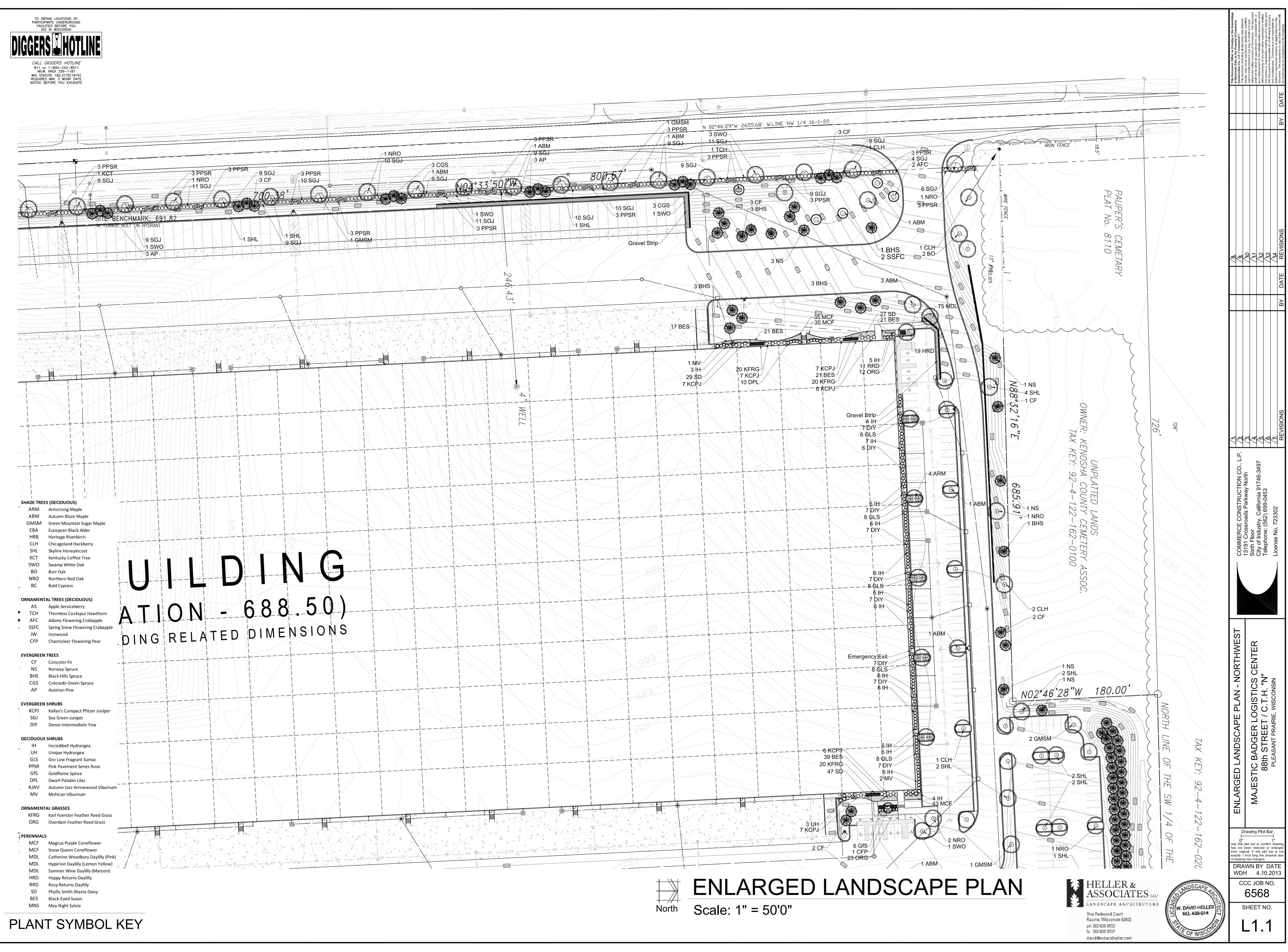


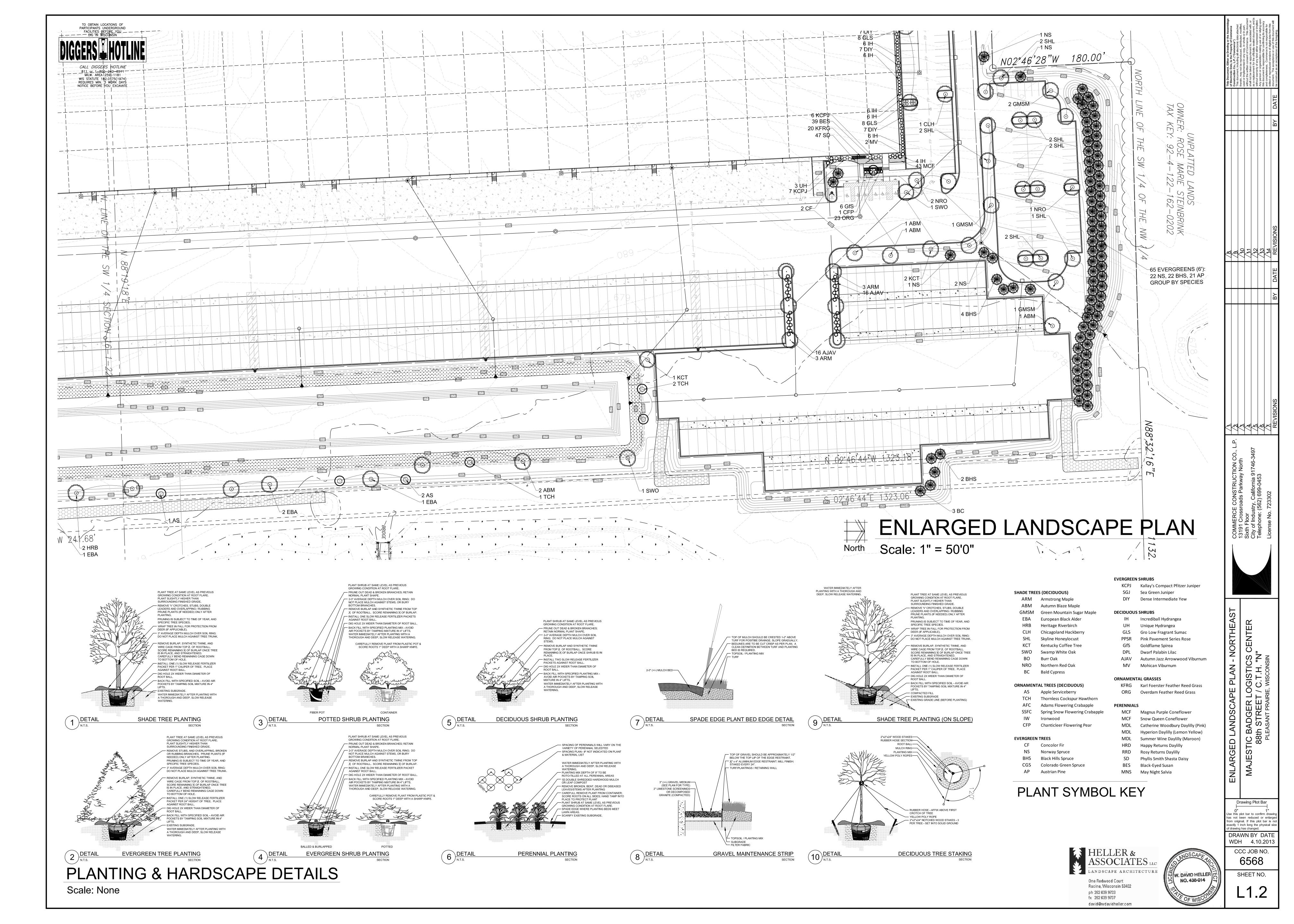


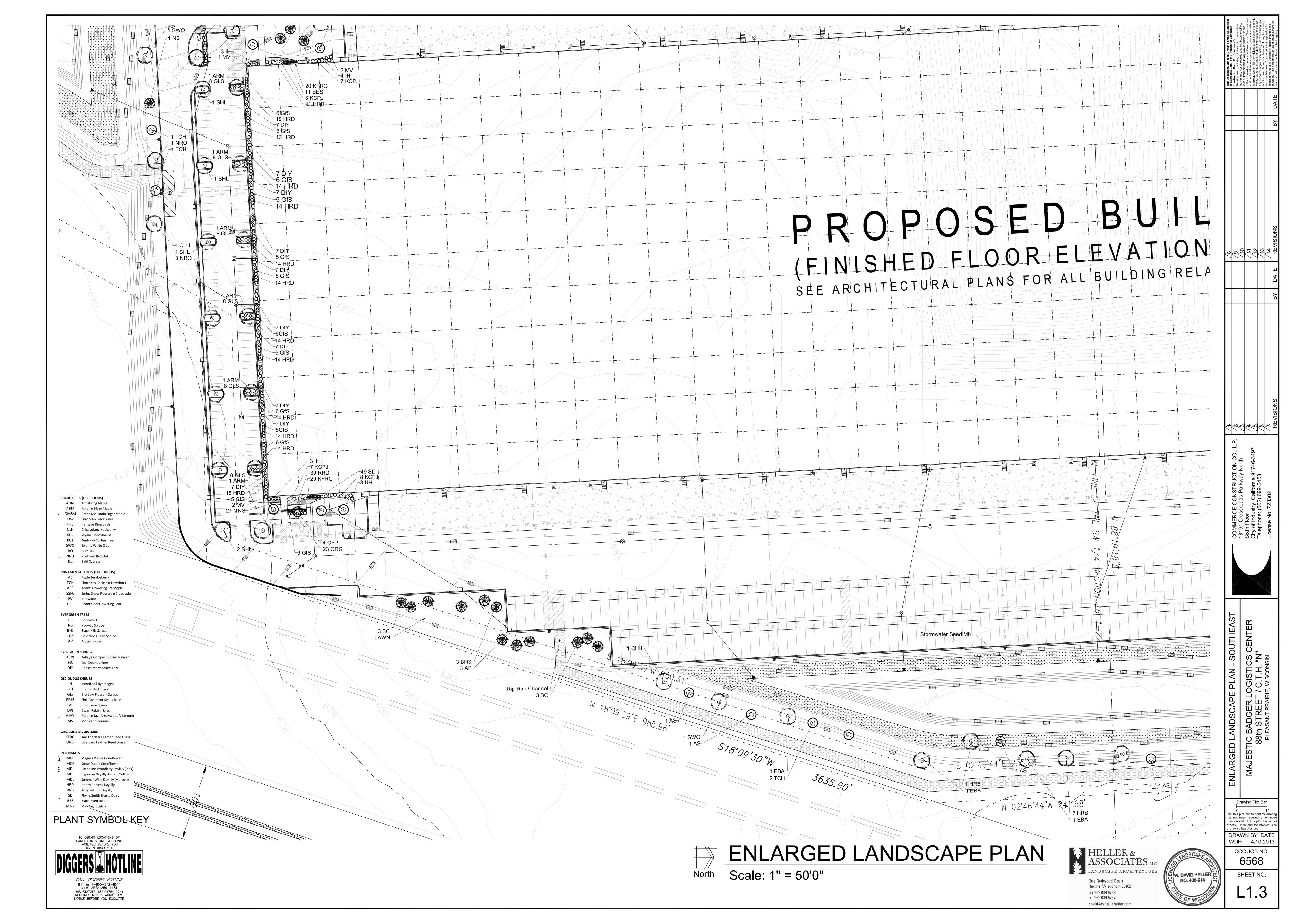


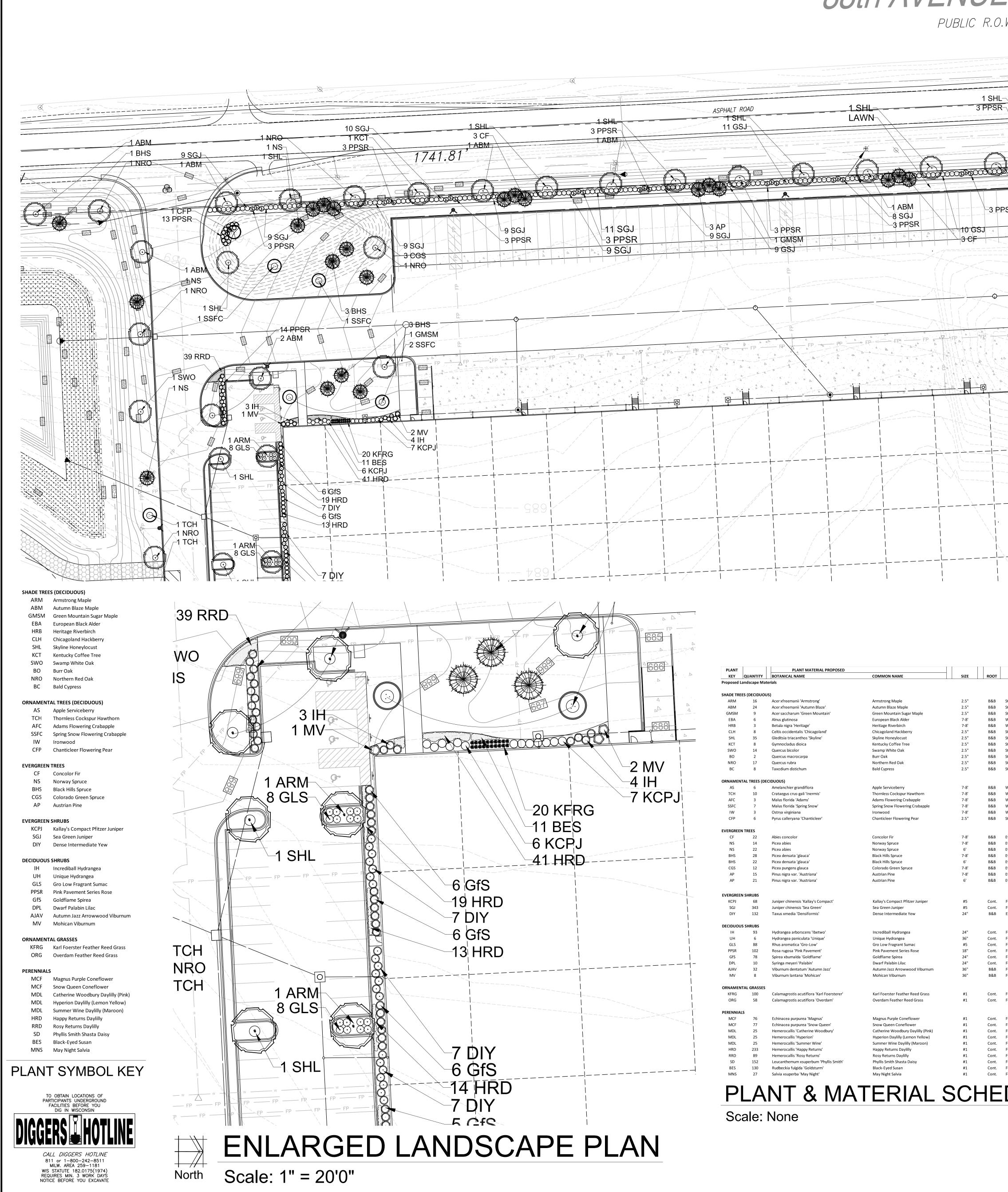
	NO POINT	NORTHLINE OF THE SW 1/4 OF THE NW 1/4		726'	40'-0" NW COR SW 1/4, NW 1/4		
					NW COR NW 1/4 SEC 16-1-22 N= 208314.490 E= 562153.124 CON. MON W/CAP FOUND		
has from exa of c							This Document, Either as Drawing or the Assemblage
e thi s nc m o actly draw DR	SIIE PLAN	COMMERCE CONSTRUCTION CO., L.P.					or Electronic Data, is the Property of Commerce Construction Co., L.P. ("Commerce")
)" is   rigi 1 /ing A				7			This document, including all electronic data attached
plot beei inal. inch g ha W CC 6		13191 Crossroads Parkway North					therein, may not be reproduced, distributed, modified, used or implemented in any way, in whole or in part.
n re If t n Ion s cha	PROPOSED BUILDINGS	Sixth Floor					without the written consent of commerce. This provide the second shall not be shear as anonirtanent to according with or
		City of Industry, California 91746-3497					arian not be taken as apputeriant to, associated with, or as a replacement for the officially sealed document, and is
							without warranty of any kind either expressed or implied.
nfirm or phy D -1 N0 8		1 elepnone: (202) 099-0433					Any person or organization making use of or relying upon this document is resonatible for confirming its accuracy
-  1" enla par is sical AT 0-1		license No 723302					and completeness. Commerce is not responsible for adfact or renorting diversions of diritiel data from this
	Pleasant Prairie, Wisconsin		REVISIONS	BY DATE	REVISIONS	BY DATE	document. The acceptance or use of the foregoing. be constructed as an acceptance of the foregoing.





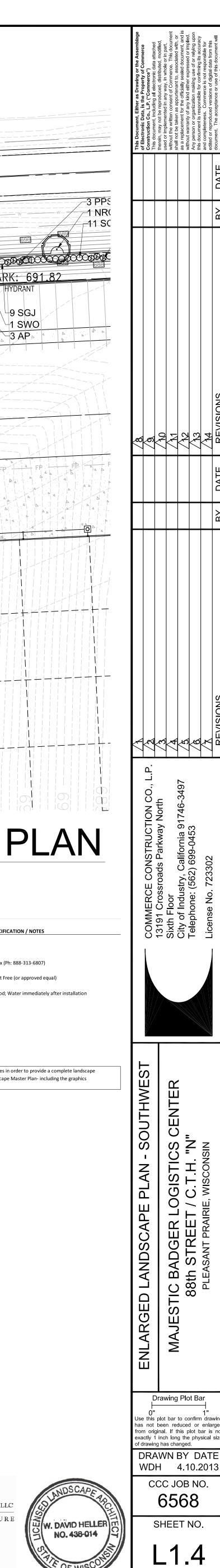






# 88th AVENUE

W. VARIES	//)		
PSR 9 SGJ	-11 SGJ -3 PPSR	9 SGJ	3 PPSR 1 KCT 9 SGJ 0 SGJ 0 STEBENCHMAR N. TLANGE BOLT ON H
HP A FP	3 CGS	1 SHL -3 AP	
	PLANT       PLANT MATERIAL PROPOR         KEY       QUANTITY       PLANT MATERIAL PROPOR         BOTANICAL NAME       BOTANICAL NAME         LAWN / NATIVE SEED MIXES       BOTANICAL NAME         VETLAND       1.6       Stormwater Seed Mix         LAWN       Lawn Establishment Area / Grading Area         7,725       Erosion Matting for sloped seeded area         SOD       Sodded Areas (identified on plan)	SED COMMON NAME	SIZE ROOT SPECIFIC Acre SY Cedar Creek Premium Blue Tag Seed Mix (Ph SF American Excelsior Company Curlex Net Fre SY Fresh cut, weed free; Bluegrass Blend Sod; V
Straight central leader, full and even crown. Prune only after planting Straight central leader, full and even crown. Prune only after planting Straight central leader, full and even crown. Prune only after planting Straight central leader, full and even crown. Prune only after planting Straight central leader, full and even crown. Prune only after planting Straight central leader, full and even crown. Prune only after planting Straight central leader, full and even crown. Prune only after planting Straight central leader, full and even crown. Prune only after planting Straight central leader, full and even crown. Prune only after planting Well balanced multi-stemmed tree with minimum four canes, and full appearance Well balanced multi-stemmed tree with minimum four canes, and full appearance Well balanced multi-stemmed tree with minimum four canes, and full appearance Well balanced multi-stemmed tree with minimum four canes, and full appearance Well balanced multi-stemmed tree with minimum four canes, and full appearance Well balanced multi-stemmed tree with minimum four canes, and full appearance Well balanced multi-stemmed tree with minimum four canes, and full appearance Straight central leader, full and even crown. Prune only after planting Evenly shaped tree with branching to the ground Evenly shaped tree with bran		34,500 SF Area: 34,500 SF irrigation / lighting access tities are provided as a service to the Landscape Contractor; Landsc on this Landscape Master Plan. In the event that a discrepancy occu and notations	CY CY CY LF Schedule 80 PVC ape Contractor is responsible for verifying these counts and quantities in irs between this schedule and the Landscape Master Plan, the Landscape depicted therein- shall govern. Seed at rate of 3# per 1000 SF
<ul> <li>Full, well rooted plant, evenly shaped</li> <li>Full, well rooted plant, evenly shaped</li> <li>Evenly shaped tree with branching to the ground</li> <li>Full, well rooted plant, evenly shaped</li> <li>Full, well rooted plant with vigorous top growth</li> </ul>			
Full, well rooted plant with vigorous top growth Full, well rooted plant with vigorous top growth			HELLER & ASSOCIATES LLA LANDSCAPE ARCHITECTUR One Redwood Court Racine, Wisconsin 53402 ph 262 639 9733 fx 262 639 9737 david@wdavidheller.com



1. Contractor responsible for contacting Diggers Hotline (811 or 800-242-8511) to have site marked prior to excavation or planting.

2. Contractor to verify all plant quantities shown on Plant & Material List and landscape planting symbols and report any discrepancies to Landscape Architect or General Contractor.

3. All plantings shall comply with standards as described in American Standard of Nursery Stock - Z60.1 ANSI (latest version). Landscape Architect reserves the right to inspect, and potentially reject any plants that are inferior, compromised, undersized, diseased, improperly transported, installed incorrectly or damaged. No sub-standard "B Grade" or "Park Grade" plant material shall be accepted. Plant material shall originate from nursery(ies) with a similar climate as the planting site.

4. Any potential plant substitutions must be approved by Landscape Architect or Owner. All plants must be installed as per sizes indicated on Plant & Material Schedule, unless approved by Landscape Architect. Any changes to sizes shown on plan must be submitted in writing to the Landscape Architect prior to installation.

5. Topspoil in Parking Lot Islands (if applicable): All parking lot islands to be backfilled with topsoil to a minimum depth of 18" to insure long-term plant health. Topsoil should be placed within 3" of finish grade by General Contractor / Excavation Contractor during rough grading operations/activity. The landscape contractor shall be responsible for the fine grading of all disturbed areas, planting bed areas, and lawn areas. Crown all parking lot islands a minimum of 6" to provide proper drainage, unless otherwise specified.

6. Tree Planting: Plant all trees slightly higher than finished grade at the root flare. Remove excess soil from the top of the root ball, if needed. Remove and discard non-biodegradable ball wrapping and support wire. Removed biodegradable burlap and wire cage (if present) from the top  $\frac{1}{3}$  of the rootball and carefully bend remaining wire down to the bottom of the hole. Once the tree has been placed into the hole and will no longer be moved, score the remaining  $\frac{2}{3}$  of the burlap and remove the twine. Provide three slow release fertilizer for each tree planted.

7. Tree Planting: Backfill tree planting holes 80% existing soils removed from excavation and 20% plant starter mix. Avoid air pockets and do not tamp soil down. Discard any gravel, rocks, heavy clay, or concrete pieces. When hole is  $\frac{2}{3}$  full, trees shall be watered thoroughly, and water left to soak in before proceeding to fill the remainder of the hole. Water again to full soak in the new planting. Each tree shall receive a 3" deep, 4-5' diameter (see planting details or planting plan) shredded hardwood bark mulch ring around all trees planted in lawn areas. Do not build up any mulch onto the trunk of any tree. Trees that are installed incorrectly will be replaced at the time and expense of the Landscape Contractor.

8. Shrub Planting: All shrubs to be planted in groupings as indicated on the Landscape Plan. Install with the planting of shrubs a <sup>50</sup>/<sub>50</sub> mix of plant starter with topsoil. Install topsoil into all plant beds as needed to achieve proper grade and displace undesirable soil (see planting detail). Remove all excessive gravel, clay and stones from plant beds prior to planting. When hole(s) are  $\frac{2}{3}$  full, shrubs shall be watered thoroughly, and water left to soak in before proceeding. Provide slow-release fertilizer packets at the rater of 1 per 24" height/diamter of shrub at planting.

9. Mulching: All tree and shrub planting beds to receive a 3" deep layer of high quality shredded hardwood bark mulch (not pigment dyed or enviro-mulch). All perennial planting areas (groupings) shall receive a 2" layer of shredded hardwood bark mulch, and groundcover areas a 1-2" layer of the same mulch. Do not mulch annual flower beds (if applicable). Do not allow mulch to contact plant stems and tree trunks.

10. Edging: All planting beds shall be edged with a 4" deep spade edge using a flat landscape spade or a mechanical edger. Bedlines are to be cut crisp, smooth as per plan. A clean definition between landscape beds and lawn is required. Pack mulch against lawn edge to hold in place.

11. Plant bed preparation: All perennial, groundcover and annual areas (if applicable) are required to receive a blend of organic soil (Soil Amendments) amendments prior to installation. Rototill the following materials at the following ratio, into existing soil beds or installed topsoil beds to a depth of approximately 8"-10"

Per 100 SF of bed area: <sup>3</sup>/<sub>4</sub> CY Peat Moss or Mushroom Compost

<sup>3</sup>⁄<sub>4</sub> CY blended/pulverized Topsoil 2 pounds starter fertilizer

 $\frac{1}{4}$  CY composted manure

12. Lawn Installation for all sodded turfgrass areas: Contractor to furnish and prepare blended topsoil (2" minimum) and sod bed, removing all debris and stones ½" and larger. Apply a 10-10-10 starter lawn fertilizer uniformly throughout areas prior to laying sod. Use only premium sod blend according to TPI (revised 1995) and ASPA Standards. Install sod uniformly with staggered joints, laid tightly end to end and side to side. Roll sod with a walk behind roller and water immediately upon installation to a 3" depth. Stake any sod installed on slopes steeper than 1:3, and in all swale applications. Contractor is responsible to provide a smooth, uniform, healthy turf, and is responsible for the first two mowings of the newly installed turf, and is also responsible for watering during this period.

13. Installation preparation for all seeded areas: remove/kill off any existing unwanted vegetation prior to seeding. Prepare the topsoil (if adequate or provide as in item #6 above) and seed bed by removing all surface stones 1" or larger. Apply a starter fertilizer and specified seed uniformly at the specified rate, and provide mulch covering suitable to germinate and establish turf. Provide seed and fertilizer specifications to Landscape Architect and Owner prior to installation. Erosion control measures are to be used in swales and on slopes in excess of 1:3 and where applicable (see Civil Engineering Drawings). Methods of installation may vary are the discretion of the Landscape Contractor on his/her responsibility to establish and guarantee a smooth, uniform, quality turf. A minimum of 2" of blended, prepared and non-compacted topsoil is required for all lawn areas. If straw mulch is used as a mulch covering, a tackifier may be necessary to avoid wind dispersal of mulch covering. Marsh hay containing reed canary grass is NOT acceptable as a mulch covering.

An acceptable quality seed installation is defined as having: No bare spots larger than one (1) square foot

No more than 10% of the total area with bare areas larger than one (1) square foot A uniform coverage through all turf areas

14. No-Mow seed areas: "No-Mow" fine fescue seed mix with annual rye nurse crop (available at Cedar Creek Seed Farm 888-313-6807; or Prairie Nursery 608-296-3679) or approved equivalent mix from a reputable seed mix provider. Apply at 220 lbs per acre or at rate recommended by supplier. Prepare seed bed and soil as specified in item #13 above.

15. Native Prairie Seed Mix / Stormwater Seed Mix: Native seed mixes as listed on the Plant and Material List or other seeding schedules outlined on the landscape plan set. Seed mixes available from Prairie Nursery 608-296-3679 or JF New 608-848-1789 or approved equivalent mix from a reputable seed mix provider. Apply at rates specified herein, or per supplier recommendation. Prepare soil and seed bed as in item #13 above.

16. Warranty and Replacements: All plantings are to be watered thoroughly at the time of planting, through construction and upon completion of project as required. Trees, Evergreens, and Shrubs (deciduous and evergreen) shall be guaranteed (100% replacement) for a minimum of one (1) year from the date of project completion. Perennials, groundcovers, and ornamental grasses shall be guaranteed for a minimum of one (1) growing season. Perennials, groundcovers, and ornamental grasses planted after September 15th shall be guaranteed through May 31st of the following year. Only one replacement per plant will be required during the warranty period, except for losses or replacements due to failure to comply with specified requirements. Watering and general ongoing maintenance instructions are to be supplied by the Landscape Contractor to the Owner upon completion of the project.

17. The Landscape Contractor is responsible for the watering and maintenance of all landscape areas for a period of 45 days after the substantial completion of the landscape installation. This shall include all trees, shrubs, evergreens, perennials, ornamental grasses, turf grass, no-mow grass, and native prairie seed mix / stormwater seed mix. Work also includes weeding, edging, mulching (only if required), fertilizing, trimming, sweeping up grass clippings, pruning and deadheading.

18. Project Completion: Landscape Contractor is responsible to conduct a final review of the project, upon completion, with the Landscape Architect, Client or Owner / Client Representative, and the General Contractor to answer questions, provide written care instructions for new plantings and turf, and insure that all specifications have been met.

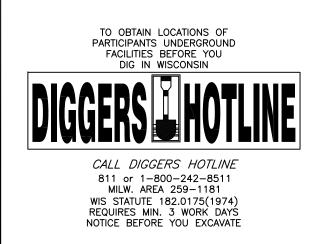
## LANDSCAPE GENERAL NOTES

Scale: None

Approximate area of coverage: Total area (SF) of coverage surrounding infiltration ponds: Total acreage of coverage surrounding bio filtration ponds:

Percentage of area for side slope/bank coverage: Total acreage of Stormwater Seed Mix:

Percentage of area for upland prairie mix coverage: Total acreage of JF New Economy Prairie Seed Mix:



Forl 69,400 1.59 70% 1.12 Perm Care Care 30%

0.48

 $\Omega$ 

S

 $\mathcal{O}$ 

Ś

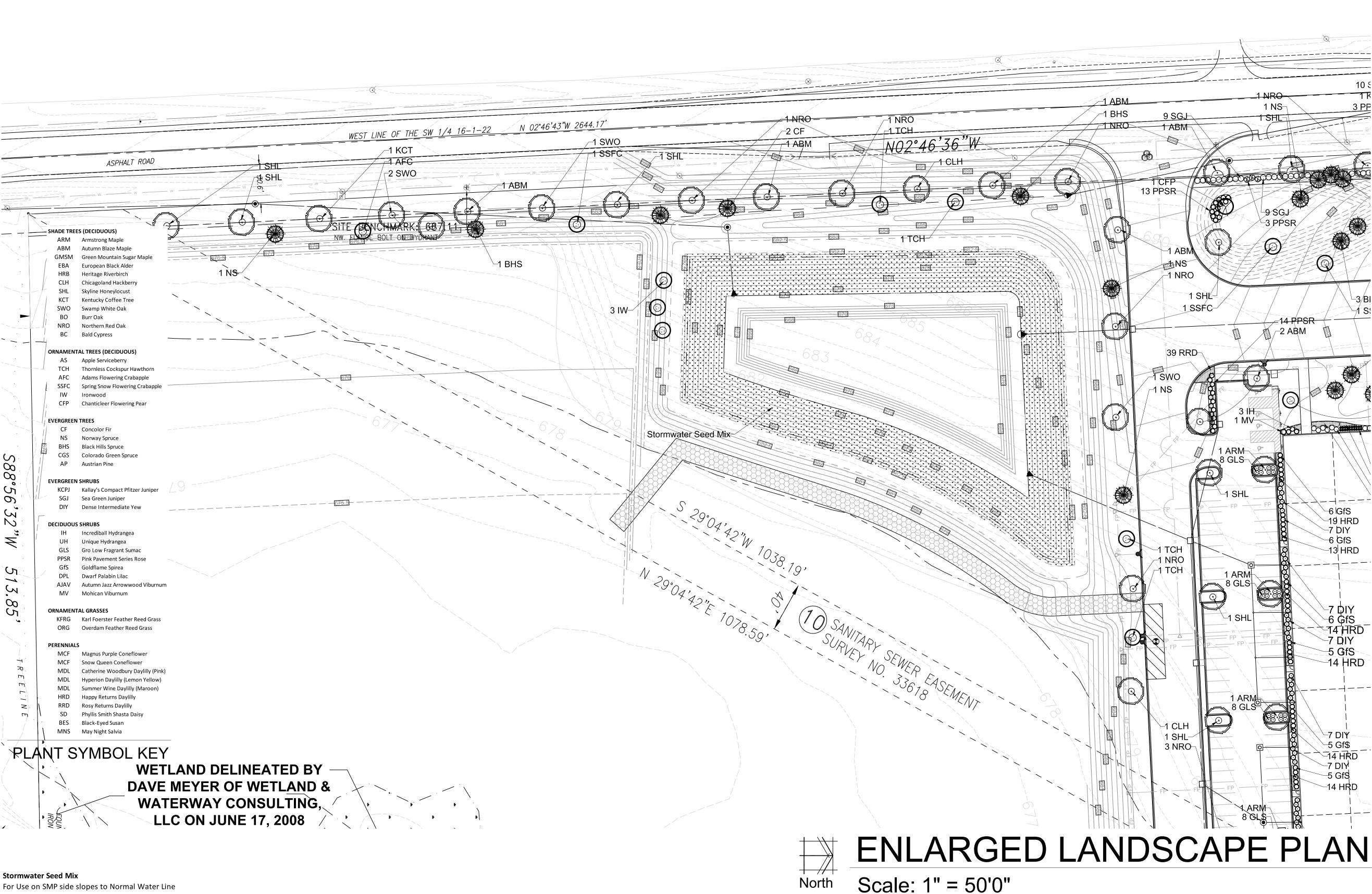
Nh

Care Care Care Flec Scirp Scirp

Aven Loliu

Forb Alisn Ascle Bider Helei Mim Penti Polyg Rudb Sagit Senno Thali





r Use on SMP side slopes to Norm	nal Water Line						
			PLS	JF New, Inc.			
<u>tanical Name</u>	<u>Common Name</u>		<u>Ounces/Acre</u>	www.jfnew.com	1402 Pankratz Street		
				708 Roosevelt Road	Suite 302		
rmanent Grasses/Sedges/Rushes	:			Walkerton, IN 46574	Madison, WI 53704		
rex cristatella	Crested Oval Sedge		1.00	Ph: 574-586-3400	Ph: 608-240-1453		
rex frankii	Bristly Cattail Sedge		1.00				
rex lurida	Bottlebrush Sedge		2.00	Economy Prairie Seed Mix			
rex sparganioidex v. cephaloidea	Rough-Clustered Sedge		2.00			PLS	
rex vulpinoidea	Brown Fox Sedge		6.00	<u>Botanical Name</u>	Common Name	Ounces/Acr	<u>re</u>
eocharis ovata	Blunt Spike Rush		0.50				
vmus virginicus	Virginia Wild Rye		12.00	Permanent Grasses:			
vceria striata	Fowl Manna Grass		1.25	Andropogon gerardii	Big Bluestem		32.00
ncus effusus	Common Rush		1.00	Bouteloua curtipendula	Side-Oats Gramma		18.00
ncus torreyi	Torrey's Rush		0.25	Carex bicknellii / Carex brevior	Prairie Sedge Mix		1.00
ersia oryzoides	Rice Cut Grass		1.00	Elymus canadensis	Canada Wild Rye		16.00
nicum virgatum	Switch Grass		2.00	Panicum virgatum	Switch Grass		2.50
rpus atrovirens	Dark Green Rush		1.00	Schizachyrium scoparium	Little Bluestem		24.00
rpus cyperinus	Wool Grass		0.50	Sorghastrum nutans	Indian Grass		12.00
rpus fluviatilis	River Bulrush		0.25	-	То	otal	105.50
rpus validus	Great Bulrush		6.00				
		Total	45.75	Temporary Cover:			
				Lolium multiflorum	Annual Rye		480.00
mporary Cover:				2	-	otal	480.00
ena sativa	Common Oat		360.00				
lium multiflorum	Annual Rye		116.00	Forbs:			
,		Total	476.00	Asclepias syriaca	Common Milkweed		0.50
				Asclepias tuberosa	Butterfly Milkweed		1.00
rbs:				, Aster leavis	, Smooth Blue Aster		0.25
sma spp.	Water Plantain (Various Mix)		4.25	Aster novae-angliae	New England Aster		0.75
clepias incarnata	Swamp Milkweed		1.50	Chamaecrista fasciculata	Partridge Pea		8.00
dens spp.	Bidens (Various Mix)		2.00	Coreopsis lanceolata	Sand Coreopsis		3.00
lenium autumnale	Sneezeweed		3.00	Echinacea purpurea	Broad-Leaved Purple Coneflower		5.00
mulus ringens	Monkey Flower		1.00	Heliopsis helianthoides	False Sunflower		0.25
nthorum sedoides	Ditch Stonecrop		0.50	Lupinus perennis	Wild Lupine		0.50
lygonum pensylvanicum	Pinkweed		4.00	Pycanthemum tenuifolium	Mountain Mint		0.50
dbeckia subtomentosa	Sweet Black-Eyed Susan		1.00	Ratibida pinnata	Yellow Coneflower		2.50
gittaria latifolia	Broad-Leaf Arrowhead		1.00	Rudbeckia hirta	Black-Eyed Susan		6.00
nna hebecarpa	Wild Senna		1.00		•	otal	28.25
alictrum dasycarpum	Purple Meadow Rue		2.00		10		20.29
		Total	21.25	Annual/Perennial Forbs Mix			24.00
			21.23				27.00
STORMW	AIEK DE		IVIIA		1Y PRAIRIE		IVIIĂ

## STORMWATER SEED MIX

Scale: None

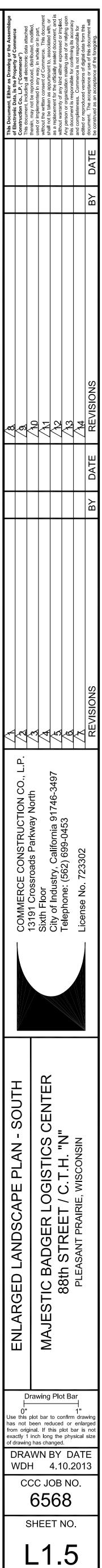
Scale: None

### PLS Ounces/Acre

32.00
18.00
1.00
16.00
2.50
24.00
12.00
105.50
480.00
480.00
0.50
1.00
0.25
0.75
8.00
3.00
5.00
0.25
0.50
0.50
2.50
6.00
28.25
24.00
24.00



h------\_\_\_\_ 10 \$ Ø -3/BI -1 S 14 PPSR ☆ *─*2 ABM -6 GfS -19 HRD -7 DIY -6 GfS -13<sup>|</sup> HRD \_6 GfS\_\_\_ \_14 HRD \_7 DIY 5 GfS 14 HRГ ' DI\ W. DAVID HELLER NO. 438-014



Consider the request of Louis Tricoli, Developer and Owner for another **one (1) year extension of the Conceptual Plan for the proposed Paradise Lake Subdivision** on the properties generally located south of 104<sup>th</sup> Street and 22<sup>nd</sup> Avenue.

### **Recommendation:**

Village staff recommends approval of another one (1) year extension (until April 21, 2014) for the Conceptual Plan approval for the proposed Paradise Lake Subdivision. Prior to the expiration of the Conceptual Plan all conditions of the April 21, 2008 approval shall be satisfied and the Preliminary Plat shall be submitted. A copy of said conditions of approval is on file with the Community Development Department. In addition, the Preliminary Plat shall comply with any new Village Ordinance amendments effective in the Village since the initial 2008 conditional approval.

### VILLAGE STAFF REPORT OF MAY 20, 2013

Consider the request of Louis Tricoli, Developer and Owner for a **one (1) year extension of the Conceptual Plan for the proposed Paradise Lake Subdivision** on the properties generally located south of 104<sup>th</sup> Street and 22<sup>nd</sup> Avenue.

On April 6, 2011, the Village received a request from the Owner and Developer for another one (1) year extension of the Conceptual Plan for the conditionally approved Paradise Lake Subdivision on the properties generally located south of 104<sup>th</sup> Street at 22<sup>nd</sup> Avenue.

The Village Board conditionally approved the Conceptual Plan for the proposed development of 45.83 acres of land to be developed into 20 single-family lots and four (4) Outlots on April 21, 2008. A Conceptual Plan approval is valid for a period of one (1) year. Prior to the expiration of the Conceptual Plan all conditions of approval shall be satisfied and the Preliminary Plat shall be submitted.

On March 2, 2009, the Village Board approved a one (1) year extension (until April 21, 2010), on April 19, 2010 the Village Board approved a second one (1) year extension (until April 21, 2011), on April 18, 2011 the Village Board approved a third one (1) year extension (until April 21, 2012) and on June 4, 2012 the Village Board approved a forth one (1) year extension (until April 21, 2013). Due to the economy, the Developer is requesting another one (1) year extension to comply with the conditions of the Conceptual Plan approval (until April 20, 2014).

Village staff recommends approval of another one (1) year extension (until April 21, 2014) for the Conceptual Plan approval for the proposed Paradise Lake Subdivision. Prior to the expiration of the Conceptual Plan all conditions of the April 21, 2008 approval shall be satisfied and the Preliminary Plat shall be submitted. A copy of said conditions of approval is on file with the Community Development Department. In addition, the Preliminary Plat shall comply with any new Village Ordinance amendments effective in the Village since the initial 2008 conditional approval.

TO: Ms. Jean Werbie & The Village Board Fm: Louis Tricoli Re: Paradise Lake Subdivision

### 04/27/2013

Pleasant Prairie Village Board,

I would like to request another extension for the property known as Paradise Lake subdivision at this time. I am sorry for the delinquency on my application but would greatly appreciate an extension for my application on my project. I have put a great deal of time and effort into this project and like to continue on the path of completion.

> Thank You, Louis Tricoli Paradise Lake Estates



**Village of Pleasant Prairie** 

General Location map





Date Printed: 5/15/2013



DISCLAIMER This map is neither a legally recorded map nor a survey and is not intended to be used as one. This drawing is a compilation of records, data and information located in various state, county and municipal offices and other sources affecting the area shown and is to be used for reference purposes only. Kenosha County is not responsible for any inaccuracies herein contained. If discrepancies are found, please contact Kenosha County.

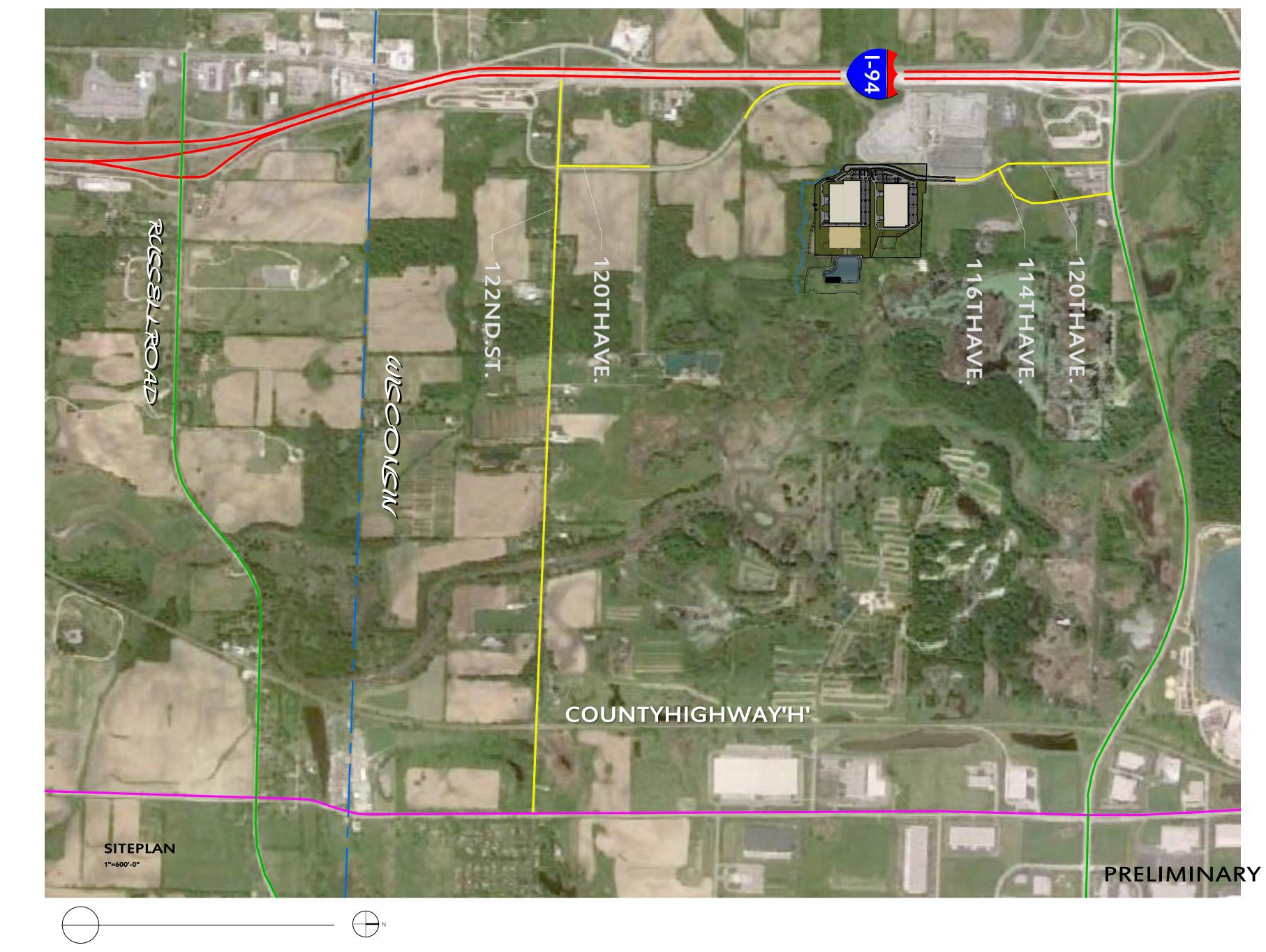


Office of the Village Engineer/Building Inspection Michael Spence, P.E., LEED<sup>®</sup>AP

TO:	Mike Pollocoff/Village Administrator
CC:	Jane Romanowski/Village Clerk
FROM:	Mike Spence/Village Engineer
DATE:	May 16, 2013
SUBJ:	Professional Services Agreement-Crispell Snyder Design Services 116 <sup>th</sup> Avenue Extension Construction, Utilities and Grading

The Village is proposing to extend 116<sup>th</sup> Avenue south approximately 0.25 mile to allow for potential development. This is in the vicinity of the Premium Outlet mall and just south of 110<sup>th</sup> Street. The proposed work will include the design of roadway, sidewalks, street lighting, sanitary sewer, water main, storm sewer, retention pond, certified survey map (CSM) for the road right-of-way and mass grading to make the site ready for development.

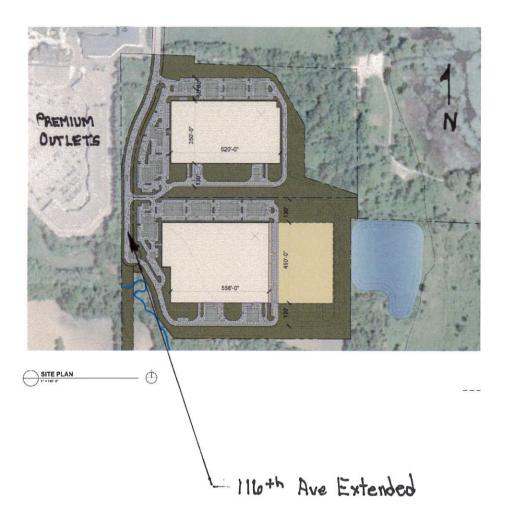
Proposals were received from Clark Dietz and Crispell-Snyder, Inc. for this effort. Both firms are qualified to perform these services. I also asked for a compensation proposal from each firm. Based on their qualifications and compensation I am recommending that the contract be executed with Crispell-Snyder, Inc. to perform these services. The total estimated not-to-exceed fee is \$105,600.



CivilEngineer StructuralEngineer LandscapeArchitect InteriorArchitect MEPEngineer

JobNo.

## Proposed 116<sup>th</sup> Avenue Extended South





Office of the Village Engineer/Building Inspection Michael Spence, P.E., LEED<sup>®</sup>AP

TO:	Mike Pollocoff/Village Administrator
CC:	Jane Romanowski/Village Clerk
FROM:	Mike Spence/Village Engineer
DATE:	May 16, 2013
ATTACHME	NTS: Intersection Exhibits (2)
SUBJ:	Professional Services Agreement-Traffic Analysis & Design, Inc. Lakeview Traffic Impact Analysis

There are a number of developments that are being constructed and are planned for the Lakeview Corporate Park. To adequately plan for the potential traffic impacts from a total of 5 new developments within the Pleasant Prairie Lakeview Corporate Park plus the proposed WisDOT Park & Ride lot, the Village requested proposals from three firms to conduct a traffic impact analysis for the Corporate Park. As such, it is necessary to analyze 14 key intersections within the study area to determine the following:

1. Existing Conditions Operations Level of Service (to determine a basis of comparison)

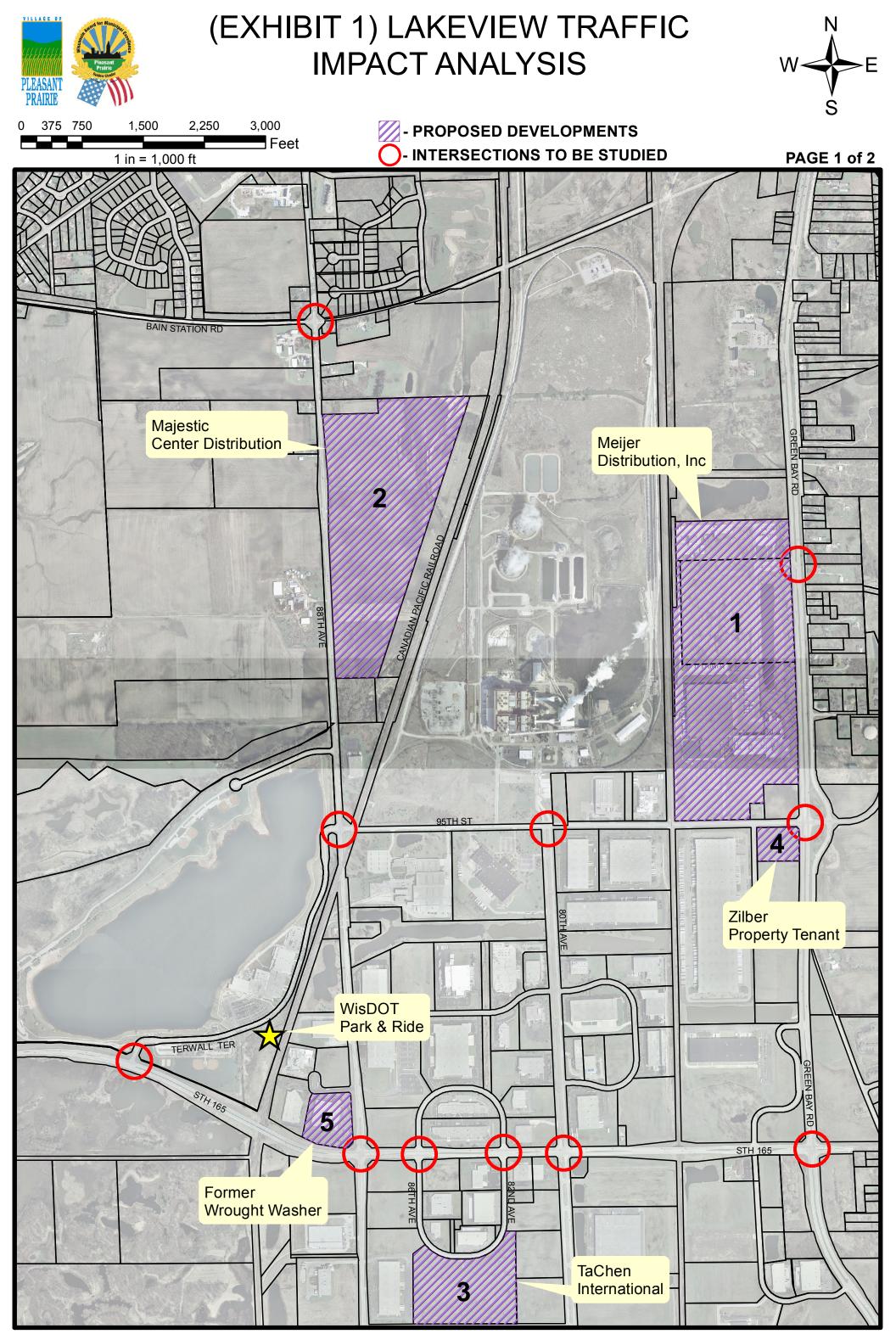
2. Future Conditions Operations Level of Service (with no improvements – to illustrate the traffic impacts from these developments)

3. Future Conditions Operations Level of Service (with improvements as needed to obtain LOS D or better for all traffic movements at the study area intersections). These improvements may include modification to intersection control, signals, roundabouts, additional lanes, turn lanes, turn lane extensions, turn prohibitions, channelization, etc.

4. Crash Analysis on STH 165 and CTH H to determine if any engineering/geometric/traffic control improvements are warranted based on crash patterns.

The structure of the report will allow the Village to assess a development or developments for any intersection/roadway improvements as a result of the traffic generated and impacts by the specific development. It will also address the WisDOT rehabilitation plans for STH 165 from STH 31 to I-94 to determine the adequacy of the improvement plans to handle the additional development traffic.

Proposals were received from Crispell-Snyder, Inc. and Traffic Analysis & Design, Inc. (TADI) for this effort. Both firms are qualified to perform these services. I also asked for a compensation proposal from each firm. Based on their qualifications and compensation I am recommending that the contract be executed with TADI to perform these services. The total estimated not-to-exceed fee is \$26,966.80.

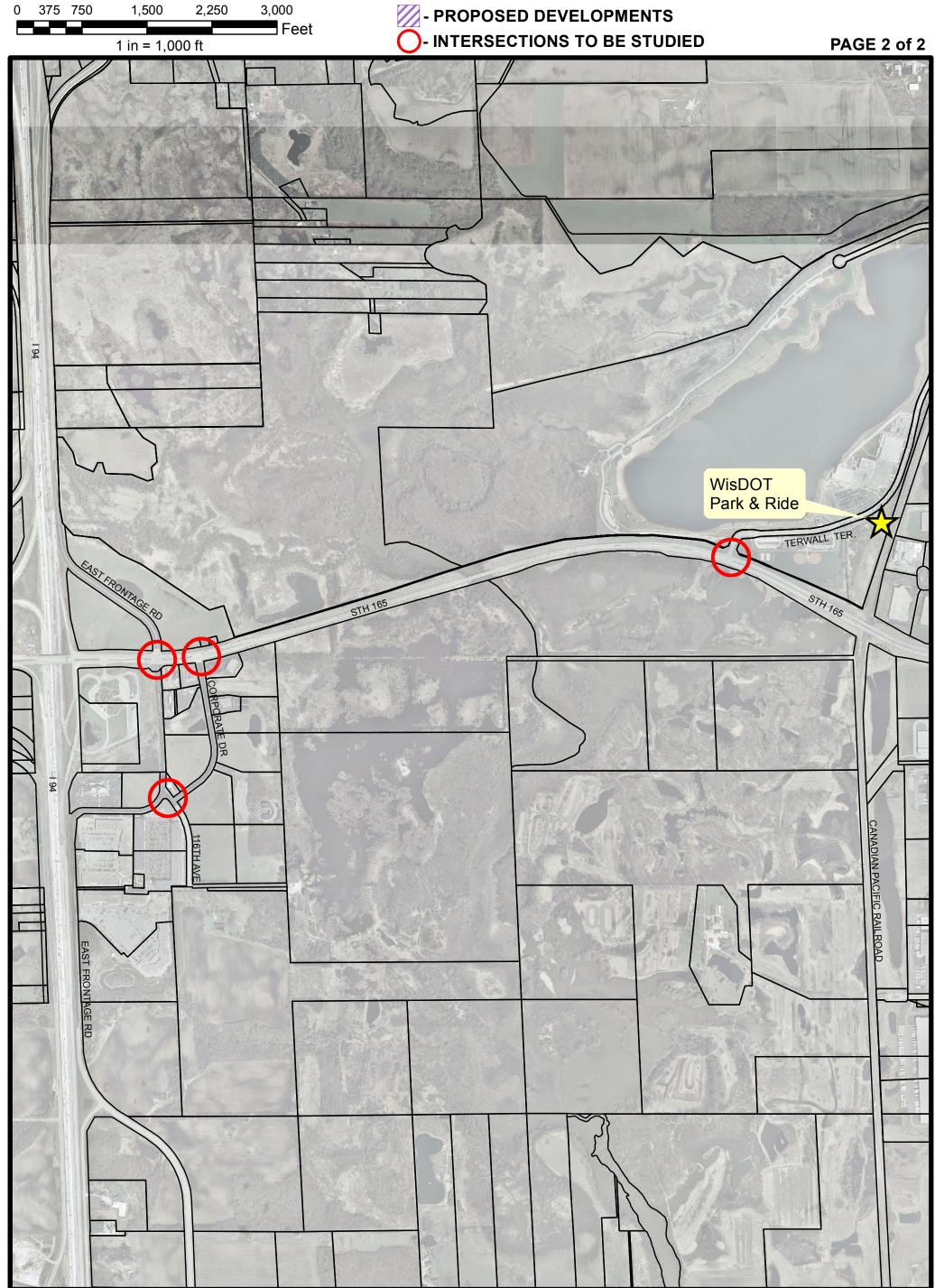


THE VILLAGE OF PLEASANT PRAIRIE MAKES NO REPRESENTAIONS CONCERNING THE ACCURACY OR VALIDITY PRESENTED ON THIS MAP. INFORMATION IS FOR GENERAL INFORMATIONAL PURPOSE ONLY AND IS NOT TO BE USED FOR SYSTEM DESIGN PURPOSES BY OTHERS.



# (EXHIBIT 1) LAKEVIEW TRAFFIC IMPACT ANALYSIS





THE VILLAGE OF PLEASANT PRAIRIE MAKES NO REPRESENTAIONS CONCERNING THE ACCURACY OR VALIDITY PRESENTED ON THIS MAP. INFORMATION IS FOR GENERAL INFORMATIONAL PURPOSE ONLY AND IS NOT TO BE USED FOR SYSTEM DESIGN PURPOSES BY OTHERS.



### **Firm Description**

Traffic Analysis & Design, Inc. is proud to have the largest traffic engineering staff of any engineering firm in Wisconsin. Traffic Analysis & Design, Inc. was established in 2002 to provide clients with competitive fees (lowest overhead rate any firm with extensive traffic engineering experience), exceptional quality and unmatched client service in the field of traffic engineering. Our growth from 3 traffic engineering professionals to 34 traffic engineering professionals in ten years, with over 95% of our client's being repeat clients, is a true indication of the quality of workmanship we provide. As a small firm focused on traffic engineering, we provide high quality workmanship and eliminate the corporate red tape often associated with larger companies resulting in quick and efficient response to your needs. Being a small firm also allows us to focus on competitive fees; our overhead rate is approximately thirty percent less than the overhead rates for large multi-disciplined firms.

Traffic Analysis & Design, Inc. has successfully completed over 1,400 traffic engineering projects for the WisDOT, counties, municipalities and private developers in the last ten years. We have conducted over 2500 traffic counts and 450 Traffic Impact Analyses (TIA's) in the past ten years, the most of any engineering firm in the State of Wisconsin. We have conducted over 75% of the TIA's submitted to WisDOT, of which all were approved by WisDOT. We have prepared over 250 traffic signal plans and over 100 traffic signal modification plans in the past six years. In addition, we have retimed over 500 signalized intersections for traffic signal timing and pedestrian timing improvements in the past 10 years. We are a WisDOT Certified TIA Provider and we have traffic engineering master contracts with the WisDOT. We have also prepared TIA's for some of the largest mixed use developments in Wisconsin consisting of Pabst Farms, Mayfair Mall, Fitchburg Master Plan, Bayshore Mall and Brookfield Square. We have also conducted several safety studies ranging from statewide safety studies for WisDOT covering hundreds of miles of roadways to local municipal safety studies addressing specific intersections.

In the last five years alone, we have completed traffic and/or safety studies in the following Wisconsin communities: Pleasant Prairie, Sturtevant, Mt. Pleasant, Somers, Summit, Racine, Kenosha, Bristol, Burlington, Lake Geneva, Oak Creek, Franklin, Wausau, Fond du Lac, Oshkosh, Greenville, Howard, Grand Chute, Little Chute, Bellevue, Sheboygan, Monona, Stoughton, Monroe, Dodgeville, Saukville, Wauwatosa, Janesville, Waukesha, West Allis, Brookfield, Oconomowoc, Grafton, Cedarburg, Mequon, Pewaukee, New Berlin, Muskego, Kewaskum, Brown Deer, Beaver Dam, West Bend, Mukwonago, Eagle, Madison, Oregon, Menomonee Falls, West Milwaukee, Delafield, Hudson, Marinette, Richfield, Delavan, River Falls, Milwaukee and Medford, in addition to conducting several traffic engineering projects for WisDOT throughout the state.

We have recently completed the Majestic Center Distribution TIA in Pleasant Prairie, which is included within the study area. We are also the traffic engineer for Meijer, and have their traffic data, for the Meijer Distribution Center is proposed within the study area. We have done TIA's along STH 165 over the years for the Simon Development (2006), KK Partners (2012) and other developments along the STH 165 corridor. We are very familiar with the study area and have a significant amount of the traffic data in house, which will be used for this TIA.



In addition to traffic engineering projects, we have the ability to effectively interact with the public on traffic engineering issues to achieve public acceptance/concurrence on your project. Our staff are leaders in the traffic engineering field. We have had technical papers published and have been invited to present papers at local, state, national and international conferences in addition to receiving awards of various engineering honors. Our staff has served as instructors for engineering courses at Marquette University, the University of Wisconsin-Madison, and the University of Wisconsin-Milwaukee. In addition, our senior level staff teaches traffic engineering courses throughout the country through the University of Wisconsin-Madison Extension.

Our office/contact information is as follows:

Traffic Analysis & Design, Inc. N36 W7505 Buchanan Ct Cedarburg, WI 53012

Phone: 1-800-605-3091 Fax: 262-377-4381 Email: jbieberitz@traffic-ad.com

Please call or e-mail me if you have any questions, comments or concerns. We are looking forward to continuing to work with the Village of Pleasant Prairie.

Very Truly Yours,

Traffic Analysis & Design, Inc.

John A. Bieberitz, P.E., PTOE Senior Traffic Engineer President

## PROJECT UNDERSTANDING

TADI understands that the Village is concerned about the potential traffic impacts from a total of 5 new developments within the Pleasant Prairie Lakeview Corporate Park plus the proposed WisDOT Park & Ride lot. As such, it is necessary to analyze 14 key intersections within the study area to determine the following:

- 1. Existing Conditions Operations Level of Service (to determine a basis of comparison)
- 2. Future Conditions Operations Level of Service (with no improvements to illustrate the traffic impacts from these developments) for the initial build year and the horizon year conditions
- 3. Future Conditions Operations Level of Service (with improvements as needed to obtain LOS D or better for all traffic movements at the study area intersections). These improvements may include modification to intersection control, signals, roundabouts, additional lanes, turn lanes, turn lane extensions, turn prohibitions, channelization, etc.
- 4. Crash Analysis on STH 165 and CTH H to determine if any engineering/geometric/traffic control improvements are warranted based on crash patterns.

The structure of the report will allow the Village to assess a development or developments for any intersection/roadway improvements as a result of the traffic generated and impacts by the specific development. The report will consist of WisDOT TIA. TADI will also address the WisDOT rehabilitation plans for STH 165 from STH 31 to I-94 to determine the adequacy of the improvement plans to handle the additional development traffic.

### PROJECT SCOPE

TADI will comply with all federal, state, county, local, and any other regulatory agency requirements regarding safety, traffic control, etc. We have organized the project approach or scope of this project into 6 separate tasks consisting of:

- Task 1: Data Collection
- Task 2: Trip Generation and Distribution and WisDOT TIA Initial Review Submittal
- Task 3: Analysis of Existing Conditions
- Task 4: Traffic Analysis of Build Conditions
- Task 5: Recommendations and Conclusions
- Task 6: WisDOT Full TIA Report and Meetings

TADI will initiate all contacts with the Village and County related to the land use plans, site plans and previous TIA's in the study area. All traffic analysis, findings and recommendations will be reviewed by the project manager, John Bieberitz, PE, PTOE. John has over 25 years of experience in traffic operations, traffic analysis and access.

The typical "problem" areas in traffic impact studies are lack of sufficient traffic count data, incorrect use of the trip generation rates per land use and unrealistic traffic projections. Based on our depth of traffic impact studies (over 450 TIA's in the past ten years), to eliminate these typical problems, we propose conducting new traffic counts on the study area roadways/intersections if the current traffic counts are more than 3 years old, which is the WisDOT standard. We have conducted countless planning studies and traffic generation for multiple land use plans of which the development is generally defined, but not specifically

defined (e.g. a gas station with x number of pumps, pharmacy, grocery store, etc.). For your land use plan, which has some general land uses defined, TADI will coordinate with the Village to make sure our assumptions for traffic generation fit the Village's vision for the area. Finally, to ensure we have accurate traffic projections, we are proposing to use our data base of WisDOT counts from 1976 to present as well as to utilize WisDOT central office projections for STH 165. This allows us to check and balance the traffic counts and future traffic projections as it will be the base for the traffic analysis and subsequent recommendations and improvements.

**Task 1 – Data Collection.** TADI will coordinate with WisDOT and the Village to obtain 24-hour traffic counts on the study area roadways.

TADI will utilize the traffic turning movements conducted by TADI (from recent past TIA's within the study area) and WisDOT at the following study area intersections within the study area:

- CTH H with Bain Station Road (TADI Previous Count)
- CTH H with 95<sup>th</sup> Street (TADI Previous Count)
- Green Bay Road with 95th Street (WisDOT Count)
- STH 165 with the I-94 East Frontage Road (WisDOT Count)
- STH 165 with Green Bay Road (WisDOT Count)
- STH 165 with CTH H/88<sup>th</sup> Avenue (WisDOT Count)
- STH 165 with 80<sup>th</sup> Avenue (WisDOT Count)

TADI will conduct the following traffic turning movement counts at the remaining intersections:

- 95<sup>th</sup> Street with 80<sup>th</sup> Avenue Weekday 6 AM to 9 AM and 3 PM to 6 PM
- STH 165 with Corporate Weekday 6 AM to 9 AM and 3 PM to 6 PM
- STH 165 with Terwall Terrace Weekday 6 AM to 9 AM and 3 PM to 6 PM
- 116<sup>th</sup> with Corporate Weekday 6 AM to 9 AM and 3 PM to 6 PM
- STH 165 with 86<sup>th</sup> Avenue peak hour between 6 AM to 9 AM and peak hour between 3 PM to 6 PM
- STH 165 with 82<sup>nd</sup> Avenue peak hour between 6 AM to 9 AM and peak hour between 3 PM to 6 PM

TADI will conduct the intersection turning movement counts, based on WisDOT standards with totals every 15 minutes accounting for autos, light trucks/ busses, heavy trucks, bikes and pedestrians tabulated separately.

TADI will compile the traffic count data from the various sources (TADI and WisDOT) and will breakdown the data to determine the weekday morning and evening peak hours and will balance the traffic data through the intersections. TADI will submit all the traffic turning movement count data to WIsDOT such that WisDOT can use the data to develop the horizon year traffic projections, which will be used as a base for the horizon year traffic analysis.

TADI will conduct a photo log of the study area intersections and will document the existing geometry, existing turn bay lengths, speed limits, and existing traffic control for development of an existing conditions base map.

TADI will collect crash data for the crash analysis. This will include obtaining MV4000 crash reports from WIsDOT and working with the County and Village for crash data/accident reports along CTH H for the years 2008 through 2012.

**Task 2 – Trip Generation and Distribution and WisDOT TIA Initial Review Submittal.** TADI will estimate the volume of traffic expected to be generated based on trip generation rate data published in the *ITE Trip Generation Manual, 9th Edition* for the following developments

- Meijer Distribution Center based on square footage of the building or specific trucking data and employee data from Meijer.
- Majestic Center Distribution based on the TIA completed by TADI
- TaChen International based on the square footage of the proposed building and specific use
- Zilber Property Tenant based on the square footage of the proposed building and specific use
- Former Wrought Washer based on the square footage of the building and potential future use
- WisDOT Park'n Ride based on the number of parking stalls and train activity
- Other future development as defined by the Village such as the buildout of the "Pleasant Farms Neighborhood", which may drive improvements along CTH H.

Traffic will be generated for the weekday AM peak hour, PM peak hour and 24-hour daily conditions. The traffic generated will be distributed to the study area based on a percent distribution of traffic for each development as follows:

- Meijer Distribution Center based on the Meijer truck routes and nearby residential areas (for employees)
- Majestic Center Distribution based on the trip distribution in the TADI TIA
- TaChen International based on an agreed upon trip distribution with the Village
- Zilber Property Tenant based on an agreed upon trip distribution with the Village
- Former Wrought Washer based on an agreed upon trip distribution with the Village
- WisDOT Park'n Ride based on an agreed upon trip distribution with the Village
- Pleasant Farms Neighborhood (if desired by the Village) based on an agreed upon trip distribution with the Village

TADI will prepare and assemble a WisDOT TIA Initial Review document. Exhibits will be prepared illustrating the trip generation, trip distribution and trip assignment scenarios for WisDOT review and approval, which will be included in the WisDOT TIA Initial Review. TADI will coordinate with WisDOT during the review of the TIA Initial Review for acceptance/approval on the report, prior to conducting the rest of the traffic analysis.

Note that if any of the development plans change (square footage, usage, access points, layout, etc.) after the TIA Initial Review TIA is submitted (which would change the trip generation or trip distribution/assignment), it will be considered additional work and will require a contract amendment.

**Task 3 – Analysis of Existing Conditions and Crash/Safety Analysis.** TADI will conduct a crash analysis for STH 165 between I-94 and STH 31 and on CTH H between STH 165 and Bain Station Road. TADI will review, analyze, and compile crash data for years 2008 to 2012 for these two corridors, specifically at the study area intersections. This will include reviewing MV4000 crash reports and preparing collision diagrams for the study area intersections on CTH H and STH 165. Collision diagrams will show individual crash dates, crash types, and highest level of crash severity. TADI will conduct a traffic safety analysis these intersections. TADI will use crash reports to identify crash patterns, trends, and recommend

treatments to improve safety. TADI will also prepare a formal safety performance evaluation including but not limited to:

- a. Summary of crash statistics
- b. Trends
- c. Patterns and crash countermeasures for each
- d. Intersection crash rates
- e. Segment crash rates
- f. Comparisons to statewide averages (provided by the DEPARTMENT)

The results of the safety analysis will be used to recommend improvements to the study area intersections for the existing conditions, which will be in addition to any operational improvement recommendations.

TADI will conduct the traffic operational analysis consistent with WisDOT TIA Guidelines. TADI will utilize the WisDOT approved software for the traffic analysis of the 14 study area intersections for the existing geometrics and existing traffic control for the Year 2015 Base Condition weekday AM and PM peak hour turning movement volumes.

TADI will make recommendations on intersection geometrics improvements, traffic control improvements, turn bay length extensions, channelization, etc. to achieve LOS D or better conditions for all movements at the 14 study area intersections.

**Task 4 – Traffic Analysis of Build Conditions**. TADI will revise the trip generation/distribution as needed based on the WisDOT review comments of the TIA Initial Review document. TADI will then analyze the 14 study area intersections with the Year 2015/Horizon Year full build traffic volumes for the weekday AM and PM peak hour conditions (volumes developed under Task 2). The analysis will utilize the methods in the WisDOT TIA Guidelines. Specifically, TADI will analyze each study area intersection for the weekday 2015/Horizon Year full build AM and PM peak hour volume conditions for the following scenarios:

- 1. Initial Build Conditions (2014 or 2015) with existing geometry and existing traffic control to determine operational deficiencies
- Initial Build Conditions (2014 or 2015) with recommended improvements of existing conditions (if any)

   to determine if additional improvements are needed above the existing condition improvements to achieve LOS D or better for all traffic turning movements
- 3. Horizon Year (2024 or 2025) Background Conditions (no development) with existing geometry and existing traffic control (WisDOT requirement)
- 4. Horizon Year (2024 or 2025) Total Build Conditions with the 2014/2015 improvements
- 5. Horizon Year (2024 or 2025) Total Build Conditions with additional improvement as needed to achieve LOS D or better for all intersection turning movements

TADI will recommend improvements such as intersection geometric improvements, traffic control modifications (signals/roundabout,4-way stop, etc.), turn bay length extensions, channelization, etc. to provide LOS 'D' or better for all traffic movements at the study area intersections. TADI will also address the STH 165 WisDOT improvements to verify the adequacy of the improvements with the additional development traffic.

If traffic signals are shown to be recommended based on peak hour traffic operating conditions, TADI will conduct a traffic signal warrant analysis based on peak hour traffic signal warrant and a projection of the four-hour traffic signal warrant. If an eight-hour traffic signal warrant or any other warrants are requested, it will be considered additional work requiring a contract amendment for the additional traffic counts (12-hour counts),

additional trip generation/assignment for 12-hours and calculation of the remaining warrants. It is noted that WisDOT requires a 12-hour signal warrant analysis for signalization of an intersection on STH 165.

It is noted that this scope of services does NOT include a WisDOT ICE analysis. If WisDOT requires an ICE analysis after review of the TIA, it will be considered additional work and will require a contract amendment.

**Task 5 – Recommendations and Conclusions.** TADI will provide a list of prioritized improvements based on the traffic analysis in Tasks 3 and 4. The list of prioritized improvements will be based on the safety analysis, existing conditions, full-build traffic volumes and severity of over capacity operations.

**Task 6 – WisDOT Full Report and Meetings.** A "draft" Traffic Impact Study report completed to the WisDOT TIA Guidelines documenting the findings of the analysis will be prepared by TADI and will be submitted to the Client for review and comments. The report will include text, tables and exhibits. The Client shall provide comments to TADI so that TADI can finalize the report and submit final copies to the Client and WisDOT.

If after review of the TIA report by WisDOT, WisDOT requires a revised analysis with an expanded study area or additional study periods, a full signal warrant study, an ICE analysis or other additions not included in this scope of services, it will be considered additional work and will require a contract amendment. However, TADI will complete any necessary revisions on the submitted analysis, tables, exhibits or text within this scope of services, if required by WisDOT for acceptance/approval.

One meeting with Pleasant Prairie is included in this scope of work and is expected to consist of a meeting with the Village after completion of the report to provide a list of prioritized improvements and to provide guidance to the Village in implementation of the plan. If an additional meeting or meetings are requested with attendance of TADI, it will be considered additional work and will require a contract amendment.

## SCHEDULE

The proposed project schedule per the project scope is as follows:

- Task 1: Data Collection 2 weeks
- Task 2: Trip Generation and Distribution and WisDOT TIA Initial Review Submittal 2 weeks
- Task 3: Analysis of Existing Conditions 1 week
- Task 4: Traffic Analysis of Build Conditions 2 weeks after receiving forecasts from WisDOT and receiving concurrence from the Village and WisDOT on the WisDOT TIA Initial Review.
- Task 5: Recommendations and Conclusions 3 weeks after receiving forecasts from WisDOT and receiving concurrence from the Village and WisDOT on the WisDOT TIA Initial Review.
- Task 6: Report and Meetings submittal of Draft Report 4 weeks after receiving forecasts from WisDOT and receiving concurrence from the Village and WisDOT on the WisDOT TIA Initial Review.

Tasks 1 thru 3 can be conducted prior to obtaining the traffic forecasts from WisDOT. Forecasts will be submitted to WisDOT within 7 days of completion of the data collection. Upon receiving the forecasts from WisDOT, TADI will submit a DRAFT report to the Village within 4 weeks. If the Village requires an accelerated schedule, every effort will be made to submit the report on the requested date.

## DESCRIPTIONS OF SIMILAR PROJECTS

TADI has successfully completed over 1400 traffic engineering projects for the WisDOT, municipalities and private developers in the last ten years. To illustrate a few of the projects which have similarities to this project, we have included the following traffic study/analysis projects which we have conducted (in the past five years) for various Wisconsin municipalities. Each of these projects was conducted to improve the safety and efficiency of traffic flow through a commercial corridor within the municipality:

- Majestic Property Development TIA Pleasant Prairie, WI
- Bellevue Master Plan, CTH GV Corridor Traffic Study Bellevue, WI
- North Hills Neighborhood Plan TIA Menomonee Falls, WI
- Village of Howard Development TIAs Howard, WI
- UWM Innovation Park TIA Wauwatosa, WI
- Grafton, STH 60 / N. Port Washington Road Traffic Study Grafton, WI
- USH 51 Intersection Control Evaluations (ICE) & Corridor Study, Minocqua, WI

In addition to the project list above, TADI has worked closely with several communities in the State of Wisconsin as well as with several WisDOT regions on various types of traffic studies ranging from safety studies, TIA's, corridor studies, access studies, intersection improvement studies and traffic signal timing/progression studies. For many of these communities, we act as an extension of their engineering staff to assist them with traffic related items. We are proud to say that we have conducted more traffic studies in the southeast region of the State than any other firm in Wisconsin. We encourage you to contact these municipal, county and WisDOT references:

**Village of Ashwaubenon** - This community is a suburb of Green Bay and boarders the Lambeau Field area. As such, Ashwaubenon is now addressing several redevelopments surrounding Lambeau Field. We have prepared and implemented traffic signal progression plans, traffic signal design plans and have conducted several traffic studies for the Village. We are currently working with the Village on several development traffic impacts, intersections and safety issues throughout the Village.

#### Contact: Doug Martin, Village Engineer, 920-492-2308

**Village of Grafton** – This community is experiencing significant growth in both the residential and commercial markets, especially in the area adjacent to the IH-43 interchange. We have recently conducted over 10 TIA's for the Village for significant developments such as: Costco, Kohl's, Colder's, Steinhafel's, Menard's, Aurora Hospital, Gauthier Biomedical, and now Meijer, in addition to several other retail and residential developments. We have also prepared signal design plans for temporary and permanent signals for eight signalized intersections in the past 5 years. We have also been called to address specific questions by the Plan Commission and Village Board on traffic concerns.

#### Contact: Dave Murphy, Director of Public Works/Village Engineer; 262-375-5325

**Village of Howard** – This community is a suburb of Green Bay and is experiencing significant development along the USH 41 corridors. Several developers have proposed large retail developments in the USH 41/Shawano/Dousman area. We have prepared several traffic studies for the Village of Howard for Woodman's, future Pick'N Save and other mixed use developments.

## Contact: David Wiese, Executive Director of Community Development, 920-434-4640 – OR - Joshua Smith, Village Administrator, 920-434-4640

**City of Mequon** – This community lies north of Milwaukee County and south of Cedarburg. This is considered one of the most affluent communities in the Milwaukee area, as such they have very high standards for development and quality of life issues within their community. We have provided the City with several TIA's, development traffic reviews as well as data collection services. We have been called to Plan Commission meetings to answer their specific traffic questions.

#### Contact: Kristin Lundeen, Engineering Services Manager, 262-236-2938

**City of Waukesha** - For the past six years, we have worked closely with the City of Waukesha on traffic studies, signal designs and signal retiming projects. Our staff has conducted all their traffic signal designs since 2000. Our staff has timed/retimed 40 of their 80 signalized intersections in the City. We recently completed three signal designs for the City this year and have provided timing plans for those signals. We have several on-going signal project contracts and a large city-wide traffic study with the City, which was conducted in 2012.

Contact: Paul Day, City Engineer, 262-524-3600

**City of West Allis** – We are called retained on a regular basis to assist the City Planning Department with TIA's, parking studies and development reviews of large redevelopments.

Contact: John Stibal, Director of Development, 414-302-8460; or Peter Daniels, Senior Engineer: 414-302-8374.

**Village of Menomonee Falls** – For the past four years, we have worked closely with the Village of Menomonee Falls on their traffic signal operations. We have designed all the newly installed traffic signals in the Village for the past four years. Jeff Fait of our office continues to be their on-call traffic signal engineer answering questions and assisting the Village with traffic signal operation modifications. In addition, we have conducted several recent TIA's in the Village such as the Kohl's Corporate Center/North Hills Neighborhood and Wal-Mart.

Contact: John Waller, Public Works Street Supervisor, 262-532-4745; or Tom Hoffman, Senior Engineer, 262-532-4415

**City of New Berlin** – This community has experienced significant growth in both residential and commercial development along Moorland Road and National Avenue. As such, we have conducted various traffic impact studies, traffic signal designs and traffic signal timing projects in the City of New Berlin since 2002.

Contact: Tammy Simonson, Senior Engineer, 262-754-1735

**City of Glendale** – Don Lee of our staff has worked closely with the City of Glendale addressing their traffic issues. We have conducted intersection improvement studies, signal design, signal modification plans and traffic signal timing plans for the City of Glendale.

Contact: Dave Eastman, Director of Public Works, 414-228-1746

**City of Franklin** – The City of Franklin is experiencing significant growth along 27<sup>th</sup> Street and other major arterials through the City. John Bieberitz of our office has worked closely with the City of Franklin on several traffic studies ranging from intersection improvement studies, corridor studies, traffic impact studies, signal designs and timing.

Contact: John Bennett, City Engineer, 414-425-7510

**City of Brookfield** – The staff of TADI has worked with the City of Brookfield since 1995 on traffic studies, traffic signal designs and timings. In the past five years, we have solidified our relationship with the City and have conducted TIA's ranging from the Brookfield Square Mall expansion, office buildings and retail development. We have continued to be their on-call consultant for traffic signal timings, modifications and signal design.

Contact: Jeff Chase, City Engineer, 262-787-3524

**Village of Brown Deer** – Jeff Fait of our office is currently working with the Village of Brown Deer on the signalization of STH 100 with Arbon Drive.

Contact: Jesse Thyes, Assistant Village Manager, 414-371-3052

**City of Janesville** – John Bieberitz of our staff has been working with the City of Janesville since 1992 on a variety of traffic engineering projects including the design of traffic signal timing/progression and reconstruction of traffic signals along STH 26 and USH 14, railroad preemption, safety improvements for the STH 26 frontage road system, and several TIA's for the numerous new developments along these corridors.

Contact: Dennis Ryan, Senior Engineer, 608-755-3171 – or – Mike Payne, City Engineer, 608-755-3164

**Waukesha County** – Our staff has been assisting the County with several traffic signal design projects from the plan stage through construction in addition to intersection improvement studies and most recently, the CTH F corridor study.

Contact: Gary Evans, Engineering Services Manager, 262-548-7740

**WisDOT SE Region** - We have conducted over 1200 traffic counts for the SE Region in addition to several corridor studies, safety studies and area traffic studies.

Contact: Robert Elkin, WisDOT SE Region Planning Supervisor, 262-548-8704

**WisDOT SW Region** - We currently have a statewide master contract with WisDOT for traffic engineering services. We have conducted several work orders in this region for intersection studies, signal investigation studies, safety studies and signal designs.

Contact: Dena Dramm, WisDOT Signal Engineer, 608-246-5360 Graham Heitz, WisDOT Traffic Engineer, 608-246-5362

**WisDOT NE Region** - We currently have a statewide master contract with WisDOT for traffic engineering services. We have conducted several work orders in this region for data collection and intersection studies. We have also conducted several Traffic Impact Analyses in this region.

Contact: Scott Nelson, WisDOT Operations and Safety Engineer, 920-492-5651

**WisDOT NC Region** - We have had a master contract with the Northcentral Region of WisDOT for the past five years. We have conducted several work orders for WisDOT in the communities of Marshfield, Colby, Stevens Point and Wisconsin Rapids. These work orders ranged from signal design, railroad preemption design and timings, detour routes, traffic control/temporary signal design, progression timing plans, TIA reviews, tube count data collection and roundabout analyses. We currently have an on-site contract with this region for ICE Analyses and Safety.

Contact: Chris Droes, WisDOT Traffic Engineer, 715-365-5749

**WisDOT NW Region** - We currently have a statewide master contract with WisDOT for traffic engineering services. We have conducted roadway safety audits, traffic signal designs and traffic signal timing plans in this region.

Contact: Greg Helgeson, WisDOT Traffic Engineer, 715-836-2980



## **Majestic Property Development TIA**

Pleasant Prairie, WI

**Project Location:** CTH H & Bain Station Road Village of Pleasant Prairie, WI

Client: Commerce Construction Co., L.P.

**Contact:** John R. Burroughs, LEED AP, President 562-948-4380



#### **Project Description:**

Majestic Realty Co. ("Majestic") is proposing to construct a 1.2 million square foot warehouse/distribution facility along the east side of CTH H between Bain Station Road and 95<sup>th</sup> Street. TADI performed the TIA for the Majestic Property Development which included an analysis along CTH H from STH 50 on the north to STH 165 on the south. The TIA was structured to account for the remaining build-out of the Pleasant Farms Neighborhood, which is generally bound by IH 94 on the west, CTH C and Bain Station Road to the north, CTH on the east, and 95<sup>th</sup> Street on the south, to aid Pleasant Prairie and Waukesha County plan the ultimate intersection needs of CTH H in the vicinity of the development. The TIA investigated all-way stop control warrants, traffic signal warrants, and roundabout possibilities for both the Bain Station Road and 95<sup>th</sup> Street intersections, as well as addressed anticipated cross-section recommendations along CTH H.

The following elements were conducted as part of this TIA studies:

- Data Collection & Trip Generation/Distribution/Assignment
- SYNCHRO analysis and modeling
- Improvements to the roadway network
- Signal Warrant Analyses
- Cross-section recommendations
- Traffic Impact Analysis report
- Coordination with the Village of Pleasant Prairie, Kenosha County and developer



## Bellevue Master Plan – CTH GV Corridor Traffic Impact Analysis

Bellevue, WI



#### **Client:**

Village of Bellevue Contact: Al Schultz; Zoning Administrator (920) 468-5225

#### **Project Description:**

The Village of Bellevue hired TADI to conduct an extensive TIA to determine interim and ultimate geometric improvements necessary for the implementation of the communities overall land use plan. The Village's land use plan included the development of approximately 570 acres of land with prime access to STH 172 via the CTH GV corridor located on the south side of Green Bay. Several developers, including Wal-Mart, submitted significant site plans to jump start the overall land use plan. Thirteen existing intersections as well as five new proposed access points were evaluated along the CTH GV and V corridors. Traffic Signal improvements as well as roundabout improvements were analyzed at the study area intersections. A cost analysis was also performed to help determine the cost sharing for the recommended improvements. The following elements were conducted as part of this project:

- Data Collection
- Coordination with WisDOT, Brown County and the Village of Bellevue
- RODEL roundabout analysis
- Conduct Traffic Signal Warrant Analysis
- SYNCHRO modeling
- Progression Analysis



## North Hills Neighborhood Plan TIA

Menomonee Falls, WI

#### **Project Location:**

USH 41/45, STH 145, STH 175, CTH W Northeast Waukesha County

#### **Client:**

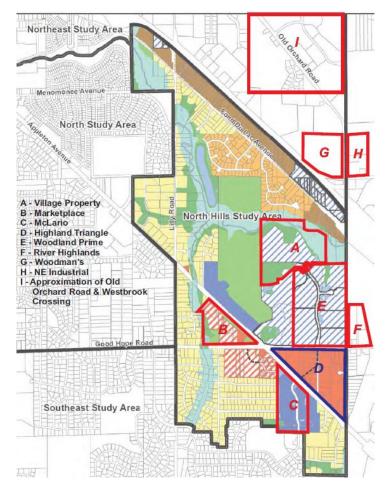
Village of Menomonee Falls

#### **Contact:**

Tom Hoffman, Village of Menomonee Falls Village Engineer (262) 532-4415

#### **Project Description:**

The Village of Menomonee Falls retained TADI to provide transportation planning and traffic engineering services for its North Hills Neighborhood Plan in an effort to accommodate a number of users, including a potential Fortune 500 company that employs over 4,000 employees. The study included 15 intersections, three USH 41/45 interchanges and their ramp meters, four scenarios for the extension of a critical collector roadway, and four land use/density alternatives to test the sensitivity of development decisions on infrastructure needs. The study provided the Village with the tools necessary to improve traffic flow within the public right-of-way, to



improve access to private property, and to plan for a target density that garnishes optimal benefit-cost. The study provided the Village with an improvement priority list and a timeline that linked critical roadway improvements to development. Most notably, TADI served as an intermediary between the Village and WisDOT and was able to help both parties agree to practical improvements within a critical one-month timeframe. The following elements were conducted as part of the project:

- Data collection
- Trip generation, distribution and assignment for 4 land use/density scenarios
- SYNCHRO modeling for 15 intersections, 3 interchanges, and 4 roadway extension scenarios
- Consensus-building with the Village and WisDOT
- Coordination of cost estimates for benefit-cost analysis



## <u>Village of Howard Developments TIAs – USH 41 at STH</u> 29/Shawano/Dousman

Howard, WI

#### **Project Location:**

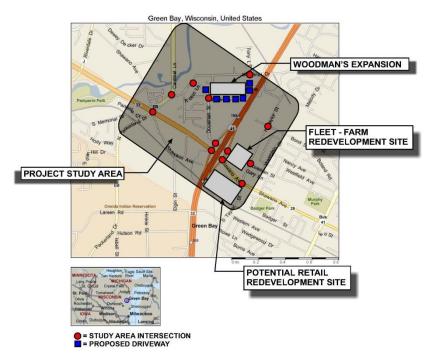
See map to the right.

#### **Reference:**

Village of Howard Contact: David Wiese, Executive Director of Community Development; Ph (920) 434-4640

#### **Project Description:**

TADI conducted TIAs within the same development area for two large developments and one large redevelopment site (Fleet-Farm) which also included WisDOT review and acceptance due to the access and modifications to the USH 41 interchange area. Several access options were analyzed for these



developments along with the WisDOT ultimate buildout plans for this area. TADI coordinated with WisDOT such that the traffic generation from these developments could be included in their traffic projections for the USH 41 reconstruction project. The results of the traffic analysis dictated the final access driveway locations for Woodman's and the potential retail development site south of Shawano Avenue as well as future access locations and along with the best location for a signalized intersection on Taylor Street for the redevelopment of Fleet Farm. A significant component of the project included coordination with the Village of Howard and WisDOT for construction staging impacts, temporary and permanent access through working meetings and traffic analysis results. The following elements were conducted as part of the project:

- Data Collection
- Trip Generation, distribution and assignment for several access scenarios
- Coordination with WisDOT for future design plans
- SYNCHRO modeling for the study area
- Public Involvement
- Consensus Building with WisDOT, the Village of Howard, adjacent developments, the Continental Companies, Woodman's and Mills Fleet-Farm



## **UWM Innovation Park TIA**

Wauwatosa, WI

#### **Project Location:** USH 41/45 Interchange with Watertown Plank Road Wauwatosa, Milwaukee County

**Client:** UWM Real Estate Foundation

#### **Contact:**

Bruce Block, UWM Real Estate Foundation Foundation Chair (414) 298-1000



#### **Project Description:**

The UWM Real Estate Foundation is planning an engineering campus to accommodate 250 apartment units, a 500 student university campus, and approximately 360,000-sf of research & redevelopment building space. With Hoyt Park and the Milwaukee Regional Medical Center – both located within 1 mile of the project site – also experiencing redevelopment, and with additional development within the Milwaukee County Research Park to the east and WisDOT Zoo Interchange reconstruction plans underway, the Foundation required the assistance of Traffic Analysis & Design, Inc. to help gain necessary approvals from the City of Wauwatosa, Milwaukee County and WisDOT. The TIA included internal circulation recommendations, improvements for pedestrians and transit users, and improvements to the internal and external roadway networks to ensure safe and efficient operations. Project tasks included:

- Data Collection & Trip Generation/Distribution/Assignment
- SYNCHRO analysis and modeling of a tight diamond interchange and a corridor of 5 traffic signalized interections
- Internal circulation recommendations, improvements for pedestrians and transit users, and improvements to the external roadway network.
- Coordination with the City of Wauwatosa, Milwaukee County & WisDOT.
- TIA & Public Meetings



## STH 60 / Port Washington Road Traffic Planning Study

Grafton, WI

#### **Project Location:**

STH 60, IH 43 to Cheyenne Avenue CTH W, Costco to Falls Road Village of Grafton

Client: Village of Grafton

#### Contact:

David Murphy, Village Engineer 262-375-5325

#### **Project Description:**

The Village of Grafton continues to experience increased development along STH 60 and along Port Washington Road and, along with it, the need for traffic engineering services. TADI has prepared TIAs for the Costco Commercial Site (large multi-use commercial area), Aurora Hospital, an industrial development, several general commercial sites, and most



recently a proposed Meijer Superstore proposed on CTH W, south of STH 60. TADI has performed a number of traffic signal warrant analyses for the area and has designed traffic signal installation plans for the STH 60 intersections with the IH 43 northbound ramps, IH 43 southbound ramps, Port Washington Road and Dakota Drive, as well as the Port Washington Road intersections with the Costco/Aurora access, Home Depot/Target access, and Hunters Lane. The following elements were conducted as part of these studies:

- Data Collection & Trip Generation/Distribution/Assignment
- SYNCHRO analysis and modeling
- TIA Report(s)
- Internal circulation recommendations, improvements for pedestrians and transit users, and improvements to the external roadway network
- Coordination with the Village of Grafton, Washington County, WisDOT and developers
- Signal Warrant Analyses
- TIA & Public Meetings
- Traffic signal design plans



## **USH 51 Intersection Control Evaluations (ICE) & Corridor Study**

Minocqua, WI

#### **Client:**

WisDOT NC Region Project ID: 1174-10-10 510 N. Hanson Lake Rd Rhinelander, WI 54501

#### **Contact:**

Richard Handrick (715) 365-5716 Richard.handrick@dot.wi.gov

#### **Project Description:**

The Wisconsin Department of



Transportation (WisDOT) has plans to reconstruct USH 51 in 2011. Through previous studies, a "Vision" plan was proposed that included a parallel local road system to USH 51 to enhance traffic circulation, minimize delays to side streets, and improve access to adjacent properties along USH 51. This study considered different traffic control alternatives (two-way stop control, all-way stop control, roundabout control, and traffic signal control) for four main intersections along the study corridor. Alternative were analyzed with both the existing (No Vision) roadway system and the proposed roadway system (Vision) that included the new local roadway connections to and alongside USH 51. The impacts were evaluated for the initial year of construction (year 2011) and for the design year (year 2031) to determine the most efficient improvements for each study intersection.

Based on the traffic control recommendations developed for the Intersection Control Evaluations (ICE) study, two additional traffic signals were recommended to be installed along the USH 51 corridor. TADI conducted a corridor analysis of the four signalized intersections to determine the most efficient cycle lengths, splits, and offsets that would minimize delays and traffic queuing along USH 51 during the peak hours.

The following elements were conducted as part of the project:

- SYNCHRO
- RODEL Roundabout Analysis
- Coordinated Signal Timings
- Signal and All-Way Stop Control Warrant Analyses
- Crash Analysis
- Data Collection

### ENGINEERING PERSONNEL TO PERFORM THE STUDY

TADI is recognized on the state and local level for outstanding traffic engineering and traffic planning expertise and will provide you with professional services that are undeniably the best available. The combination of our depth of resources, personal knowledge of the area, familiarity with the WisDOT analysis & design guidelines and our diverse workload consisting of private development, municipalities, counties, and the WisDOT make us uniquely qualified for this project.

TADI's staff includes thirteen traffic engineers, of which all thirteen of our traffic engineers are licensed Professional Engineers (PEs) and eight are certified as Professional Traffic Operations Engineers (PTOEs) by the Institute of Transportation Engineers (ITE). Our experienced traffic engineering staff means that all of our projects are conducted by Engineers with extensive traffic engineering experience and all of our projects are reviewed by a PTOE with at least 15 years' experience in traffic engineering, resulting in a quality project. Each of our engineers has a specific specialty area or niche in the traffic engineering/planning field, which allows us to continue to be a leader in the traffic engineering/planning field both locally and nationally.

The experience and talent of the individuals who make up the project team will, to a large extent, determine the success of your project. Because your project requires engineers with specific expertise in the field of traffic planning, operations, and WisDOT TIA guideline applications we have carefully organized our staff to provide you with engineers specializing in these areas. The staff members assigned to this project were also selected based on their ability to effectively interact with the public on traffic engineering/planning and design issues to achieve public acceptance/concurrence on your project.

The TADI engineers assigned to this project is as follows:

- John Bieberitz, PE, PTOE Project Manager
- Michael May, PE, PTOE Senior Traffic Engineer
- Amy Pomeroy, PE Traffic Engineer

One-page resumes of these engineers are on the following pages.





## JOHN BIEBERITZ, P.E., PTOE Senior Traffic Engineer

#### **Specialties:**

Traffic Impact Studies Traffic Signal Design & Timing Plans Roundabout Analysis Corridor Studies Teaching and Training

#### **Education:**

B.S. Civil Engineering, University of Wisconsin-Milwaukee, 1989

M.S. Transportation Engineering, University of Wisconsin-Milwaukee, 1994

#### **Certifications:**

Professional Engineer: Wisconsin, 1992 Professional Traffic Operations Engineer, 2003 WisDOT SE Region Certified TIA Preparer

#### **Professional Affiliation:**

Institute of Transportation Engineers, Wisconsin Chapter

Chairman of the ITE Traffic Engineering Workshop

Tau Beta PI, National Engineering Honor Society

### 1.800.605.3091 Direct: 262.377.1845 jbieberitz@tadi-us.com www.linkedin.com/in/jbieberitztraffic

Mr. Bieberitz is a Senior Traffic Engineer and also the President of Traffic Analysis & Design, Inc. Mr. Bieberitz manages the staff of 32 traffic engineering professionals in addition to project management and traffic engineering tasks. Mr. Bieberitz is responsible for traffic engineering tasks including signalized intersection analyses, roundabout analyses, signal progression analyses, development of traffic signal timing plans, traffic impact studies, corridor studies, traffic calming and traffic simulation.

Mr. Bieberitz has over 24 years of traffic engineering experience ranging from traffic signal designs/timing to traffic impact studies. Mr. Bieberitz has conducted over several hundred traffic impact studies, designed over one hundred traffic signals, analyzed over fifty roundabouts and has retimed several hundred traffic signals. Mr. Bieberitz serves as an "on-call" traffic engineer for several Wisconsin communities.

Mr. Bieberitz has presented and published several papers on traffic engineering for both the Institute of Transportation Engineers and the American Society of Civil Engineers. Mr. Bieberitz regularly teaches traffic engineering courses for the University of Wisconsin-Madison on topics such as intersection safety and efficiency, traffic impact analyses, access control, site design, traffic engineering software and signal design and timing.





## MICHAEL MAY, P.E., PTOE Senior Traffic Engineer

#### **Specialties:**

Traffic Calming Studies Corridor Traffic Analysis Traffic Impact Studies Safety Studies Pedestrian Accessibility Studies Alternatives Development/Analysis

#### **Education:**

B.S. in Civil Engineering, Marquette University, 2002

M.S. in Civil Engineering, University of Wisconsin – Milwaukee, 2008

#### **Certifications:**

Professional Engineer – Illinois & Wisconsin Professional Traffic Operations Engineer

#### Affiliations & Involvement:

ITE, International Member WI Traffic Engineering Council, Advisory Member

Young Professional of the Year Award, 2007

City of West Allis

Alderman & Plan Commissioner

Community Block Grant Committee, Vice Chair

**Comprehensive Plan Committee** 

**Bicycle & Pedestrian Steering Committee** 

### 1.800.605.3091 Ext. 83 Direct: 414.807.1912 mmay@tadi-us.com www.linkedin.com/in/trafficengineer

Michael is a Senior Traffic Engineer with Traffic Analysis & Design, Inc. His responsibilities include project management and performance of complex traffic impact studies and corridor studies, traffic calming studies, development of traffic signal timing plans (corridors and isolated), safety studies, signalized and stop-sign controlled intersection capacity analyses, and simulation & modeling. He serves as an in-house expert on innovative intersection treatments and traffic signal operations.

Mr. May has served as traffic engineer for more than 250 traffic studies in more than 80 communities – urban, suburban and rural. Studies range in size from investigating isolated intersection safety, to performing complex corridor studies, to retiming entire cities, to investigating the impacts of small to over 2,200+ acre development plans. He has also evaluated and/or modified numerous traffic studies conducted by other firms on behalf of public sector review agencies.

As a Plan Commissioner for the City of West Allis, Mr. May is leading the narrow street design standard for neighborhood streets in West Allis, as well as use of devices such as speed tables, bump-outs, chicanes, and mid-block narrowing/crossing, etc.

He occasionally co-instructs short courses at UW-Madison, and he guest lectures classes at Marquette University and UW-Milwaukee.





## AMY C. POMEROY, P.E. Traffic Engineer

#### **Specialties:**

Traffic Data Collection Management Intersection Capacity Analysis Traffic Impact Studies Traffic Safety Studies

#### **Education:**

B.S. Civil Engineering, University of Wisconsin – Milwaukee, 1992

#### **Certifications:**

Professional Engineer, 1998

1.800.605.3091 Direct: 414.803.3712 apomeroy@tadi-us.com www.linkedin.com/in/apomeroy

Ms. Pomeroy is a traffic engineer with Traffic Analysis & Design, Inc. based in Cedarburg, Wisconsin. Her current responsibilities include traffic count data management, assisting with intersection capacity analyses, corridor studies, traffic impact analyses, development of optimal signal timing plans, and corridor safety studies.

Ms. Pomeroy is responsible for compiling and reporting intersection turning movement counts for the WisDOT traffic count program, which involves over 200 intersections per year. Ms. Pomeroy has been instrumental in providing WisDOT with accurate intersection traffic count data for over 800 intersections throughout the State of Wisconsin.

Ms. Pomeroy has worked closely with senior engineers compiling, plotting and analyzing crashes on 34 corridors throughout Wisconsin for a Statewide Speed Management Study recently completed for WisDOT. She is currently assisting with research for a Rational/Irrational Speed Study also for WisDOT.

Ms. Pomeroy has twelve years of work experience ranging from data collection management to assisting in both municipal and WisDOT traffic impact analysis studies, corridor studies, traffic signal studies and corridor safety studies.

## PROPOSED TIME FRAME FOR COMPLETION

Refer to "SCHEDULE" on previously listed in this proposal.

## PROPOSED HOURS AND FEE FOR SERVICES

The following page contains our hours assigned per task per staff and the resulting lump sum fee based on the scope of work in this proposal.

## PROPOSED CHANGES OR DELETIONS TO SCOPE

Based on review of the RFP, the study area and past projects in the area completed by TADI, the potential additions to the scope may be as follows, which would require a contract amendment:

- 12-hour traffic turning movement counts for a full signal warrant study may be needed if traffic signals are recommended and warranted for the peak hour and four-hour volume warrant at the study area intersection(s) on STH 165
- Full traffic signal warrant study, based on more than the peak hour volume warrant or the fourhour volume warrant, if required by the Village, County or WisDOT
- General cost estimates of the recommended improvements
- Intersection Control Evaluation (ICE analysis), if it is desired to compare a signalized intersection improvement to a roundabout.
- Additional Meeting attendance (one meeting is included in this scope of services)

At this point, it is unknown whether a full traffic signal warrant analysis is needed until we conduct the traffic operational analysis. To minimize cost, we will conduct a partial signal warrant analysis for the peak hour and four hour warrants. If an intersection strongly meets these two warrants and an analysis shows the need for a signal, then a full traffic signal warrant analysis would be recommended.

If any of these items are requested to be added or any other items are requested to be added, TADI can add those items to the contract at any time thru the duration of the project through a contract amendment.

TASK	РМ		Eng	Jr. En	g	Technician	TOTAL	Expenses	•	FEE		
	\$ 118.	80 \$	88.00	\$ 66.	.00	\$ 37.40				1		
Terffin Date Online and Date Mana												
Traffic Data Collection and Base Maps Project Management and Administration	3						3		\$	356.40		
Traffic Count - Green Bay Road with 95th Street - Use DOT Count	0						0 0		\$	-		
Traffic Count - 95th St with 80th Avenue - 6-9 AM and 3-6 PM						10	10	\$160	\$	534.00		
Traffic Count - STH 165 with 86th Avenue - AM peak Hr, PM peak Hr						6	6	\$160	\$	384.40		
Traffic Count - STH 165 with 82nd Avenue - AM peak Hr, PM peak Hr						6	6	\$160	\$	384.40		
Traffic Count - STH 165 with Corporate - 6-9 AM and 3-6 PM						10	10	\$160	\$	534.00		
Traffic Count - STH 165 with Terwall Terrace - 6-9 AM and 3-6 PM						10	10	\$160	\$	534.00		
Traffic Count - STH 165 with East Fronatage Road - Use DOT Count							0		\$	-		
Traffic Count - STH 165 with CTH H/88th Avenue - Use DOT Count							0		\$	-		
Traffic Count - STH 165 with Green Bay Road - Use DOT Count							0		\$	-		
Traffic Count - STH 165 with 80th Avenue - Use DOT Count							0		\$	-		
Traffic Count - 116th with Corporate - 6-9 AM and 3-6 PM						10	10	\$160	\$	534.00		
Traffic Count - 88th Ave with Bain Station Road - Use prior TIA Count							0		\$	-		
Traffic Count - 88th Ave with 95th Street - Use prior TIA Count							0		\$	-		
Compile Traffic Count Data				6			6		\$	396.00		
Review other TIA reports			4				4		\$	352.00		
Coordinate with Village for future development plans and Conf Call			5				5		\$	440.00		
Photos, Distances, Geometrics, measure turn bay lengths - field trip			_			8	8	\$94	\$	393.20		
Balance Traffic Data, Apply seasonal factors, coordinate with DOT for projections			2				2		\$	176.00		
Historical Count Data, Submittal to WisDOT for Projections			3				3		\$	264.00	•	
Traffia Analysia Initial Duild Yaar and Havisan Yaar											\$ 5,2	282.40
Traffic Analysis - Initial Build Year and Horizon Year			8				8		¢	704.00		
Trip Generation for sites 1,3,4 and 5 - Initial Build and Full Build Trip Distribution (different for all sites)			8 6				6		\$ \$	704.00 528.00		
WisDOT TIA Initial Review Submittal	1		8			8	17		э \$	1,122.00		
Traffic Assignment - 2013 (AM and PM) - 14 intersections	1		11			0	11		\$	968.00		
Total Traffic 2013 (AM and PM)			8				8		գ \$	704.00		
Traffic Assignment - 2023 (AM and PM) - 14 intersections			8				8		\$	704.00		
Total Traffic 2023 (AM and PM)			6				6		\$	528.00		
Traffic Operational Analysis - Existing Conditions (14 intersections)			13				13		\$	1,144.00		
Traffic Operational Analysis - Build - no improvements (14 intersections) - 2013			13				13		\$	1,144.00		
Traffic Operational Analysis - Build - with improvements (14 intersections) - 2013			8				8		\$	704.00		
Traffic Operational Analysis - Base Conditions (14 intersections) - 2023 Background			12				12		\$	1,056.00		
Traffic Operational Analysis - Build - no improvements (14 intersections) - 2023			12				12		\$	1,056.00		
Traffic Operational Analysis - Build - with improvements (14 intersections) - 2023			8				8		\$	704.00		
Traffic Signal Warrant Analysis - 5 intersections estimated - peak hour and 4-hour only - 2	2013		12				12		\$	1,056.00		
Crash Analysis - STH 165 and CTH H - 5 years of Data			6	30		12	48		\$	2,956.80		
Recommendations	4		8				12		\$	1,179.20		
											\$ 16,2	258.00
Report & Meetings			00			00	50		¢	0 500 00		
Draft TIA	4		26			22	52	¢050	\$	3,586.00		
Final TIA - Addressing comments	6		6			4	10	\$350	\$	1,027.60		
Assist Pleasant Prairie in implementation of Plan - 1 meeting	6						6	\$100	\$	812.80	\$ 5.4	426.40
Total Hours	18		193	36		106	353	\$1,504	\$	26,966.80	+ - /	426.40 966.80
Raw Labor		40 ¢			00	\$ 3,964.40 <b>\$</b>	25,462.80	φ1,304	φ	20,300.00	ψ 20,8	500.00
Expenses	ψ 2,130.	φυ	10,304.00	ψ 2,570.	.00	ψ 0,304.40 Φ	\$1,504					
							ψ1,004					

Full TIA

\$ 26,966.80



May 9, 2013

#### RECEIVED

MAY 1 3 2013 Village of Pleasant Prairie

John Steinbrink Village of Pleasant Prairie 9915 39th Avenue Pleasant Prairie, WI 53158

Dear Mr. Steinbrink,

Unfortunately, the costs involved in operating the shelter have increased dramatically. Much of this can be attributed to Act 90, a law put into effect to counteract puppy mills in our state. However, the new law extended beyond breeders and impacted animal shelters as well. Under this new law, every animal at intake must be medically examined as well as administered a rabies vaccination by a veterinarian. All animals in our care must now be assessed on a daily basis. These provisions necessitated the need for a full time veterinarian on staff. Act 90 also mandates that every animal is out of its cage for a specified period of time. This alone requires us to schedule an additional 22 man hours per day. This legislation has also significantly increased the amount of record keeping required to verify compliance, resulting in higher administrative costs.

The end result is that we are left with no other option, but to ask for a 10% increase in fees beginning with July 2013. We have made every attempt to economize wherever possible, including asking more of our volunteer staff, but it still isn't enough. However, please be assured, we do take pride in providing a valuable service at a cost far less than what some municipalities pay outside of our area. In addition to the rate increase, the only other contract change involves collection of fines. Sadly, on several occasions we are now seeing owners who refuse to reclaim their animals if they must pay the recovery fee in addition to impoundment and boarding fees. Beginning July 1, we will no longer act as a collection agent. Going forward we will furnish you with the necessary information needed, so that you can bill for the recovery fee. If you have any questions or comments please feel free to contact me at my office. I can be reached during normal business hours at 262-564-8800. In my absence please feel free to ask for my assistant Reanna Stockdale.

Sincerely,

Frank Carmichael Safe Harbor Board President

FJC/rs

7811 60th Avenue • Kenosha, WI 53142 • 262-694-4047

#### AGREEMENT

#### VILLAGE OF PLEASANT PRAIRIE AND KENOSHA COUNTY HUMANE SOCIETY, INC. D.B.A. SAFE HARBOR HUMANE SOCIETY

This agreement, made and entered into by and between the Village of Pleasant Prairie Wisconsin, hereinafter referred to as the "Village", and Kenosha County Humane Society DBA Safe Harbor Humane Society, a non-profit Wisconsin corporation, hereinafter referred to as the "Humane Society",

#### Witnesseth:

Whereas, this agreement supersedes all previous agreements, and all prior written agreements and all understanding not reduced to writing are expressly nullified and superseded by this sole written agreement, which contains the full and complete agreement between the Village and the Humane Society.

For the purpose of this agreement, the term "animals" will apply only to dogs and cats. The term "Animal Control Officer" shall refer to the Village's contracted animal control service.

Now, therefore, in consideration of the mutual undertakings and agreement, set forth, the Village and the Humane Society agree as follows:

1. IMPOUNDMENT. The Humane Society shall house, feed, water, and care for animals brought there by the Animal Control Officer or transported by The Humane Society pursuant to Section 1.1. The Humane Society shall also administer distemper shots for canines and felines as well as bordetella shots for canines and shall obtain veterinary care for ill and injured animals at the sole discretion of the Humane Society as determined on a case by case basis. The Humane Society shall promptly attempt to notify the owners of any strays bearing tags, and shall hold and care for as provided in this agreement, and unless reclaimed by their owners will be deemed property of the Humane Society. The Humane Society will not impound bite cases if the owner is known, unless there is no current rabies vaccination. If the owner is unknown or the owner surrenders the dog to the Humane Society, the dog will be impounded by the Humane Society. If in the event that the impoundment is dictated by the Village or it's agents, including animal control, the Village shall be responsible for a \$17 daily boarding fee. The Humane Society veterinarian will be contacted for observation under Wisconsin State Statutes, section 95.21. All veterinarian charges will be billed to and payable by the Village. At the conclusion of the observation period, the animal will become property of the Humane Society. All veterinarian charges will be billed to and payable by the Village. For impounded animals, the required three day veterinary inspection fee shall be billed at \$50 per day for a total of \$150. At the conclusion of the impoundment or observation period, an abandoned animal will be evaluated and become property of the Humane Society.

- 1.1 **TRANSPORTATION.** The Village shall be responsible for transportation of all animals to the Humane Society. If the Village or its agents(s) or representative(s) requests that the Humane Society transport an animal or animals, there shall be \$50.00 per animal transportation fee charged by the Humane Society to the Village. Said transportation fee shall be due and payable within 30 days of its billing date. Any transportation by the Humane Society shall be from the Village's municipal animal holding location, at reasonably agreeable times. It is understood that the Humane Society will not act as animal control for the Village and will only undertake the transportation of animals pursuant to the terms of this paragraph and agreement.
- 2. SCHEDULE OF FEES AND VILLAGE PAYMENTS. The Village shall pay the following fees to the Humane Society:
  - A. Beginning on July 1, 2013
    - 1. A monthly retainer/service fee of \$1133.00 is due and payable by the 15th of each month.

It is mutually understood that the monthly retainer/service fee is based on the current population in the Village of Pleasant Prairie.

- 3. **COLLECTION OF IMPOUNDMENT AND RECOVERY FEES.** Before releasing any stray animal to its owner, the Humane Society shall collect from the owner an impoundment fee of \$39 and a boarding fee of \$17 for each day boarded.
- 4. CRUELTY CASES. In the event the Humane Society, at the direction of an authorized representative of the Village, accepts animals under conditions of extreme neglect or filth, the Humane Society can, at its discretion, charge the Village an additional \$17 per animal per day fee for expenses incurred in cleaning and caring for the animals and transportation fees, if transportation is provided by the Humane Society. In the event the Humane Society, at the direction of any law enforcement officer, judge, prosecutor, or authorized representative of the Village, the County, the State, or any other appropriated governmental unit, is requested to hold animals as evidence or otherwise for a period exceeding the seven day impoundment period, then a written agreement obligating said Village, County, or other entity to pay the reasonable costs thereof, will be required as a condition of the Humane Society caring for the animals in excess of the seven day impoundment period or in the event that no such agreement is received by the Humane Society, then an additional \$17.00 per animal per day fee shall apply for each day any such animal is held by the Humane Society. The agreement obligating the requesting body to pay the Humane Society for expenses caring for the animal(s) is intended to cover a situation where the expenses are not collected by the Humane Society under Wisconsin State Statue Chapter 173. The agreement shall provide that the authorizing entity shall pay the Humane Society for the aforementioned care on a monthly basis and then said entity is entitled to seek these expenses from the defendant or the county under Wisconsin State Statute Chapter 173.

- 5. **AUTHORIZED REPRESENTATIVE OR THE VILLAGE.** The Village shall provide the Humane Society with a list of authorized representatives and their telephone numbers.
- 6. INDEMNIFICATION AND HOLD HARMLESS. The Village will at all times during the term of this agreement or any renewal or extension thereof, indemnify and hold harmless the Humane Society, its officers, agents, employees or representatives, against any liability, loss, charges, damages, costs, expenses or attorney fees which they may hereafter sustain, incur, or be required to pay as a result of a willful or negligent act or omission of the Village, which act or omission, or failure causes any person or party to suffer personal injury, death property loss or damage. The Humane Society will at all times during the term of this agreement or any renewal or extension thereof, indemnify and hold harmless the Village, its officers, employees, or representatives, against any liability, loss, charges, damages, costs, expenses or attorney fees which they may hereafter sustain, incur or be required to pay as a result of a willful or negligent act or omission of the Humane Society, which act, omission, or failure causes any person or party to suffer personal injury, death, property loss or damage.
- 7. **ASSIGNMENT OR TRANSFER.** The Humane Society shall not in any manner assign or transfer this agreement or any portion thereof.
- 8. **NON-WAIVER.** The failure of either the Village or the Humane Society to insist upon strict performance of any of the terms, covenants or conditions contained in this agreement shall not be construed as a waiver of any subsequent breach of default.
- 9. **NOTICE OF AMENDMENT.** Any notice required by or relevant to this agreement shall be by personal service or provided by certified mail. Notice to the Village shall be delivered or sent to:

John Steinbrink Village of Pleasant Prairie 9915 39th Avenue Pleasant Prairie, WI 53158

Notice to the Humane Society shall be delivered or sent to:

The President Safe Harbor Humane Society, Inc. P.O. Box 2306 Kenosha, WI 53141

- 10. **EFFECTIVE DATE, TERM OF AGREEMENT, RENEWAL.** This agreement shall become effective after the appropriate authorizations have been made by the Village Board and the Board of Directors of the Humane Society. This agreement may be modified by the mutual consent of the Village and the Humane Society.
- 11. **TERMINATION OF THE AGREEMENT.** Either the Village or the Humane Society may terminate this agreement upon thirty (30) days advance notice as provided in Section 9 of this agreement, upon the grounds that either party has failed to abide or comply with the terms of the agreement.
- 12. **AUTHORITY.** The Village enters into this agreement by the authorization of the Village Board at a meeting held on the \_\_\_\_\_ day of \_\_\_\_\_, 2013, in the Village of Pleasant Prairie, Wisconsin.
- 13. **INTEGRATION.** This agreement is the sole, full, and complete agreement of the Village and the Humane Society.
- 14. **SEVERABILITY.** The provisions of this agreement shall be construed as being severable, wherever this can be accomplished without either party being materially prejudiced.

IN WITNESS WHEREOF the parties hereto have executed this agreement of the dates listed below.

#### VILLAGE OF PLEASANT PRAIRIE

Village Chairman	Date					
Village Clerk	Date					
KENOSHA COUNTY HUMANE SOCIETY INC. D.B.A. SAFE HARBOR HUMANE SOCIETY						
President, Board of Directors	Date					
Secretary, Board of Directors	Date					





# MEMORANDUM

TO: VILLAGE BOARD OF TRUSTEES

- FROM: JOHN P. STEINBRINK SR. VILLAGE PRESIDENT
- DATE: MAY 15, 2013
- RE: COMMISSION APPOINTMENTS

I recommend the following appointments to the committees for the terms listed below:

## **Police & Fire Commission**

Roger Mayer

Term – May 1, 2018

## **Board of Review**

Bill Morris (Alternate #1)

Term – May 1, 2016

#### CLERK'S CERTIFICATION OF BARTENDER LICENSE APPLICATIONS Period Ending: May 14, 2013

I, Jane M. Romanowski, Village Clerk of the Village of Pleasant Prairie, Kenosha County, Wisconsin, do hereby certify the following persons have applied for bartender licenses and **each applicant is in compliance with the guidelines set forth in Chapter 194 of the Municipal Code.** I recommend approval of the applications for each person as follows:

#### NAME OF APPLICANT

#### LICENSE TERM

- 1. Patrick V. Cook
- 2. Manual E. Reyes

thru June 30, 2015 thru June 30, 2015

Jane M. Romanowski Village Clerk